

BENJAMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

July 28, 1998

Kalaka'ua Ave. Bridge Imp.

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINA'AI
GLENN M. OKIMOTO

IN REPLY REFER TO:

HWY-DS
2.0291

TO: GARY GILL, DIRECTOR
OFFICE OF ENVIRONMENTAL QUALITY CONTROL

FROM: KAZU HAYASHIDA, DIRECTOR *←*
DEPARTMENT OF TRANSPORTATION

SUBJECT: FINDING OF NO SIGNIFICANT IMPACT FOR
KALAKAUA AVENUE BRIDGE IMPROVEMENTS
TMK 2-3-34
WAIKIKI, OAHU, HAWAII

OFFICE OF
QUALITY CONTROL

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We have reviewed the Draft Environmental Assessment comments received during the 30-day public comment period which began on April 23, 1998. We have determined that this project will not have significant environmental effects and have issued a FONSI. ✓

Please publish this notice in the August 23, 1998, OEQC Environmental Notice.

We submit the following enclosed items for the Kalakaua Avenue Bridge Improvements, State DOT Project No. HWY-O-13-98:

- Four (4) copies of the Final Environmental Assessment.
- One (1) completed OEQC publication form.
- One (1) diskette file of the updated project summary and FONSI determination.

Should you have questions concerning the above, please call Edmund Yoshida at 587-2122.

Enclosures

90

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1998-08-23-0A- FEA- Kalakaua Avenue
Bridge Improvements

AUG 23 1998

FILE COPY

FINAL ENVIRONMENTAL ASSESSMENT for the

**KALAKAUA AVENUE
BRIDGE IMPROVEMENTS
Waikiki, Oahu, Hawaii
T.M.K. 2-3-34**

AUGUST 1998

PREPARED FOR:

**State of Hawaii
Department of Transportation
Highways Division**

PREPARED BY:

**Akinaka & Associates, Ltd
250 North Beretania Street, #300
Honolulu, Hawaii 96817**

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RECEIVED

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1 PROJECT SUMMARY

The State of Hawaii Department of Transportation-Highways Division proposes to construct a new pedestrian bridge and sidewalk improvements adjacent to the existing Kalakaua Avenue Bridge, near the intersection of Kalakaua Avenue and Ala Wai Boulevard.

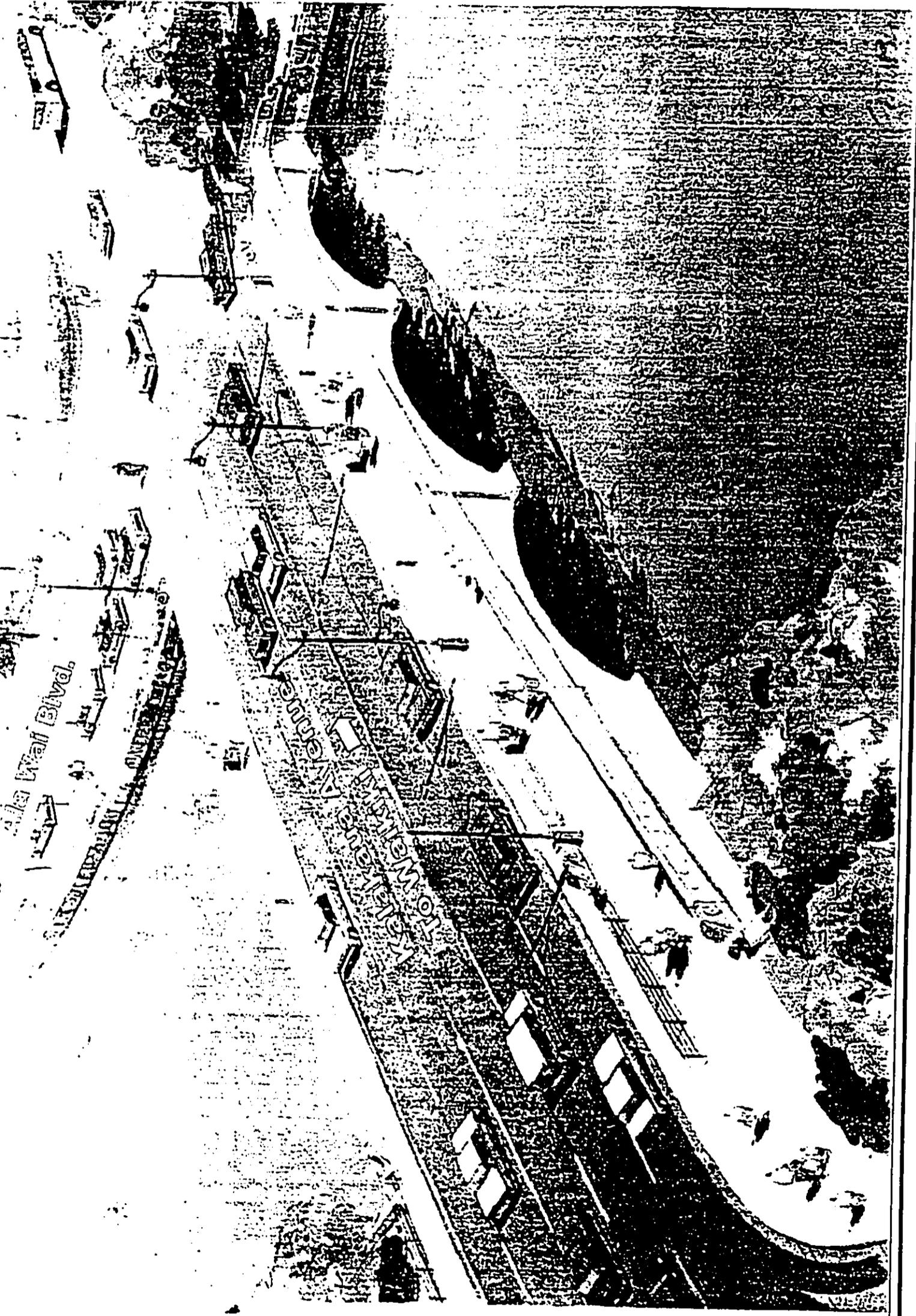
The purpose of this project is to:

