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CITY & COUNTY OF HONOLULU

FINAL ENVIRONMENTAL ASSESSMENT

HAUULA BASEYARD IMPROVEMENTS (SMA)*
Hauula, Koolauloa District, Honolulu, Hawaii

Proposing Agency:

State of Hawaii
Department of Transportation
Highways Division
869 Punchbowl Street
Honolulu, Hawaii 96813

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FINAL ENVIRONMENTAL ASSESSMENT

HAUULA BASEYARD IMPROVEMENTS

Hauula, Koolauloa District, Honolulu, Hawaii

Prepared In Partial Fulfillment of the Requirements
of Chapter 25, Revised Ordinances of Honolulu,
City and County of Honolulu

Proposing Agency:

State of Hawaii
Department of Transportation
Highways Division
869 Punchbowl Street
Honolulu, Hawaii 96813

Prepared By

Lou Chan & Associates, Inc.
and
Gerald Park Urban Planner

May 1999

PROJECT PROFILE

Project: HAUULA BASEYARD IMPROVEMENTS

Proposing Agency: State of Hawaii
Department of Transportation
Highways Division
869 Punchbowl Street
Honolulu, Hawaii 96813

Approving Agency: Department of Planning and Permitting
City and County of Honolulu
650 South King Street
Honolulu, Hawaii 96813

Tax Map Key: 5-4-02: 12
Land Area: 13,184 Square Feet
Building Site: 13,184 Square Feet
Land Owner: State of Hawaii

Existing Use: Highway Maintenance Baseyard
State Land Use Designation: Urban
General Plan: Rural
Development Plan Area: Koolauloa
Land Use Map: Residential
Public Facilities Map: No Symbol
Zoning: R-5 Residential

Special Management Area: Within SMA

Need for Assessment: Major SMA Permit Required

Contact Person: Peter Chan, Project Manager
State of Hawaii
Department of Transportation
Highways Division
Oahu District-Maintenance Design Section
727 Kakoi Street
Honolulu, Hawaii 96819

Telephone: 837-8046

Note: Revisions to the text of the Draft Environmental Assessment appear in *bold italic* type. Deleted text appears in bold brackets.

TABLE OF CONTENTS

	<u>Page</u>
Project Profile	i
SECTION 1 DESCRIPTION OF THE PROPOSED ACTION	
A. Purpose of the Project	1
B. Technical Characteristics	1
C. Economic Characteristics	2
D. Social Characteristics	2
SECTION 2 DESCRIPTION OF THE AFFECTED ENVIRONMENT	
A. Existing Improvements	7
B. Physical Characteristics	9
C. Land Use Controls	9
D. Public Facilities	9
SECTION 3 SUMMARY OF ENVIRONMENTAL IMPACTS AND MEASURES TO MITIGATE ADVERSE EFFECTS	
A. Assessment Process	14
B. Short-term Impacts	14
C. Long-term Impacts	15
D. Impacts on Special Management Area Resources	16
SECTION 4 ALTERNATIVES TO THE PROPOSED ACTION	
A. No Action	23
B. Alternative Sites	23
SECTION 5 PERMITS AND APPROVALS	24
SECTION 6 AGENCIES AND ORGANIZATIONS CONSULTED IN THE PREPARATION OF THE ENVIRONMENTAL ASSESSMENT	25
SECTION 7 DETERMINATION OF SIGNIFICANCE	26
REFERENCES	
APPENDIX A COMMENTS AND RESPONSES	

LIST OF EXHIBITS AND PHOTOGRAPHS

Exhibit	Title	Page
1	Location Map	3
2	Site Plan	4
3	Office Building Roof Plan, Floor Plan, Elevations	5
4	Storage Shed Roof Plan, Floor Plan, Elevations	6
5	Flood Hazard Area	10
6	Development Plan Land Use Map	11
7	Special Management Area	12
Photographs		
1	Existing Field Office	8
2	Existing Equipment Shed	8

SECTION 1

DESCRIPTION OF THE PROPOSED ACTION

The State of Hawaii Department of Transportation, Highways Division, proposes to demolish its existing Hauula Baseyard and construct a new baseyard on the same site. The Hauula Baseyard is located in the coastal town of Hauula, Koolauloa District, City and County of Honolulu, Hawaii. The site is identified as Tax Map Key 5-4-02: 12 encompassing a land area of 13,184 square feet. A Location Map is shown in Exhibit 1.

A. Purpose of the Project

The Department of Transportation ("DOT") has determined that the two existing structures generally comprising the Hauula Baseyard are in deteriorated condition and should be replaced. There is also a need to redesign the site to provide space for two new structures, equipment and vehicle access and parking, and areas for storing material.

B. Technical Characteristics

Two new buildings are proposed. A new office building will be constructed on the mauka side (south) side of the lot. Interior space will be divided between an office (21' X 11') and restroom, shower, and locker facilities for workers (9' X 26'). The 465 square foot building will be erected on a raised concrete foundation 1½ feet above natural grade, enclosed with CMU walls, and topped by a pitched metal roof. The height of the building is about 12'6" measured from existing grade.

A new equipment shed will be located at the rear (east) of the site. The structure measures 25' X 70' and will be erected with CMU walls set on a concrete foundation. A pitched metal roof is proposed and the tallest part of the building is about 18'3" in height. The entire structure will be enclosed except for metal doors and awnings and roll up metal doors at the front of the structure for vehicle access. The floor of the equipment shed will be paved with concrete.

An open material storage area will be built at the north side of the site adjacent to the new storage shed. The storage area will be enclosed by an 5-foot high fully grouted CMU wall with grouted CMU partitions for four storage bins. Each bin measures 10' long X 8' wide and is open on the driveway side for access. Cold asphalt concrete, gravel, sand, crushed coral, and other materials used for highway maintenance will be stored in the bins.

[New 8-foot high chain link fencing topped with 3 strands barbed wire will be erected around the perimeter.] *The Department of Transportation is evaluating several alternatives to chain link fencing. One alternative is a 4-foot high CMU wall topped with a 4-foot high chain link fence without barbed wire. A revised wall/fence design will be incorporated into the project plans.* A 20-foot wide driveway will be located off Hauula Homestead Road. The driveway will be secured at nights by a gate. The driveway and area around and under the storage bins will be paved with asphalt.

Off-street parking for four vehicles will be accommodated on site. One parking stall is marked for handicapped use. Both buildings will be ADA accessible.

Water service will connect to the existing baseyard water meter in Hauula Homestead Road. *The Board of Water Supply (1999) commented that the existing water system is adequate to accommodate the proposed baseyard improvements and a water allocation from the Department of*

Land and Natural Resources will be required. A septic tank system will be used for wastewater disposal. Solids will be collected in a 750 gallon precast concrete septic tank and effluent disposed of in a leach field. The septic tank will be located in the northwest corner of the site about 20-feet from the property line. Effluent will be disposed in a 200 square foot (10' by 20') leach field to be located in the west corner of the front yard adjacent to Hauula Homestead Road.

No permanent drainage structures or alterations to existing on-site drainage patterns are planned. The site and paved areas will be graded to direct runoff in the direction of Kamehameha Highway as under existing conditions.

Power and communication lines will be brought to the new office building as overhead service. From the office building, the lines will be installed in underground ducts to the new equipment building.

All structural improvements are set back 30 feet from Hauula Homestead Road (front yard) and 15 feet on all other sides (rear and side yards) in observance of the yard requirements of the Residential zoning district. None of the structures will exceed the 25-foot maximum allowable building height.

The site contractor will be responsible for the proper disposal of demolition debris. At this time the location of the disposal site is unknown. All solid waste generated by activities at the baseyard are currently collected by Department of Transportation Highways Division maintenance personnel and transported to the Kapaa/Kailua Sanitary Landfill for proper disposal. This practice will continue when the new baseyard is operational. (Department of Planning and Permitting, 1999).

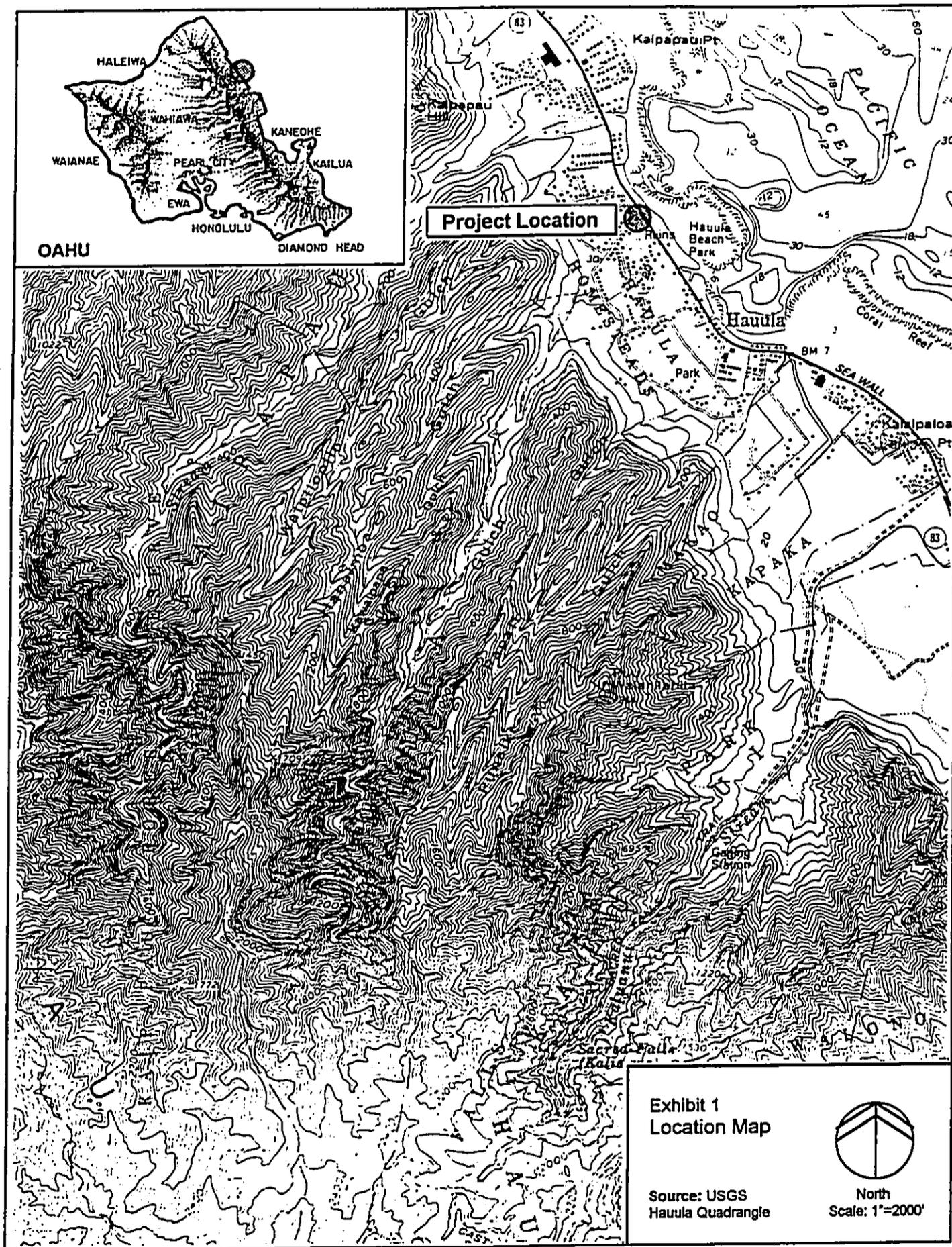
C. Economic Characteristics

The construction cost for the project is estimated at \$ 560,000 and will be funded by the State of Hawaii. Construction is projected to commence during the last quarter of 1999. The project will be built in one phase with an estimated 8 months for completion.

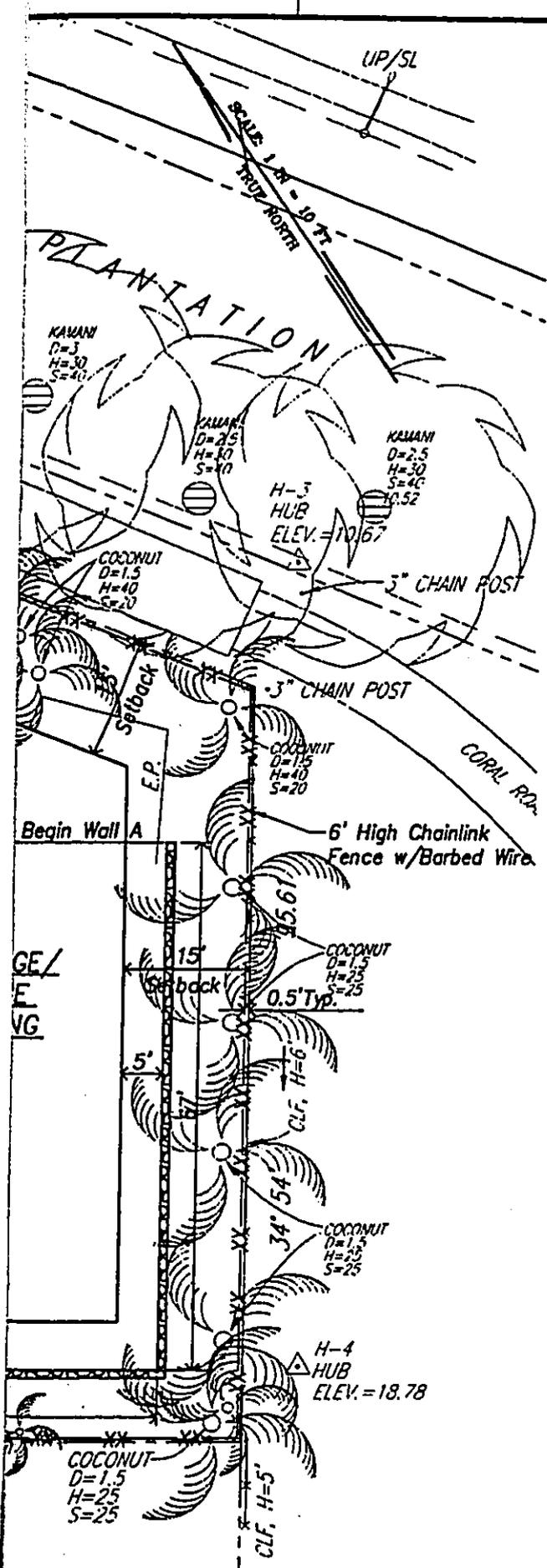
The property (TMK: 5-4-02: 12) is owned by the State of Hawaii.

D. Social Characteristics

The Hauula highway maintenance crew will be assigned temporarily to the Kaneohe Baseyard during construction. The Kaneohe Baseyard is the nearest State highway maintenance facility to the Hauula Baseyard.



FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	HWY-0-03-98	1998	-	-



UTILITY PLAN
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 CITY & COUNTY OF HONOLULU



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Exhibit 2

STATE OF HAWAII
 DEPARTMENT OF TRANSPORTATION
 HIGHWAYS DIVISION

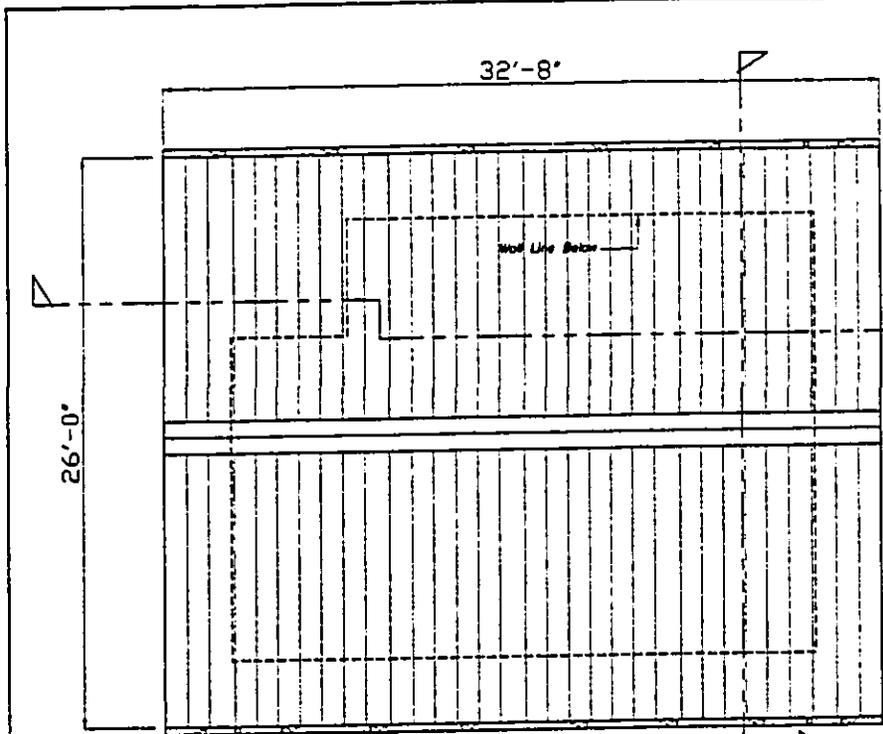
SITE & UTILITY PLAN

HAULLA BASEYARD IMPROVEMENTS

Project No. HWY-0-03-98

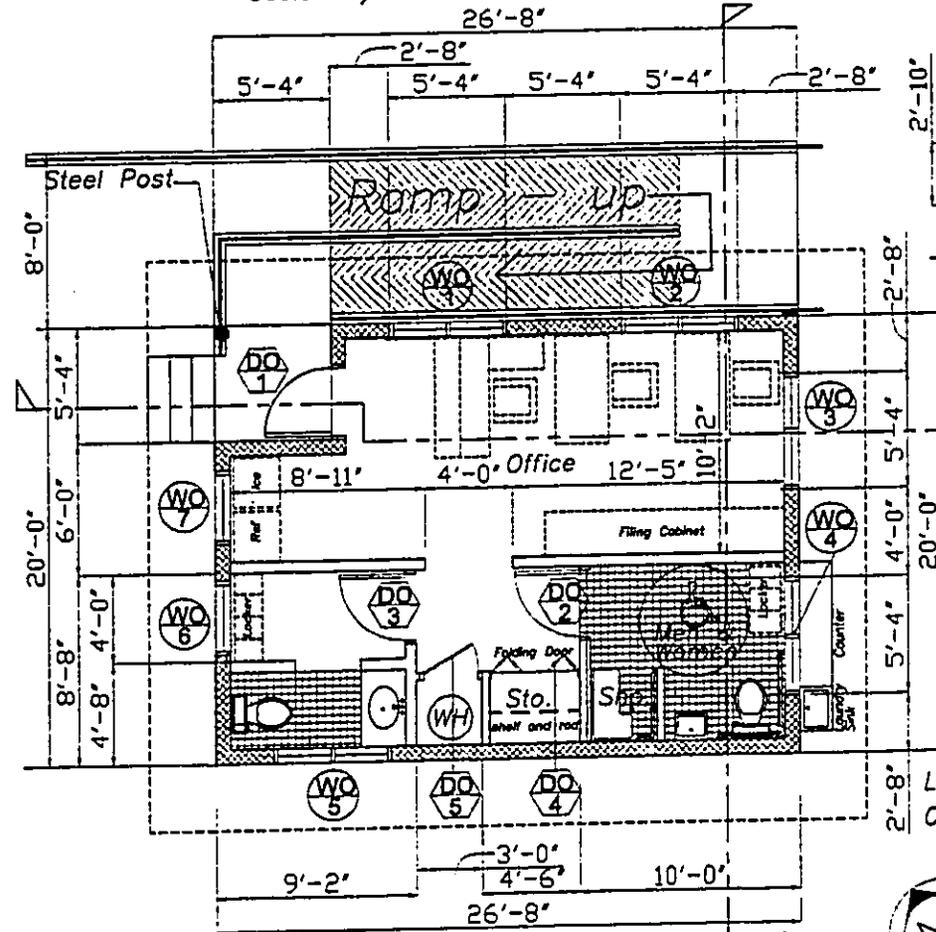
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SHEET No. C3 OF 36 SHEETS



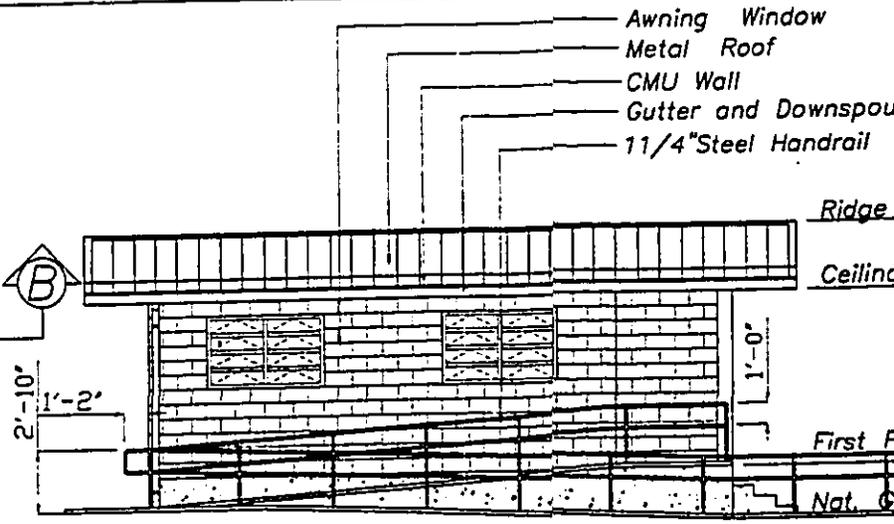
OFFICE ROOF PLAN

Scale 1/4" = 1' - 0"



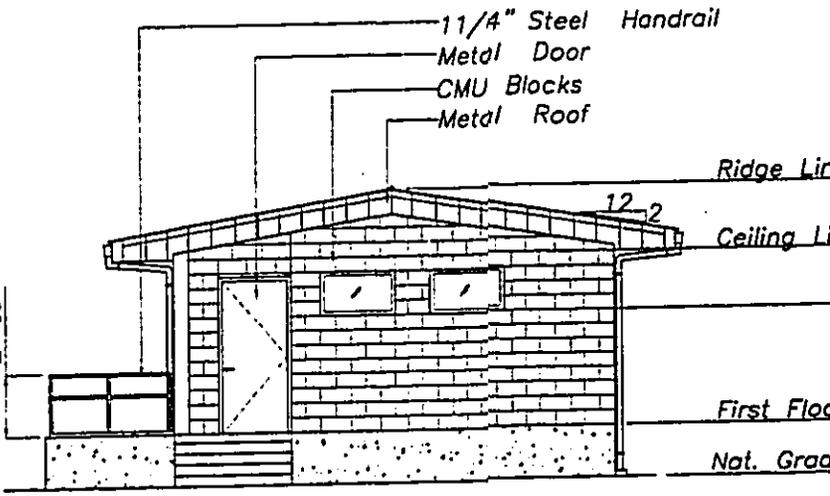
OFFICE PLAN

Scale 1/4" = 1' - 0"



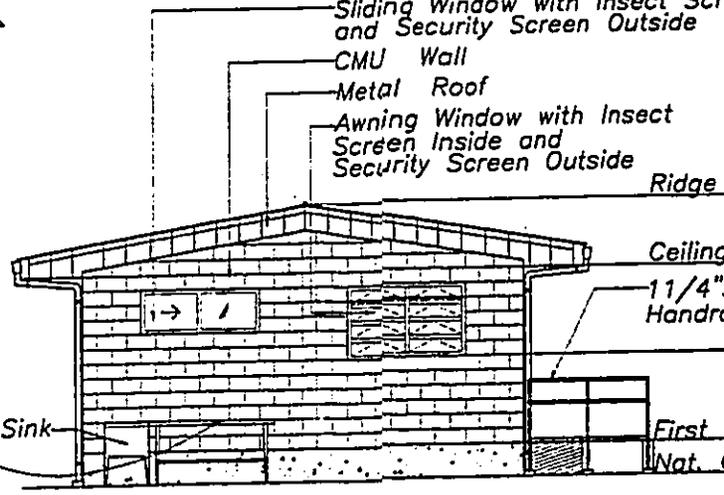
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NORTHWEST ELEVATION