- Increase safety for pedestrians traveling along Kalakaua Avenue across the Ala Wai Canal. The new bridge will increase safety by moving pedestrian traffic away from the vehicle travel lanes crossing the existing Kalakaua Avenue Bridge.
- Reconstruct portions of the existing sidewalk at the Kalakaua Avenue-Ala Wai Boulevard intersection to conform with the Americans with Disabilities Act. The project will reconstruct the sidewalk at the southwest corner of the intersection to conform with Americans with Disabilities Act slope requirements.
- Improve pedestrian access along Kalakaua Avenue crossing the Ala Wai Canal, between Waikiki and the Hawaii Convention Center.

The new pedestrian bridge will be a three-span concrete structure approximately 180 feet long and 14 feet wide. The new bridge will abut the seaward side of the existing Kalakaua Avenue Bridge and will have the same profile and architectural design as the existing bridge. A rendering of the new bridge is shown in Figure 1-1.

The estimated construction cost is in the range of \$5 million. The start of construction is pending funding availability and is undetermined at this time.

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KALAKAUA AVENUE BRIDGE IMPROVEMENTS

FIGURE

PROJECT RENDERING

1-1

2 AGENCY DETERMINATION

Pursuant to Hawaii Administrative Rules Title 11 Chapter 200, the State Department of Transportation determines that the proposed action will not have significant environmental effects and has issued a Finding of No Significant Impact (FONSI).

The State Department of Transportation has evaluated the 13 significance criteria cited in Section 11-200-12 to reach this FONSI. The following numbered items first identify the significance criteria then give the State's reasons for supporting the FONSI.

I. Involves an irrevocable commitment to loss or destruction of any natural or cultural resources.

The construction of the pedestrian bridge does not involve an irrevocable commitment to loss or destruction of any natural or cultural resources.

- The cultural resources within the project area are the items having historical significance. These are the existing Kalakaua Avenue Bridge, the Ala Wai Promenade, and the Ala Wai Canal and the stonewalls lining the canal. The State of Hawaii Department of Land and Natural Resources Historic Preservation Division was consulted in preparing the design of this project. The State Historic Preservation Division has determined that this project causes no adverse effect to the historical character of the Kalakaua Avenue Bridge, and no effect to the historical character of the Ala Wai Promenade and the Ala Wai Canal and its stonewalls.
- The natural resources within the project area are the existing "exceptional" banyan trees along the Ala Wai Boulevard and the Ala Wai Promenade. The City and County of Honolulu Department of Parks and Recreation's Arborist Advisory Committee and The Outdoor Circle have been consulted to develop appropriate treatment and protection measures for these trees within the project area. Impacts to the trees will consist of selective trimming of canopies and root pruning. To minimize other impacts to the

trees during the construction period, protective barricades and root protection will be installed, and the trees will be provided watering. These efforts should prevent inadvertent damage to the trees.

2. Curtails the range of beneficial uses of the environment.

The project does not curtail the range of beneficial uses of the environment.

- When completed, the pedestrian bridge and sidewalk improvements will provide an increase in beneficial uses of the environment for pedestrians by providing improved pedestrian safety and conformance with ADA regulations.
- Although there will be impacts to water recreational activities during the construction period, these impacts will be temporary only. Construction equipment and work platforms placed upon the canal waters will impact the passage of canoe paddlers, kayakers, boaters and other canal users through the construction area as they travel to and from the ocean and other parts of the Ala Wai Canal.