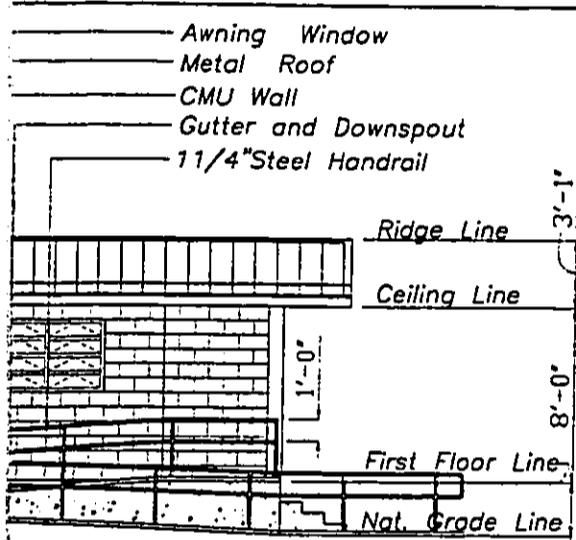
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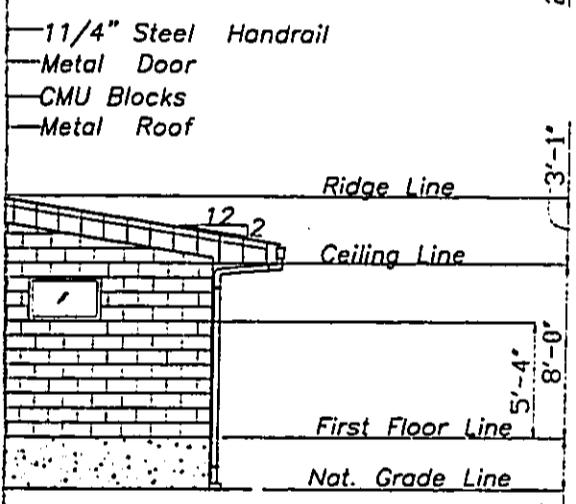
SOUTHEAST ELEVATION

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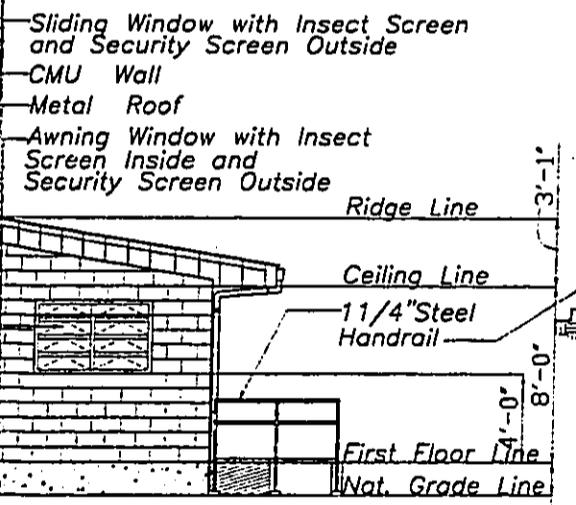




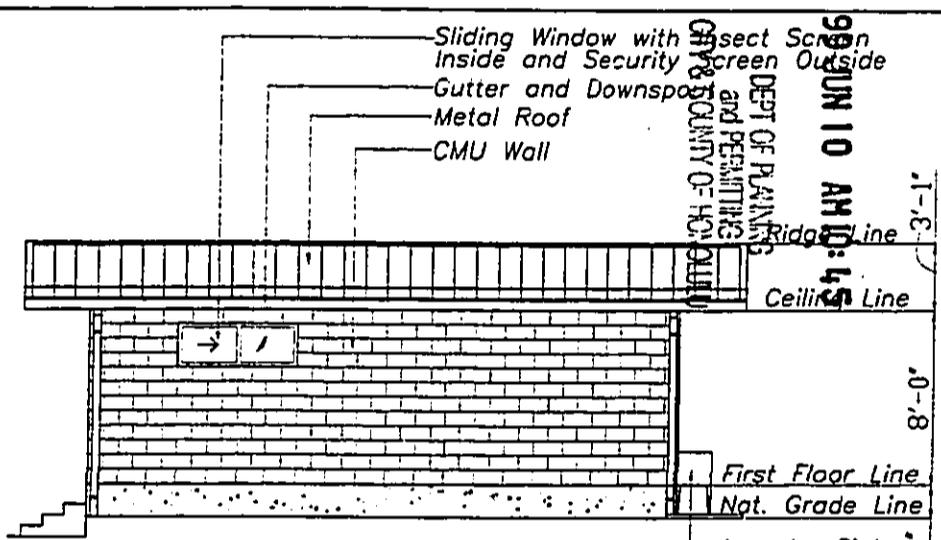
ELEVATION



ELEVATION

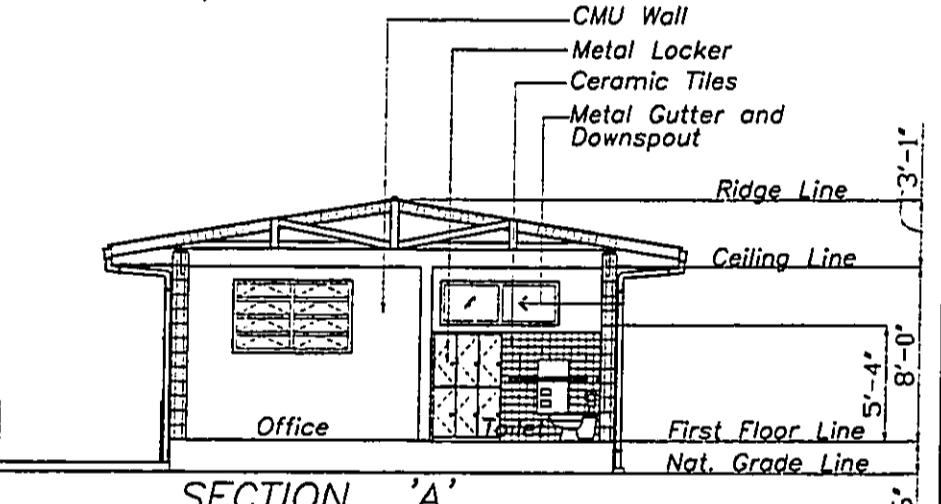


ELEVATION



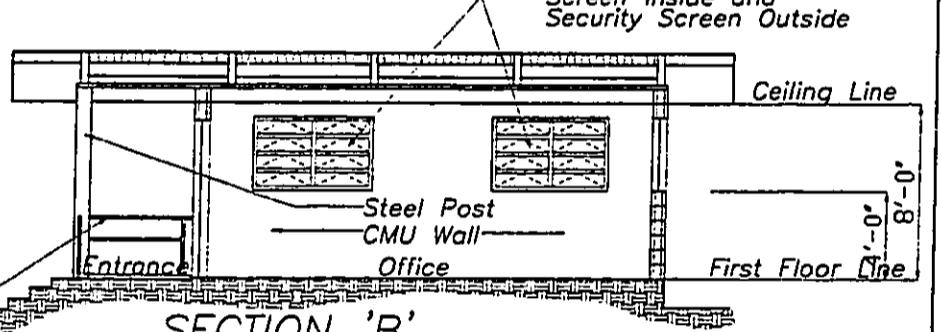
SOUTHWEST ELEVATION

Scale 1/4" = 1' - 0"



SECTION 'A'

Scale 1/4" = 1' - 0"



SECTION 'B'

Scale 1/4" = 1' - 0"

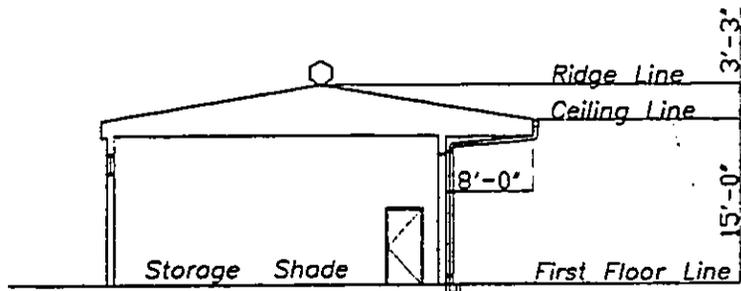
Exhibit 3



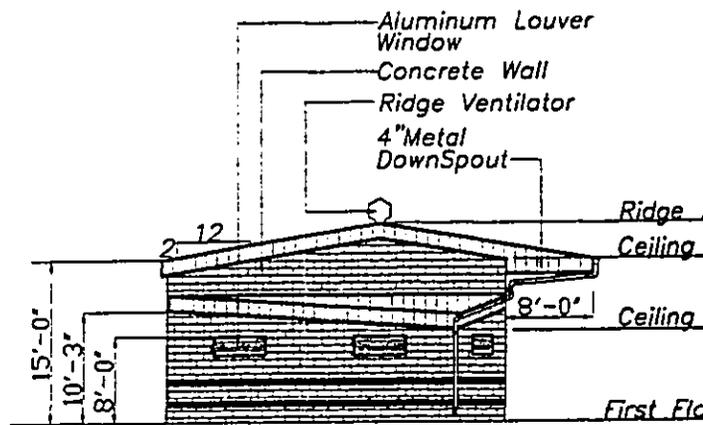
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STATE OF HAWAII
 DEPARTMENT OF TRANSPORTATION
 HIGHWAYS DIVISION
**FLOOR PLAN, ROOF PLAN
 ELEVATIONS & SECTIONS**
**HAUULA BASEYARD
 IMPROVEMENTS**
 Project No. HWY-0-03-98
 Scale: AS SHOWN Date: OCT, 1998

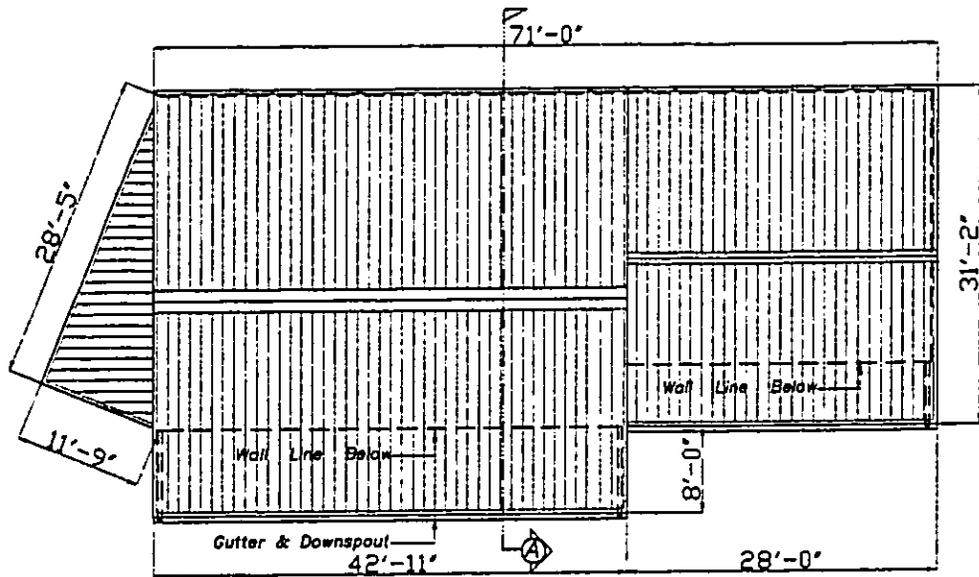
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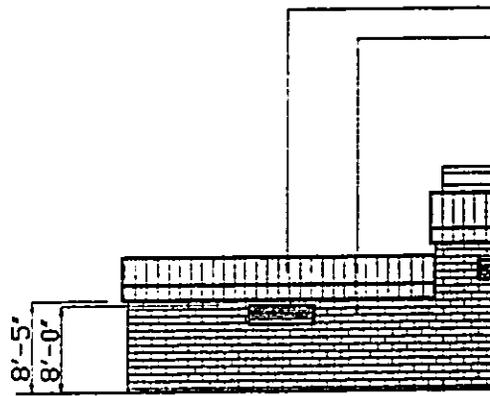
SECTION A'-A'
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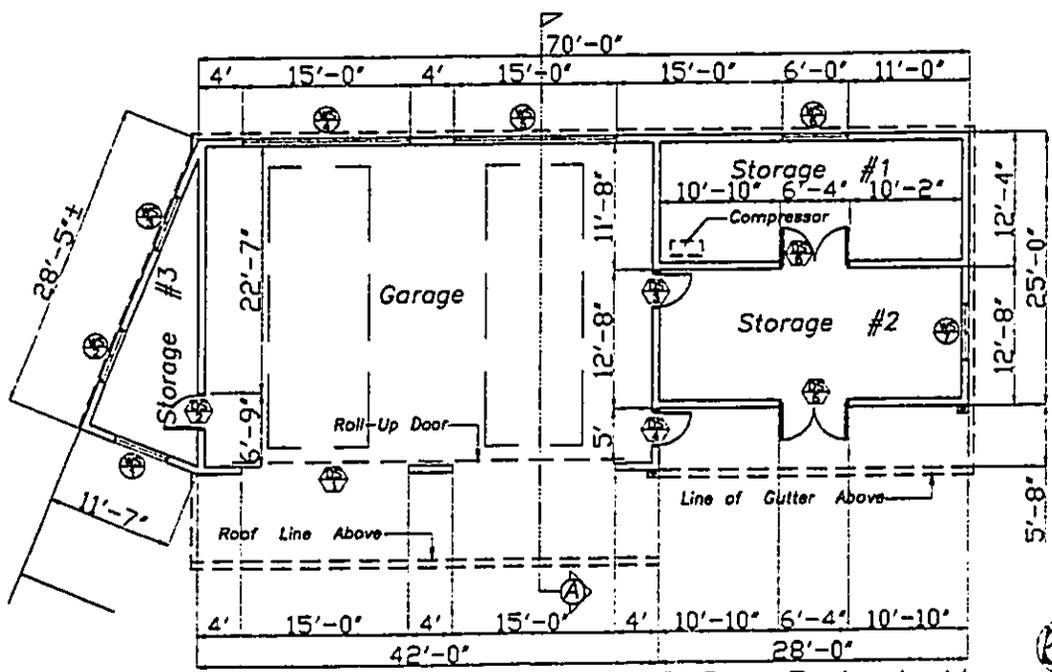
NORTHEAST ELEVATION
Scale 1/8" = 1' - 0"



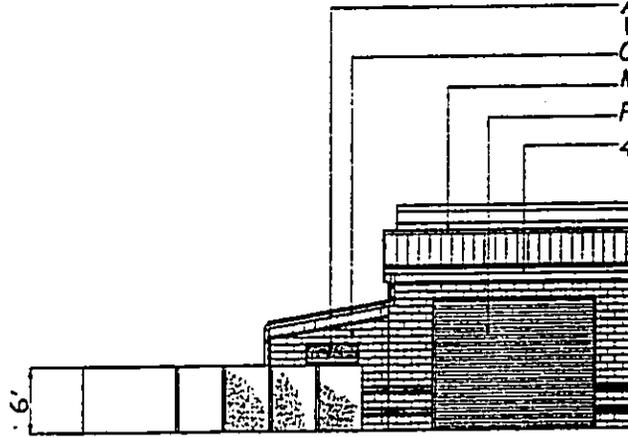
STORAGE SHED PLAN
Scale 1/8" = 1' - 0"



SOUTHEAST ELEVATION
Scale 1/8" = 1' - 0"

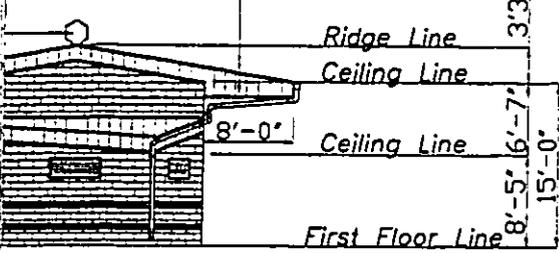


STORAGE SHED PLAN
Scale 1/8" = 1' - 0"



NORTHWEST ELEVATION
Scale 1/8" = 1' - 0"

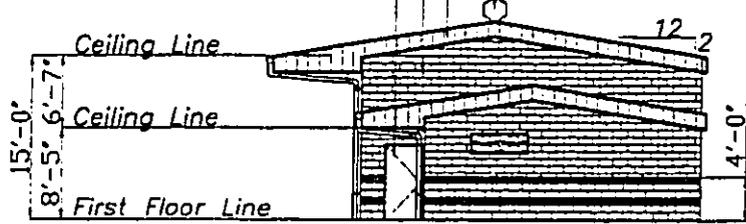
- Aluminum Louver Window
- Concrete Wall
- Ridge Ventilator
- 4" Metal DownSpout



EAST ELEVATION
Scale 1/8" = 1' - 0"

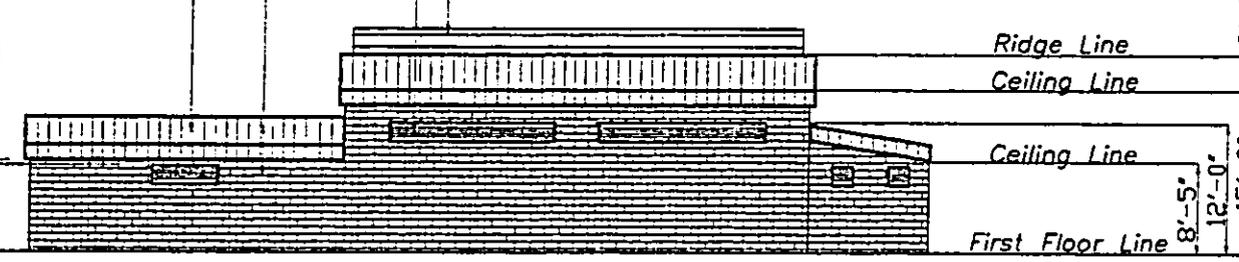
DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	HWY-0-03-98	1998		

- Metal Door
- Concrete Wall
- Metal Roof
- Ridge Ventilator (See Specs)



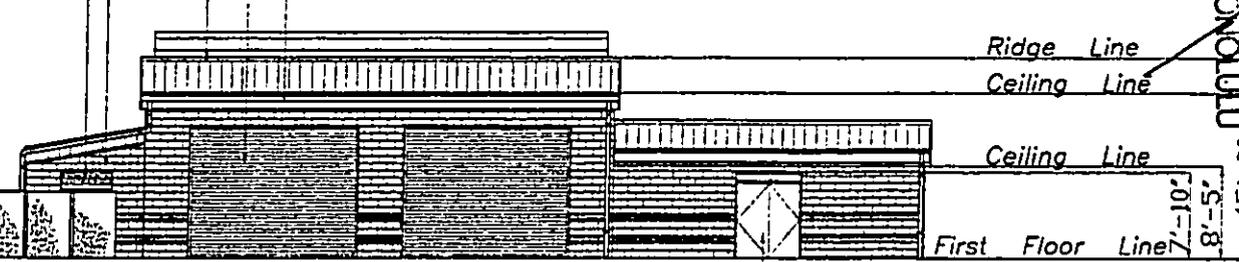
SOUTHWEST ELEVATION
Scale 1/8" = 1' - 0"

- Metal Roof
- Concrete Wall
- Aluminum Louver Window
- Ridge Ventilator

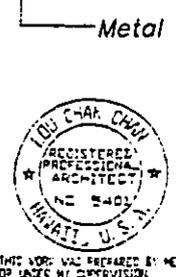


SOUTHEAST ELEVATION
Scale 1/8" = 1' - 0"

- Aluminum Louver Window
- Concrete Wall
- Metal Roof
- Roll Up Door
- 4" Metal DownSpout



NORTHWEST ELEVATION
Scale 1/8" = 1' - 0"



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

**FLOOR PLAN, ROOF PLAN
SECTION & ELEVATIONS**

**HAUULA BASEYARD
IMPROVEMENTS**

Project No. HWY-0-03-98
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Exhibit 4

A. Existing Improvements

Located near the corner of Kamehameha Highway and Hauula Homestead Road, the Hauula Baseyard is bounded by Hauula Homestead Road to the north, Hauula Congregational Church to the south, a coral road leading to the Hauula Congregational Church to the east, and a single-family dwelling to the west. It has functioned as a baseyard for the DOT Highways Maintenance Division since constructed in 1971. The seven employees assigned to the baseyard are responsible for road maintenance functions in north-northeast Oahu from Waimea Bay to Kualoa Beach Park.

Two structures are located on the premises: a field office and a covered equipment shed. The one-story, wooden field office is divided into an office and a bathroom/locker room. The equipment shed, which measures 25' wide X 40' long X 17' high (1,000sf), is a *steel framed corrugated metal building*. The building is enclosed with corrugated siding on three sides and topped with a corrugated metal roof. The siding is severely rusted with large holes in the walls and roof seams.

An area for storing flammable fuels and chemicals is located in a corner of the equipment building. The 80 square foot storage area is enclosed by a 10 foot high wire fence and metal roof. The entry gate is secured with a padlock. There are no underground or above ground storage tanks on the premises.

There are no holding bins for materials. Sand, gravel, and ac cold mix are stockpiled in the northern corner of the lot next to the equipment building. The stockpiles are uncovered. Tires, batteries, steel plates, dried plant materials, and debris are neatly piled in the open space on the south side of both buildings. There is sufficient room to park vehicles alongside the buildings and a trailer is parked adjacent to the office building.

The parcel is a "cut" lot generally lower in elevation than adjoining lots to the south and west. The "cut" is about 5 feet inside the property line and the high ground retained by stone walls about three feet in height along the south side and 2+ feet on the west. Retained areas are landscaped with assorted palms and hedge materials. Dense plantings along the west property line helps to screen the baseyard from the adjoining residential dwelling. Mature coconut trees are planted along the east property line facing Kamehameha Highway.

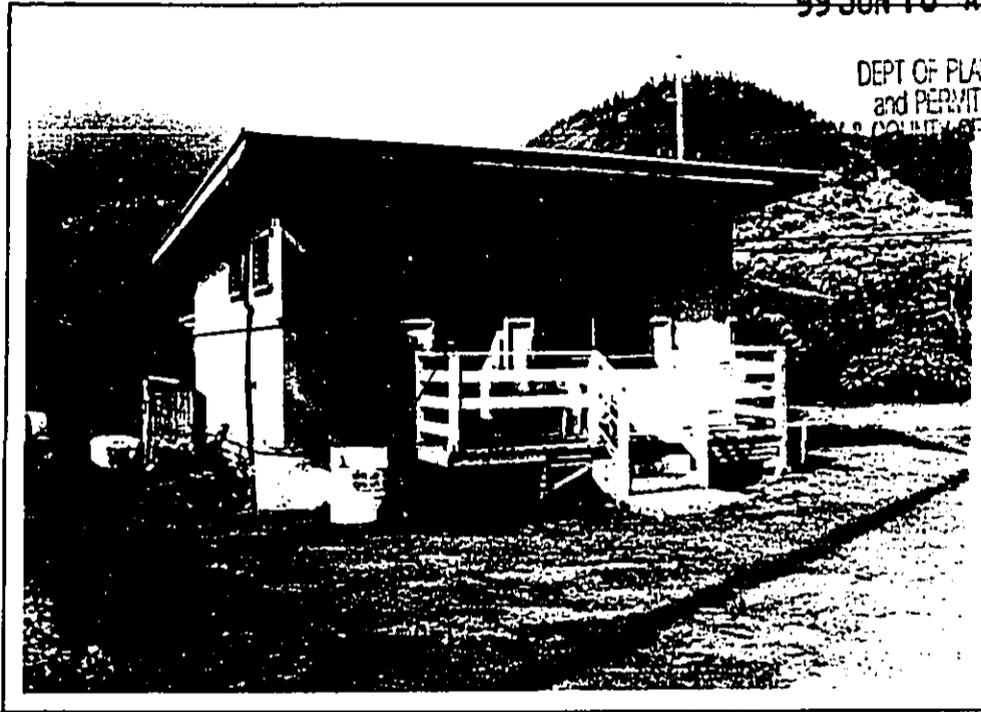
Most of the lot is paved with asphalt concrete. In addition to the perimeter landscaping described above, grass is planted at the rear and north sides of the office building.

Vehicles, mechanized equipment, and tools are stored (or parked) in the covered equipment shed. However, there is not enough covered space for all the vehicles and equipment assigned to the baseyard and vehicles and equipment are stored in the open and exposed to the elements. Parking for six vehicles fronts the grassy area next to the office building.

Water is drawn from [an 1-1/2"] a 5/8-inch water meter and a 2-inch water meter (*Board of Water Supply, 1999*) in Hauula Homestead Road. Power and communication services are provided by overhead lines to the office building and underground from the office building to the equipment shed.

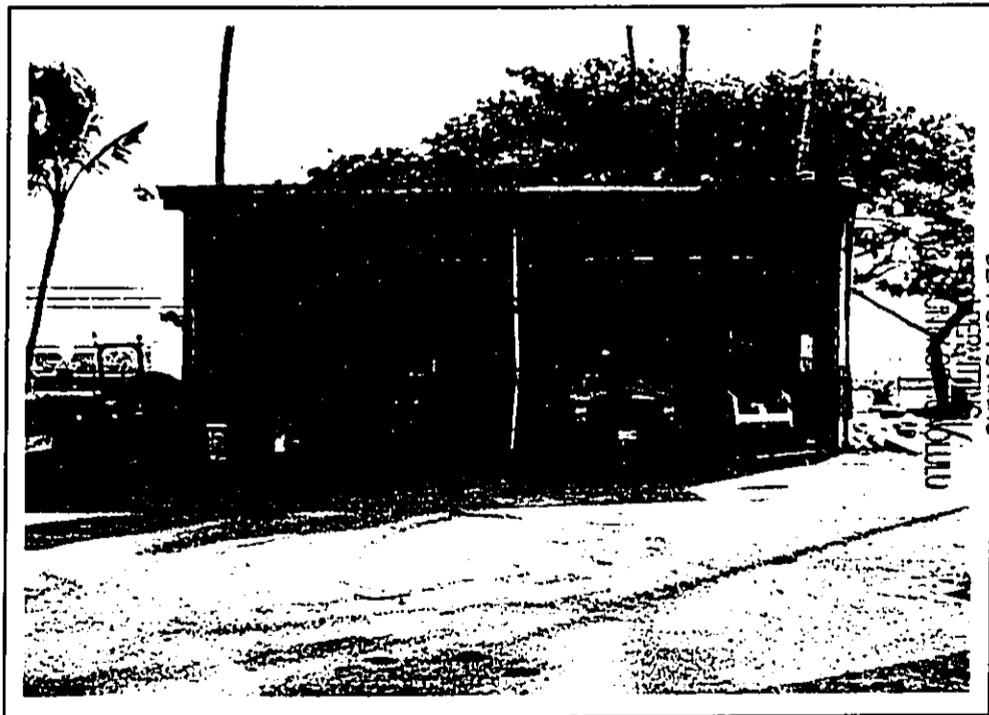
Wastewater is discharged into a cesspool on the lot. No drain inlets on the premises were observed and, more than likely, on-site runoff flows across the site in the direction of Kamehameha Highway.

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Photograph 1. Hauula Baseyard Field Office.



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COUNTY OF HONOLULU

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Photograph 2. Hauula Baseyard Equipment Shed.

B. Physical Characteristics

The site slopes mauka to makai from a high elevation of about 12 feet measured at the base of a stone wall in the southwest corner to a low of 9 feet in the northeast corner. The site has been graded to drain outward in the direction of Hauula Homestead Road and Kamehameha Highway.

According to the Soil Conservation Service (1972), soils in the area are of the Hanalei series. This poorly drained, stony silty clay does not pose a significant erosion hazard.

Flood Hazard Maps for this section of Hauula (see Exhibit 5) place the baseyard in Zone X which is defined as "areas determined to be outside the 500-year flood plain (Federal Emergency Management Agency)". The property is also outside a delineated coastal high hazard area.

There are no archaeological features on the premises.

On-site flora is limited to ornamental plants and trees. Grass and wedelia are the primary ground cover. Coconut, plumeria, shower, and kamani trees interspersed with ti and hibiscus (variegated) are planted around the perimeter. A single ponytail grows on the north side of the office building.

No wildlife was seen in the vicinity but a crowing rooster was heard and dogs seen in adjacent yards and streets.

C. Land Use Controls

The community of Hauula is general planned Rural on the Oahu General Plan. The Koolauloa Development Plan Land Use Map (Ordinance No. 83-9) classifies the property Residential (Exhibit 6) and it is zoned R-5. The proposed improvement is not symbolized on the Koolauloa Development Plan Public Facilities Map.

The property is located in the county delineated Special Management Area (See Exhibit 7) and a Special Management Area Use Permit is required prior to construction.

According to Chu (1987), "based on the rural character of Hauula and several stretches of continuous views from the highway, unity and a sense of rural intactness are the primary visual attributes of the area." Stationary views are available from all beach parks and also provide exceptional lateral coastal views due to the undulation form the coastline. Chu cited Hauula Beach Park as providing significant stationary views of the coastline for pedestrians.

D. Public Facilities

Kamehameha Highway, one of the major roads on Oahu, links Hauula with other shoreline communities in both the Koolauloa and North Shore districts, Central Oahu, East Honolulu, and Honolulu. Near the Hauula Baseyard, two 10-foot travel lanes are accommodated within a 50-foot right of way.

A 52-foot wide right-of-way separates Kamehameha Highway and the baseyard. Topographical survey maps identify a government remnant road (4' wide), the former Kahuku Plantation Company railroad right-of-way (32' wide), and a crushed coral driveway (16' wide) leading to the Hauula

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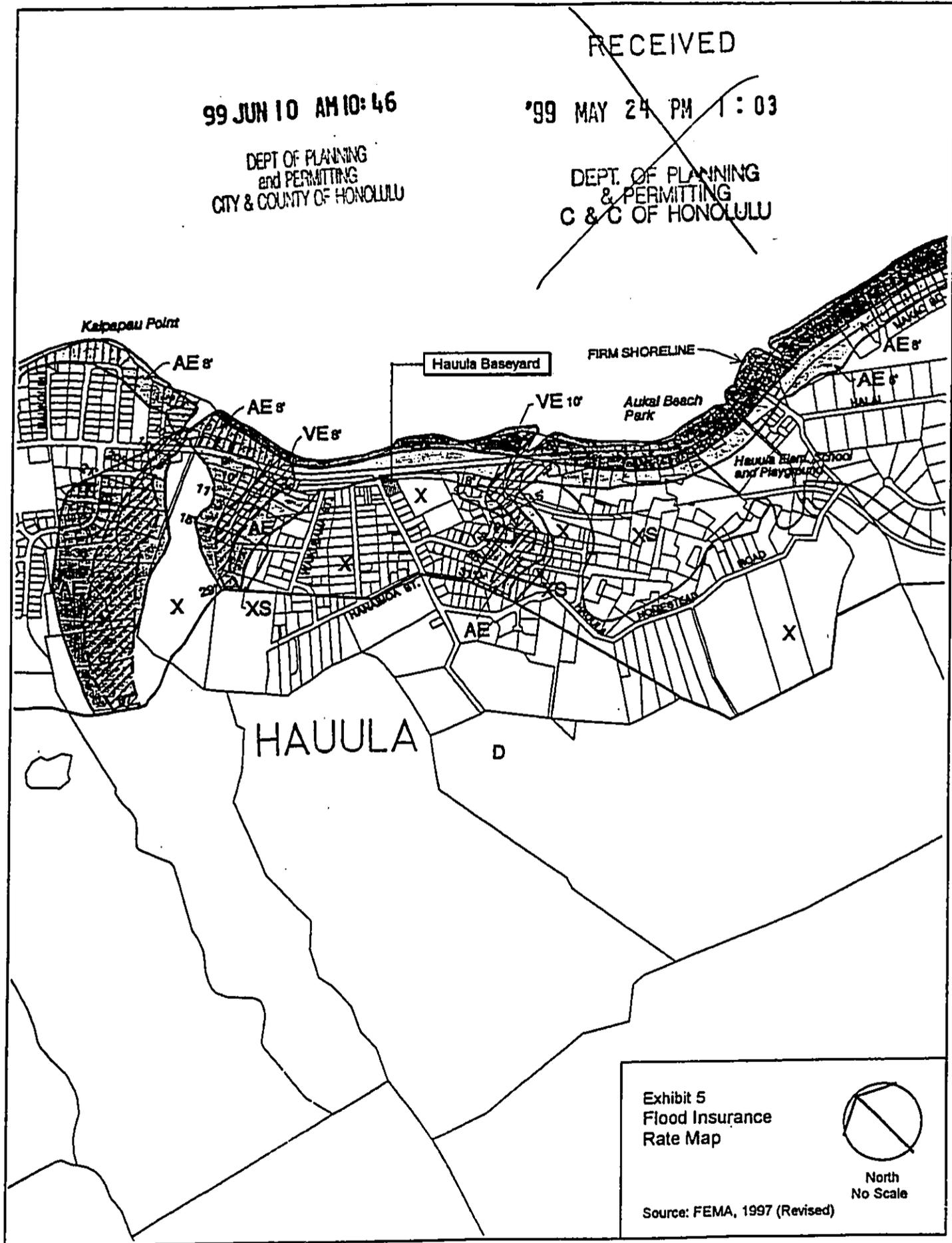


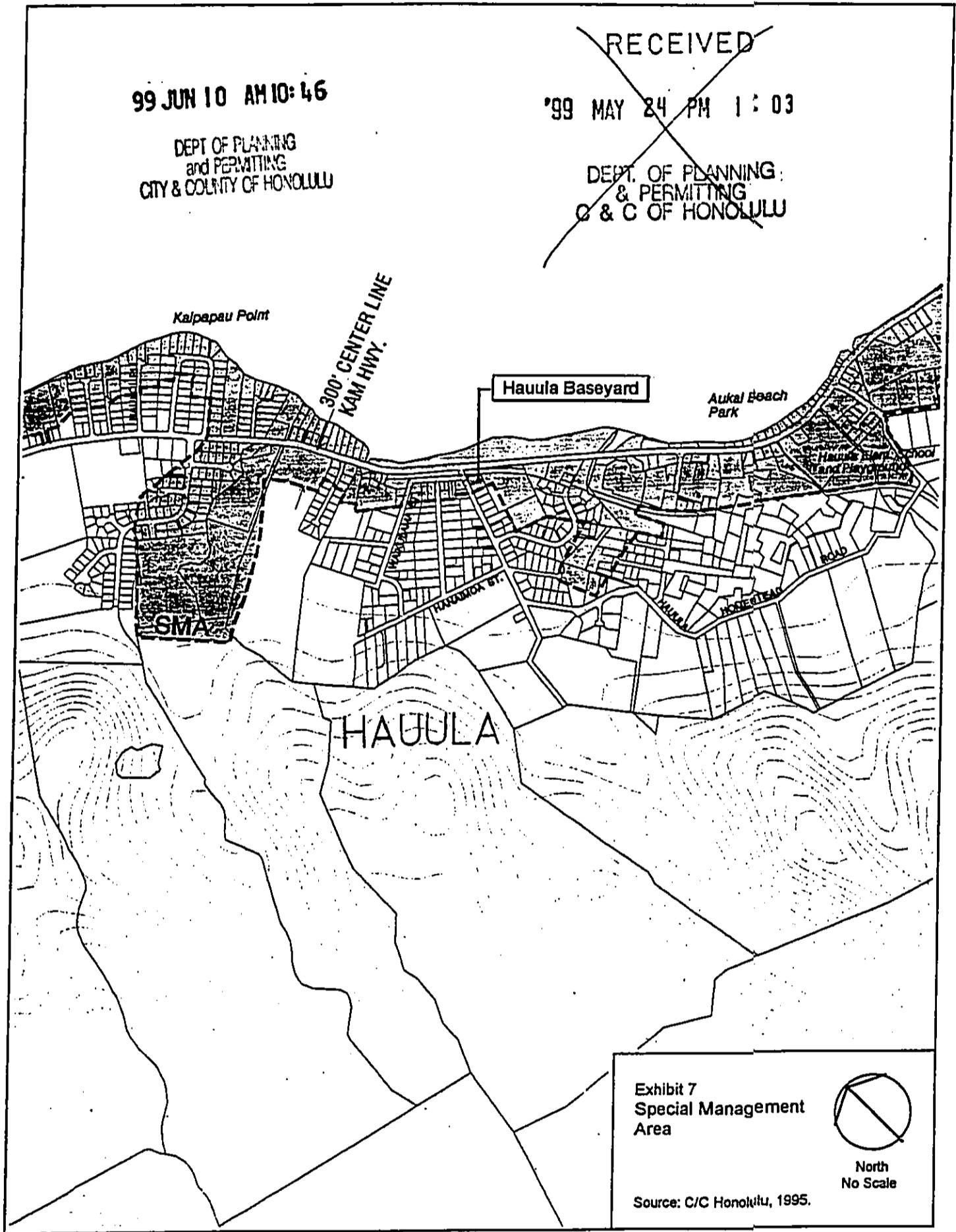
Exhibit 5
Flood Insurance
Rate Map



North
No Scale

Source: FEMA, 1997 (Revised)

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Congregational Church as comprising the right-of-way. There are no structural improvements within the right-of-way.

Hauula Homestead Road, a residential street within a 40-foot right-of-way, connects Kamehameha Highway with residential development in Hauula Valley. Egress onto Kamehameha Highway is controlled by a stop sign. The posted speed limit is 25 mph. The two-lane, two-way undivided street features a pavement width of about 20 feet fronting the baseyard. There are no curbs, gutters, or sidewalks. Sections of the road shoulder are a paved drainage swale which direct runoff into raised 18" drain inlets on either side of the road. The inlets are within the road right-of-way just mauka of the baseyard.

Electrical power and telephone service is drawn from overhead lines along Hauula Homestead Road.

Hauula Beach Park, a City and County of Honolulu beach park, is located across Kamehameha Highway from the Hauula Baseyard. Facilities at the 9 acre park include a bathhouse/pavilion, softball and volleyball fields, picnic areas, and campsites.

The baseyard is located below the Underground Injection Control line and in a critical wastewater disposal area as determined by the Oahu Wastewater Advisory Committee (Note: all Oahu has been declared a critical wastewater disposal area).

SECTION 3

SUMMARY OF POTENTIAL ENVIRONMENTAL IMPACTS AND MEASURES TO MITIGATE ADVERSE EFFECTS

A. Assessment Process

The project was discussed with the consulting architect, engineer, and staff of the Department of Transportation. State and County agencies were contacted for information relative to their areas of expertise. Time was spent in the field noting site conditions and conditions in the vicinity of the baseyard. The discussions and field investigations allowed us to identify existing conditions and features which could affect or be affected by the project. These conditions are:

- The new baseyard will be constructed on the site of the existing baseyard;
- No change in use is proposed;
- The site is located in a residential area that has been altered by roads, *public infrastructure, and dwellings*;
- There are no rare or endangered flora or fauna on the premises;
- There are no archaeological features on the premises;
- There are no surface water bodies such as streams, lakes, and ponds on or near the site that would be affected by the proposed action;
- The site is not in a flood hazard area; and
- Water, power, and telephone systems are adequate to serve the facility.

B. Short-term Impacts

Prior to demolition, existing water lines will be cut and plugged, electrical and telephone systems disconnected, and the existing cesspool drained and taken out of from service. Plant material will be selectively grubbed, all ac pavement demolished, fencing removed, and the two structures razed. The wooden office building will be tested for asbestos containing materials and lead based paint. If either substance is detected, they shall be properly removed, disposed of in accordance with applicable rules and regulations, and the buildings razed. Demolition and clean up should be completed in one week.

The site will be rough graded to design elevation and the ground excavated for building foundations and utility systems. Demolition and sitework will raise fugitive dust, create noise, and contribute construction traffic to Kamehameha Highway and adjoining streets.

Fugitive dust can and will be controlled by sprinkling water over exposed areas or by the application of other dust suppression measures stipulated in Chapter 60 (Air Pollution Control) of Title 11, Administrative Rules of the State Department of Health.

Construction noise will persist for the projected 8-month construction period. Noise will be most pronounced during the early stages of development (demolition and site work) to erection of the two structures. Construction noise may bother residents living in the vicinity of the property.

Allowable daytime noise levels for residential zoning districts set by the State Department of Health is 55 dBA measured at the property line. Construction work will temporarily exceed this standard and, per Administrative Rules (Chapter 43) of the Department of Health, the Contractor will obtain a noise

permit prior to construction. Construction will be limited to between the hours of 7:00 a.m. to 3:30 p.m., Mondays through Fridays.

Some [O]n-site vegetation will be grubbed in order to site the improvements. Existing stone walls inside the lot on the Kaneohe and mauka sides of the lot will be demolished and rebuilt. Landscape plantings on high ground behind the stone walls may be removed and the area replanted; coconut trees on the makai and Kaneohe side of the lot will remain. All plants and trees on the premises are commonly found on Oahu and none are considered rare or endangered.

Due to the small scale of the project, the small size of the baseyard lot, and the relatively level terrain, the potential for erosion is minimal. Erosion control measures will be identified in an erosion control plan and Best Management Practices (BMPs) to be submitted with grading plans to the Department of Design and Construction, City and County of Honolulu for review and approval. These measures will include gravel ingress/egress pads for construction vehicles and silt fences provided at the perimeter of the site. An estimated 25 cubic yards of material will be used to fill the site to design elevations.

Should subsurface archaeological features be unearthed, work in the immediate area will cease and historic authorities consulted for proper disposition of the finds. *The State Historic Preservation Division recommended that archaeological monitoring be conducted during all ground disturbing activities related to this project. The Department of Transportation will prepare an archaeological monitoring plan that addresses the concerns raised by the State Historic Preservation Division (February 18, 1999). The monitoring plan will be submitted to the State Historic Preservation Division for review and approval before beginning any ground disturbance.*

A temporary construction fence will be erected around the property for security and safety purposes. The fence will be replaced by a permanent fence after construction is completed .

Construction notices will be posted to alert residents and motorists of roadway construction. Flagmen will be posted to marshal vehicles around any excavations in Hauula Homestead Road and during the unloading of material. One traffic lane will be kept open at all times to minimize inconveniences to residents and motorists. Excavations in the roadway will be restored to pre-construction conditions or better. Open trenches will be covered with steel plates at the end of each working day and safety devices posted during night hours.

C. Long-term Impacts

The proposed project will continue the existing use of the property as a highway maintenance baseyard. The structures to be built will serve the same purposes as the existing structures although they will be placed at different locations on the property. The completed project will shelter state vehicles and equipment from the elements and provide space for storing maintenance materials.

No vehicle maintenance (such as oil changes) or repairs will be performed on-site. These activities are performed at the Highway Division's main facility at Shafter Flats near the Mapunapuna Industrial Area in Honolulu.

The proposed project is consistent with existing land use controls for the area. Public uses and structures are permitted uses in the residential district under the Land Use Ordinance, City and County of Honolulu. No change in use and location of the use is proposed.

Manpower levels (seven employees) and operating hours will remain unchanged thus no appreciable increase in daily water consumption, wastewater discharge, traffic, and power usage are expected.

The completed project may "open" a view corridor of the ocean from the property and areas mauka. The new equipment building will be placed on the south side of the property and oriented perpendicular to Kamehameha Highway on its long axis. The ocean can be viewed over the baseyard site but may be partially obstructed from some mauka locations by a stand of kamani trees growing between Kamehameha Highway and the baseyard..

A 3-5 foot wide planting strip inside the baseyard fronting Hauula Homestead Road may be landscaped with screening hedges. Plants requiring low maintenance and water requirements will be considered. The planting strip is too narrow to accommodate trees (Department of Planning and Permitting, 1999 and Hauula Community Association, 1999).

Domestic wastewater from the shower and bathroom facilities will be discharged into an individual wastewater treatment system consisting of a septic tank and leach field. Solid matter will be collected in the septic tank and effluent drained to a leach field. Wastewater flow is estimated at less than 500 gallons per day. This system is preferred over the continued use of cesspools and should minimize the potential for subsurface contamination. The system will be designed and operated in compliance with Chapter 62 (Wastewater Systems), Administrative Rules, State Department of Health.

D. Impacts on Special Management Areas Resources

1. Recreational Resources

The property is neither a shoreline lot nor used for public recreational purposes. There is no public access (or access of any kind) to the shoreline over the subject property.

2. Historic Resources

There are no *known* recorded historic resources on the premises. *Archaeological monitoring will be performed during ground disturbing activities.*

3. Scenic and Open Space Resources

The proposed improvements will not affect coastal scenic and open space resources. The Coastal View Study (Department of Land Utilization, 1987) does not identify any significant stationary view areas mauka of Kamehameha Highway. The Coastal View Study identifies significant stationary views along the coastline from Hauula Beach Park which is located makai of Kamehameha Highway.

4. Coastal Ecosystems

The absence of a diversity of flora and fauna and the improved condition of the site indicates the site does not support valuable coastal ecosystems.

5. Economic Uses

Not Applicable.

6. Coastal Hazards

The building site is located in Flood Zone X which is defined as areas outside the 500-year flood plain. Tsunami inundation maps place the property outside areas prone to inundation from coastal waves.

7. Managing Development

The proposed project will not adversely affect the intensity of land uses on the premises. The site is currently a highway maintenance baseyard where equipment and material for carrying out highway maintenance functions are stored and maintained and where highway maintenance crews report for work. No change in these uses (or other functions at the baseyard) are proposed.

This Environmental Assessment has been prepared to communicate potential short and long term impacts of the proposed project on shoreline resources.

The environmental assessment process is the first step in obtaining a Special Management Area (SMA) Permit. After completing the environmental assessment process, the Department of Planning and Permitting will schedule a public hearing on the project. Hearing notices will be published in the local daily newspaper. Adjoining property owners and lessees will be notified by mail as to the time and place of the hearing.

Special Management Area permits are approved by the City Council. This application will be heard before the City Council Zoning Committee and the City Council. In addition, the Council can also schedule a public hearing if warranted. City Council hearing procedures provide the public ample opportunities to offer comments on the application.

D. Special Management Area Review Guidelines

The following guidelines are used by the Honolulu City Council or its designated agency for the review of development in the special management area:

- a) All development in the special management area shall be subject to reasonable terms and conditions set by the council to ensure that:
 - 1) Adequate access, by dedication or other means, to publicly owned or used beaches, recreation areas and natural reserves is provided to the extent consistent with sound conservation principles;
 - 2) Adequate and properly located public recreation areas and wildlife preserves are reserved;
 - 3) Provisions are made for solid and liquid waste treatment, disposition and management which will minimize adverse effects upon special management areas resources; and
 - 4) Alterations to existing land forms and vegetation, except crops, and construction of structures shall cause minimum adverse effect to water resources and scenic and recreational amenities and minimum danger of floods, landslides, erosion, siltation or failure in the event of earthquakes.

- b) No development shall be approved unless the council has first found that
- 1) The development will not have any substantial, adverse environmental or ecological effect except as such adverse effect is minimized to the extent practicable and clearly outweighed by public health and safety, or compelling public interest. Such adverse effect shall include, but not be limited to, the potential cumulative impact of individual developments, each one of which taken in itself might not have a substantial adverse effect and the elimination of planning options;
 - 2) The development is consistent with the objectives and policies set forth in Section 25.3.1 and area guidelines contained in HRS Section 205A-26;
 - 3) The development is consistent with the county general plan, development plans and zoning. Such a finding of consistency does not preclude concurrent processing where a development plan amendment or zone change may also be required.
- c) The council shall seek to minimize, where reasonable:
- 1) Dredging, filling or otherwise altering any bay, estuary, salt marsh, river mouth, slough or lagoon;
 - 2) Any development that would reduce the size of any beach or other area suitable for public recreation;
 - 3) Any development which would reduce or impose restrictions upon public access to tidal and submerged lands, beaches, portions of rivers and streams within the special management area and the mean high tide line where there is not beach;
 - 4) Any development which would substantially interfere with or detract from the line of sight toward the sea from the state highway nearest the coast; and
 - 5) Any development which would adversely affect water quality, existing areas of open water free of visible structures, existing and potential fisheries and fishing grounds, wildlife habitats, or potential or existing agricultural uses of land.

Discussion:

The subject property is not a shoreline lot and development will not affect the shoreline, public access to the shoreline, or shoreline recreation areas. Dredging or other significant land alterations are not proposed. The baseyard is located mauka of Kamehameha Highway, the state highway nearest the coast, and the new structures will not interfere with views of the shoreline from Kamehameha Highway. Siting the proposed equipment building on the south side of the lot may "open" views of the shoreline from mauka areas.

The project is not proposed in an area of open waters, potential fisheries and fishing grounds, and wildlife habitats.

No alterations to the existing on-site drainage pattern are proposed.

A septic tank and leach field system of wastewater disposal will replace the existing cesspool. Because there is no sanitary sewer in the community of Hauula, the proposed disposal system will have the least impact on the environment and nearby coastal waters.

The proposed Hauula Baseyard is consistent with the residential zoning of the area. Public uses and structures are a permitted use of R-5 Residential zoned land.

SECTION 4

ALTERNATIVES TO THE PROPOSED ACTION

A. No Action

The no action alternative would maintain the status quo of the building site and preclude the occurrence of all environmental impacts, short and long-term, beneficial and adverse described in this Assessment.

B. Alternative Sites

The State of Hawaii has no alternative sites available in the Hauula area on which to build a new baseyard.

SECTION 5

PERMITS AND APPROVALS

Permits and approvals required for the project are indicated below. Other permits and approvals may be required depending on final construction plans.

<u>PERMIT/APPROVAL</u>	<u>AUTHORITY</u>
State of Hawaii Variance From Pollution Controls	Department of Health
City and County of Honolulu Special Management Area Use Permit	City Council
Grubbing, Grading, and Stockpiling Permit	Department of Planning and Permitting
Building Permit for Building, Electrical, Plumbing Sidewalk/Driveway and Demolition Work	Department of Planning and Permitting
Permit to Excavate Public Right-of-Way (Trenching)	Department of Planning and Permitting

SECTION 6

AGENCIES AND ORGANIZATIONS TO BE CONSULTED

*Notice of the Draft Environmental Assessment for the Hauula Baseyard Improvements was published in the Office of Environmental Quality Control Environmental Notice of February 8, 1999 and February 23, 1999. Copies of the Draft Environmental Assessment were mailed to the agencies and organizations listed below. Publication in the Environmental Notice initiated a 30-day public comment period which ended on March 10, 1999. An asterisk * identifies agencies and organizations that submitted written comments to the Draft Environmental Assessment. Comment letters and responses are founded in Appendix A of the Final Environmental Assessment.*

Federal

US Army Corps of Engineers

State

Department of Land and Natural Resources

**State Historic Preservation Division*

**Land Division*

Department of Hawaiian Home Lands

Department of Health

City and County of Honolulu

**Board of Water Supply*

Department of Design and Construction

**Department of Planning and Permitting*

Department of Transportation Services

Planning Department

Police Department

Fire Department

Other

Hawaiian Electric Company

GTE Hawaiian Telephone Company

**Hauula Community Association*

Koolauloa Neighborhood Board No. 28

Councilman Steve Holmes

**Janine Brand*

SECTION 7

DETERMINATION OF SIGNIFICANCE

Chapter 200 (Environmental Impact Statement Rules) of Title 11, Administrative Rules of the State Department of Health, establishes criteria for determining whether an action may have significant effects on the environment (§11-200-12). The relationship of the proposed project to these criteria is discussed below.

- 1) Involves an irrevocable commitment to loss or destruction of any natural or cultural resource;

There are no *known* natural or cultural resources on the premises to be affected by the proposed action. *Archaeological monitoring will be performed during ground disturbing activities.*

- 2) Curtails the range of beneficial uses of the environment;

The project does not curtail the beneficial uses of the environment.

- 3) Conflicts with the state's long-term environmental policies or goals and guidelines as expressed in chapter 344, Hawaii Revised Statutes, and any revisions thereof and amendments thereto, court decisions or executive orders;

The project will not conflict with long-term environmental policies, goals, and guidelines of the State of Hawaii.

- 4) Substantially affects the economic or social welfare of the community or State;

The project will not substantially affect the economic or social welfare of the State. Short-term economic benefits will be derived in the form of construction wages and material purchases.

- 5) Substantially affects public health;

Public health will not be adversely affected by the proposed project.

- 6) Involves substantial secondary impacts, such as population changes or effects on public facilities;

Substantial secondary impacts are not anticipated.

- 7) Involves a substantial degradation of environmental quality;

Environmental quality of the site and surrounding neighborhood will not be degraded.

- 8) Is individually limited but cumulatively has considerable effect upon the environment or involves a commitment for larger actions;

The proposed action does not involve a commitment for larger actions.

- 9) Substantially affects a rare, threatened or endangered species, or its habitat;

There are no rare, threatened or endangered flora or fauna on the premises.

- 10) Detrimentially affects air or water quality or ambient noise levels; or

Ambient air quality will be affected by fugitive dust and combustion emissions during construction but can be controlled by measures stipulated in this Assessment. Construction noise will be pronounced during site preparation work but should diminish once the buildings are erected. All construction activities will comply with air quality and noise pollution regulations of the State Department of Health.

Erosion control measures will be prescribed in grading plans and best management practices prepared for the project.

- 11) Affects or is likely to suffer damage by being located in an environmentally sensitive area such as a flood plain, tsunami zone, beach, erosion-prone area, geologically hazardous land, estuary, fresh water, or coastal waters.

The Hauula Baseyard is not located in a flood hazard area or tsunami inundation zone.

- 12) Substantially affects scenic vistas and view planes identified in county or state plans or studies, or:

The Hauula Baseyard will not affect scenic vistas along the coastline identified in the Coastal View Study prepared for the Department of Land Utilization (now Department of Planning and Permitting).

- 13) Requires substantial energy consumption.

Energy consumption has not been determined. It is anticipated that power consumption may increase slightly because of air conditioning to be installed in the office building.

Based on the above criteria, the Hauula Baseyard Improvements project will not result in significant adverse environmental impacts and an Environmental Impact Statement should not be required.

REFERENCES

Chu, Michael S. and Robert B. Jones. 1987. *Coastal View Study*. Prepared for City and County of Honolulu, Department of Land Utilization.

Federal Emergency Management Agency. 1987. *Flood Insurance Rate Map*. Community Panel No. 150001 0031B.

Planning Department, City and County of Honolulu. 1983. *Koolauloa Development Plan Land Use Map and Public Facilities Map*. Ordinance No. 83-9.

U.S. Department of Agriculture, Soil Conservation Service. 1972. *Soil Survey Report for Islands of Kauai, Oahu, Maui, Molokai, and Lanai, State of Hawaii*. In Cooperation with the University of Hawaii Agricultural Experiment Station.

APPENDIX A

COMMENTS AND RESPONSES

DEPARTMENT OF PLANNING AND PERMITTING
CITY AND COUNTY OF HONOLULU

840 SOUTH KING STREET • HONOLULU, HAWAII 96813
PHONE: (808) 522-4116 • FAX: (808) 527-9742



JEREMY HARRIS
MAILER

JAN NAOE SULLIVAN
DIRECTOR
LOGISTICAL & CHECK
SERVICES SECTION

February 19, 1999

1999/SMA-6(DT)



Mr. Gerald Park
Gerald Park Urban Planner
1400 Rycroft Street, Suite 876
Honolulu, Hawaii 96814-3021

Dear Mr. Park:

Comments To Draft Environmental Assessment (EA)
Hauula Baseyard
307 Hauula Homestead Road, Hauula, Oahu
Tax Map Key: 5-4-21_12

The following comments should be implemented in the final EA:

1. Two site plans should be added to the final EA. One site plan should show the proposals, including the septic tank and leach field. Another site plan should show the existing structures on the property. The grades can be omitted from the site plans.
2. A landscaping plan should be added to the final EA. Will the parking lot be screened from view from the road with canopy trees and screening hedges?
3. The method of solid waste disposal to be utilized after the project is completed and the location where the solid waste will be disposed of should be included in the final EA.
4. Where will the debris from the demolition of the existing structures be disposed?
5. Page 15 of the EA mentions that the project will contain an erosion control plan with Best Management Practices (BMPs). The types of BMPs that will be implemented during grading and construction should be mentioned in the final EA.

Mr. Gerald Park
Page 2
February 19, 1999

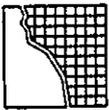
Should you have any questions regarding this letter, please call Dana Teramoto of our staff at 523-4648.

Very truly yours,

JAN NAOE SULLIVAN
Director of Planning
and Permitting

JNS:am

please doc no. 2443



GERALD PARK
Urban Planner

Planning
Land Use
Research
Environmental
Studies

1400 Sport Street
Suite 676
Honolulu, Hawaii
96814-3021

Phone/fax
808 947-7484

April 2, 1999

Jan Nease Sullivan, Director
Department of Planning and Permitting
City and County of Honolulu
650 South King Street
Honolulu, Hawaii 96813

Dear Mr. Sullivan:

Subject: Maunaloa Baseyard Improvements
TMK: 5-4-02: 12
Maunaloa, Koolauloa, Oahu

Thank you reviewing and commenting on the Draft Environmental Assessment prepared for the subject project. We offer the following responses to your comments.

1. The septic tank, leach field, and existing structures are shown on Exhibit 2, Site and Utility Plan.
2. A landscaping plan has not been prepared for the project. The Department of Transportation plans to keep the coconut trees along the makai and Kaeohe sides of the lot. As shown on the project site plan, existing stone walls inside the lot on the Kaeohe and maunaloa sides will be retained as will the landscape plantings planted on "high ground" behind the stone walls. The project site plan also shows a 3-5-foot wide planting strip inside the yard fronting the street. There are no plans to plant trees in the planting strip because there is no suitable space for them. Screening hedges could be planted along the fence perhaps by Department of Transportation Highways (HDOT) maintenance personnel.
3. All solid waste generated by activities at the baseyard are currently collected by Department of Transportation Highways Division maintenance personnel and transported to the Kapapa/Kailua Sanitary Landfill for proper disposal. This practice will continue when the new baseyard is operational.
4. The Contractor will be responsible for the proper disposal of demolition debris.
5. Due to the small scale of the project, the small size of the baseyard lot, and the relatively level terrain, the potential for erosion is minimal. Erosion controls, however, will include gravel ingress/egress pads for construction vehicles and silt fences provided at the perimeter of the site.

We thank the Department of Planning and Permitting for participating in the environmental review process. Your comments and our responses will be included in the Final Environmental Assessment.

Jan Nease Sullivan
April 2, 1999
Page 2

Sincerely,

GERALD PARK URBAN PLANNER

Gerald Park

Gerald Park

c: P. Chan, DOTH

BOARD OF WATER SUPPLY
CITY AND COUNTY OF HONOLULU
630 SOUTH BERETANIA STREET
HONOLULU, HAWAII 96843
PHONE (808) 537-2714
FAX (808) 533-2714

99 FEB 24 AM 8:07



DEPT. OF PLANNING
630 SOUTH BERETANIA STREET
CITY AND COUNTY OF HONOLULU
January 19, 1999

JERRY HARPER, Mayor
EDOE FLORTEL, JR., Chairman
FOREST C. MURPHY, Vice Chairman
EAO HAWAIIHOLA
JAN H.L.Y. AUM
BARBARA ILM STANTON
CHARLES A. STEB

CLIFFORD S. JAMILE
Manager and Chief Engineer

TO: MS. JAN NAOE SULLIVAN, DIRECTOR
DEPARTMENT OF PLANNING AND PERMITTING

FROM: *Clifford S. Jamile*
CLIFFORD S. JAMILE

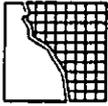
SUBJECT: YOUR MEMORANDUM OF JANUARY 27, 1999 REGARDING THE
DRAFT ENVIRONMENTAL ASSESSMENT FOR THE PROPOSED HAULULA
BASEYARD IMPROVEMENTS, HAULULA, OAHU, TMK: 5-4-02: 12

Thank you for the opportunity to review and comment on the Draft Environmental Assessment for the proposed Haulula Baseyard Improvements.

We have the following comments:

1. The existing water system is presently adequate to accommodate the proposed baseyard improvements.
2. The applicant will be required to obtain a water allocation from the Department of Land and Natural Resources.
3. There is currently a 3/8-inch water meter (Premise I.D. Number 1037095) and a 2-inch water meter (Premise I.D. Number 1037094) serving the project site.
4. The availability of water will be confirmed when the building permit application is submitted for our review and approval. When water is made available, the applicant will be required to pay our Water System Facilities Charges for transmission and daily storage.
5. If a three-inch or larger meter is required, the construction drawings showing the installation of the meter should be submitted for our review and approval.
6. The proposed project is subject to Board of Water Supply cross-connection control requirements prior to the issuance of the building permit application.

If you have any questions, please contact Barry Usagawa at 527-5235.



GERALD PARK
Urban Planner

Planning
Land Use
Research
Environmental
Studies

1400 Kapaeha Street
Suite 210
Honolulu, Hawaii
96814-3021

Phone/Fax
808 942-7484

April 2, 1999

Clifford S. Jamile
Manager and Chief Engineer
Board of Water Supply
City and County of Honolulu
630 South Beretania Street
Honolulu, Hawaii 96843

Dear Mr. Jamile:

Subject: Haulula Baseyard Improvements
TMK: 5-4-02: 12
Haulula, Koolauloa, Oahu

Thank you for reviewing the Draft Environmental Assessment for the subject project. We have passed on your comments to the State Department of Transportation and its consultants for consideration in preparing a water system plan for the baseyard.

We thank the Board of Water Supply for participating in the environmental assessment review process.

Sincerely,

GERALD PARK URBAN PLANNER

Gerald Park
Gerald Park

c: P. Chan, DOTH

BERNARD L. CAULFIELD
GOVERNOR OF HAWAII



1999 MAR 2 PM 1 43

DEPARTMENT OF LAND AND NATURAL RESOURCES
CITY & COUNTY OF HONOLULU
HISTORIC PRESERVATION DIVISION
201 Kalia Road, Room 505
Honolulu, Hawaii 96815

MICHAEL S. WELLS, CHIEF DIVISION
DIVISION OF LAND AND NATURAL RESOURCES
SERVING
DEPUTY COMMISSIONER
THOMAS L. JONES
AGRICULTURE
NATIVE RESOURCES
NATURAL RESOURCES
CONSERVATION AND RESTORATION
ENVIRONMENT
FORESTRY AND WILDLIFE
HISTORIC PRESERVATION
LAND AND
WATER RESOURCES TECHNOLOGY

February 18, 1999

Jan Naoe Sullivan, Director
Department of Planning and Permitting
City and County of Honolulu
650 South King Street, 7th Floor
Honolulu, Hawaii 96813

Dear Ms. Sullivan:

SUBJECT: Chapter 6E-8 Historic Preservation Review - Draft Environmental Assessment for the Hauula Baseyard Improvements, 307 Hauula Homestead Road Hauula, Ko'olaupua, O'ahu
IMK: 5-4-002:012

LOG NO: 23056 ✓
DOCNO:9902EJ18

Thank you for the opportunity to review this project which proposes demolition of the existing Hauula Baseyard structures (built in 1971) and construction of a new baseyard at the same site. A review of our records shows that there are no known historic sites at the project location. However, subsurface historic sites have been identified along Kamehameha Highway and within Hauula Beach Park on both sides of the baseyard. These sites, (SIHP 50-80-06-4794 and 4795) are habitation and burial site areas and are considered significant for the information they have or are likely to yield which is important to the history and prehistory of the area. They also have cultural significance due to the presence of human burials.

This parcel was graded during construction of the existing baseyard in 1971 and is at a lower elevation than adjoining lots. However, it is unclear whether previous grading would have disturbed subsurface cultural deposits. The current project proposes additional grading and ground excavation for building foundations, utility systems, septic tank and a leach field. Therefore, we recommend that archaeological monitoring be conducted during all ground disturbing activities related to this project.

An acceptable archaeological monitoring plan should be prepared and submitted to this office for review and acceptance prior to beginning any ground disturbance. In addition, provisions must be made for the treatment of human burial remains discovered inadvertently during routine construction activities.

Jan Naoe Sullivan, Director
Page Two

An archaeological monitoring plan must contain the following eight specifications: 1) The kinds of resources that are anticipated; 2) Where in the construction area the resources are likely to be found; 3) How the expected types of resources will be treated, if found; 4) The archaeologist conducting the monitoring has the authority to halt construction in the immediate area of a find in order to carry out the plan; 5) A coordination meeting between the archaeologist and construction crew is scheduled, so that the construction team is aware of the plan; 6) What laboratory work will be done on resources that are collected; 7) A schedule for report preparation; and 8) Details concerning the archiving of any collections that are made.

If an acceptable archaeological monitoring plan is prepared which addresses these concerns, and if the plan is implemented, then we can concur with your determination that the proposed undertaking will have "no adverse effect" on significant historic sites which may be present in the project area.

If you have any questions please call Sara Collins at 692-8026 or Elaine Jourdane at 692-8027.

Honolulu,

Don Hibbard, Administrator
State Historic Preservation Division

EJ:jk



April 16, 1999

GERALD PARK
Urban Planner

Don Hibbard, Administrator
State Historic Preservation Division
Department of Land and Natural Resources
State of Hawaii
555 Kakahelewa Building
691 Kamohila Boulevard
Kapolei, Hawaii 96707

Dear Mr. Hibbard:

1400 Byron Street
Suite 876
Honolulu, Hawaii
96814-3021

Subject: Haunala Backyard Improvements
TMK: 5-4-02: 12
Haunala, Koolauloapoko, Oahu

Phone/Fax
(808) 942-7484

Thank you reviewing and commenting on the Draft Environmental Assessment prepared for the subject project. We offer the following response to your comments.

The Department of Transportation ("DOT") will prepare an archaeological monitoring plan as recommended by the State Historic Preservation Division. Due to project scheduling and funding constraints, the DOT does not anticipate having an archaeological monitoring plan completed prior to circulation of the Final Environmental Assessment.

An archaeological monitoring plan, however, will be prepared and submitted to your office for review and approval prior to beginning any ground disturbance. A final report will be submitted for your review and acceptance after completion of the monitoring fieldwork.

We thank the State Historic Preservation Division for participating in the environmental assessment review process. Your comments and our response will be included in the final environmental assessment.

Sincerely,

GERALD PARK URBAN PLANNER

Gerald Park

c: P. Chan, DOT

'99 MAR 2 AM 7 59

CITY & COUNTY OF HONOLULU
STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES
LAND DIVISION
P. O. BOX 521
HONOLULU, HAWAII 96809



AGRICULTURE DEVELOPMENT
PROGRAMS
AQUATIC RESOURCES
BURNING AND OCEAN RECLAMATION
CONSERVATION AND
RESTORATION
CONSTRUCTION
COUNCILS
FORESTRY AND WILDLIFE
HISTORIC PRESERVATION
LAND USE
STATE PARKS
WATER RESOURCE MANAGEMENT

FEB 26 1999

REP: PS:EH

Ms. Jan Naeve Sullivan, Director
Department of Planning and Permitting
City and County of Honolulu
650 South King Street
Honolulu, Hawaii 96813

Dear Ms Sullivan:

Subject: Draft Environmental Assessment (DEA) for Hauula
Baseyard Improvements

We have reviewed the subject DEA and offer the following comments
for your consideration.

Our current projects are not affected by the proposed project.

We recommend that the water requirements for the proposed project
be coordinated with the Land Division, Department of Land and
Natural Resources if additional water is required for the
proposed improvements.

We confirm that the proposed project is located in Zone X. This
is an area determined to be outside the 500-year flood plain.

Thank you for the opportunity to review this matter.

Very truly yours,

Alvin Uchida
Dean Uchida,
Administrator

c.c. Engineering

1999/0006-1602

HAU'ULA COMMUNITY ASSOCIATION

P.O. Box 164
Hau'ula, Hawaii 96717

March 2, 1999

City & County of Honolulu
Department of Planning and Permitting
600 South King Street
Honolulu, Hawaii 96813

Attn: Jan Naeve Sullivan, Director

Re: Draft EA for Hau'ula Baseyard Improvements

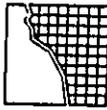
At the regular meeting of the Hau'ula Community Association (HCA) held on March 2, 1999, community members discussed the plans for the Hau'ula baseyard improvements as outlined in the draft EA. Our comments are:

1. Please make provisions for proper care and maintenance of the plants on the property. For example, the existing coconut trees should be replaced. They are old and have not been cared for.
2. Consider the use of landscaping to preserve water and minimize maintenance.
3. Fill up the water trucks inside the property, not in the driveway or on the street.
4. The overall appearance of the facility should be designed to fit into a rural environment.
5. You should work closely with surrounding property owners to create a compatible appearance. For example, check with the Parks Department to coordinate plantings for the baseyard and Hau'ula park.
6. Work with the adjoining church to ensure plants and appearance are acceptable to the community.
7. The entrance to the facility must be improved so that there is sufficient room for vehicle entry and exit from the property. Complaints were made that large vehicles have been seen driving onto property across from the existing entrance.

Thank you.

Barbara Kahana

Barbara Kahana, Vice President of HCA
cc: Councilman Steve Holmes
Neighborhood Board # 28



GERALD PARK
Urban Planner

Barbara Kahana
Vice President
Hau'ula Community Association
PO Box 164
Hau'ula, Hawaii 96717

Dear Ms. Kahana:

Subject: Hau'ula Baseyard Improvements
TMK: 5-4-02-12
Hau'ula, Koolauloa, Oahu

1400 Kuyot Street
Suite 676
Honolulu, Hawaii
96814-3021

Thank you reviewing and commenting on the Draft Environmental Assessment prepared for the subject project. We offer the following responses to your comments.

1. The Department of Transportation Highways Division will replace the diseased or unhealthy trees only if they are considered to be unique or significant to the surrounding environment.
2. Plants with low maintenance and water requirements will be considered.
3. A facility to fill water trucks inside the baseyard will be provided.
- 4-6. The new office building and equipment shed are designed to conform with local zoning codes and to provide for the functional needs of the personnel and equipment assigned to the baseyard. The height, scale, and exterior appearance of the structures have been designed in consideration of the surrounding low-rise dwellings and residential character of the neighborhood.
7. A 20-foot wide entry gate into the baseyard is proposed. The wider entry will provide better truck access to and from Hau'ula Homestead Road.

Your comments and our responses will be included in the Final Environmental Assessment. We thank the Hau'ula Community Association for participating in the environmental assessment review process.

Sincerely,

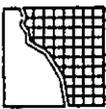
GERALD PARK URBAN PLANNER

Gerald Park

Gerald Park

c: P. Chan, DOTH

SENT BY STATE DOT, HIGHWAYS : 3-31-88 : 3:34AM : OAHU DISTRICT-1088427485 : 2



GERALD PARK
Urban Planner

- Planning
- Land Use Research
- Environmental Studies
- 1400 Konoct Street, Suite 676, Honolulu, Hawaii 96814-3021
- Phone/Fax: 808/942-7484

April 2, 1999
Janine Brand
54-033 Kamehameha Highway
Hanalei, Hawaii 96717

Dear Ms. Brand:
Subject: Hanalei Bayside Improvements
TMK: 5-4-02: 12
Hanalei, Koolauloa, Oahu

Thank you for your interest in the proposed project. As a result of your comment concerning the perimeter fence, the Department of Transportation is considering several design alternatives and their respective cost. The design should be functional and the wall or fence should not require frequent upkeep. One alternative is a 4-foot high CMU wall topped with a 4-foot high chain link fence without the barbed wire. The finish of the wall would match the exterior finish of the proposed structures for continuity in design.

A revised wall/fence design will be incorporated into the project plans and can be made available upon written request. Please contact Mr. Peter Chan of the Department of Transportation at 837-8046 for information about the project.

Sincerely,

GERALD PARK URBAN PLANNER

Gerald Park
Gerald Park

c: P. Chan, DOTH
B. Kahana, HCA

DIRECTOR'S OFFICE
DEPT. OF
TRANSPORTATION

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COMMUNITY DEVELOPMENT

Janine Brand
54-033 Kamehameha Highway
Hanalei, Hawaii 96717
Phone 293-5022 (Bus)
593-2533 (Bus)

March 10, 1999

Barbara Kahana, Vice President
Hanalei Community Association
P O Box 264
Hanalei HI 96717

Dear Barbara:

I am writing regarding the Hanalei Bayside Improvements project. First, thank you for coordinating our Community Association response. I was impressed with your written preparation for the Association meeting; your summary was informative and well presented. It sounds as if you elicited a good discussion among those attending.

In your March 3 letter to Jan Sullivan you list 7 comments from the Association. These points fully reflect my thoughts about the project. I would like to add one thought:

I believe that an 8 foot high chain link fence topped with 3 strands of barbed wire erected around the perimeter is neither compatible with the location of the bayside (along the highway access from the beach park) nor with the pleasant country character of Hanalei. (Chain link and particularly barbed wire bring to mind prisons/jails.) It is possible the Department of Planning and Permitting could request that the Transportation Department-Highways Division be more creative in the "design" phase. A rock wall with night blooming cereus or some other thorny plant along the top of the wall would be an attractive addition to our Hanalei Highway Corridor.

Again, Barbara, thanks for your efforts. I will assist in any way I can. Please call on me.

Sincerely,

Janine Brand
Janine Brand

cc: Steve Holmes, Councilmember Honolulu City Council
Jan Sullivan, Director Department of Planning and Permitting
Craigston Maitou, Chair Koolauloa Neighborhood Board
Kane Hayashida, Director Transportation Department