The DOT has met with the groups participating in water activities to discuss the construction impacts to those activities. In general, these groups comment that at least one of the three bridge spans should be available to their activities for passage through the construction area. Based on this comment, the DOT will direct the project contractor to allow such passage through the construction area by the water activities, except when overhead lifting or other work causes safety hazards. The DOT will further direct the contractor to coordinate the work schedule with the water activities, post appropriate warning signs, and provide advance notice of the times that safety hazards will prevent passage through the work area.

- After the construction is completed, water activities on the canal will not be curtailed due to this project. Since the pedestrian bridge will have the same profile and clearances beneath its spans as the existing Kalakaua Avenue Bridge, the passage of canal users beneath the pedestrian bridge will not be affected.

3. Conflicts with the State's long-term environmental policies or goals and guidelines as expressed in Chapter 344 of the Hawaii Revised Statutes, and any revisions thereof and amendments thereto, court decisions, or executive orders.

The project does not conflict with the State's long-term environmental policies or goals and guidelines as expressed in Chapter 344 of the Hawaii Revised Statutes.

- In brief, the State's environmental policy is to conserve the natural resources and enhance the quality of life.
- The project is consistent with the State policy and its guidelines in that the nearby natural resources, the "exceptional" banyan trees, are protected during the construction period and in that the pedestrian safety improvements enhance the quality of life.

4. Substantially affects the economic or social welfare of the community or State.

The project does not substantially affect the economic or social welfare of the community or the State.

- In general, the project is a safety improvement responding to concerns generated in part by the surrounding land uses and the Hawaii Convention Center. This improvement has negligible effect upon economic or social welfare.

- The construction of the pedestrian bridge and the sidewalk improvements do not cause substantial effects either in the construction employment created, the economic activity generated, or the potential for inducing growth or land use changes in the existing community.

5. Substantially affects public health.

The project does not substantially affect public health.

- Upon its completion, the construction of the pedestrian bridge and the sidewalk improvements will not cause effects to public health.
- The project will generate temporary noise, dust, and water quality impacts during the construction period. Measures to minimize these impacts will be implemented during the construction period. In addition, the contractor will be directed to communicate with the community to mitigate public concerns during construction. These impacts are identified in this environmental assessment document and are addressed through the permit processes established by the government regulatory agencies.

6. Involves substantial secondary impacts, such as population changes or effects upon public facilities.

The project does not involve substantial secondary impacts, such as population changes or effects upon public facilities.

- The project is a safety improvement.
- The construction of the pedestrian bridge and sidewalk improvements will not result in population changes, effects upon public facilities, or other substantial secondary impacts.

7. Involves a substantial degradation of environmental quality.

The project does not involve a substantial degradation of environmental quality.

- During the construction period, the work activities will generate temporary noise, dust, and water quality impacts. These impacts are not anticipated to cause a substantial degradation of environmental quality. Measures to minimize these temporary impacts will be implemented during the construction period.
- After the construction is completed, the project will not involve a substantial degradation of environmental quality.

8. Is individually limited but cumulatively has considerable effect on the environment or involves a commitment for larger actions.

The project does not have a cumulative considerable effect on the environment and does not involve a commitment for larger actions.

- The construction activities will cause temporary noise, dust, and water quality impacts which are not anticipated to have a considerable effect upon the environment. These temporary impacts will cease when the construction is completed.
- The project is a safety improvement and does not commit larger actions.

9. Substantially affects a rare, threatened, or endangered species, or its habitat.

The project does not substantially affect any rare, threatened, or endangered species, or its habitat.

- There are no rare, threatened, or endangered species, or their habitats, existing within the project area.

10. Detrimently affects air or water quality or ambient noise levels.

The project does not involve a substantial degradation of environmental quality.

- During the construction period, the work activities will generate temporary noise, dust, and water quality impacts. Mitigative measures to minimize these temporary impacts will be implemented during the construction period.
- After the construction is completed, the pedestrian bridge and the sidewalk improvements will not detrimentally affect air or water quality or ambient noise levels.

11. Affects or is likely to suffer damage by being located in an environmentally sensitive area such as a flood plain, tsunami zone, beach, erosion-prone area, geologically hazardous land, estuary, fresh water, or coastal waters.

The project does not affect and is not likely to suffer damage by being located in an environmentally sensitive area.

- The project is not located within a State Conservation District nor a Special Management Area.
- The project is located within the AO Zone shown on the Flood Insurance Rate Map Panel No. 150001-0120C dated September 4, 1987. The effects of the project construction upon the base flood elevation is negligible in both the upstream and downstream directions. The flow within the Ala Wai Canal is not significantly impeded by the construction of the foundation supports within the canal.

12. Substantially affects scenic vistas and view planes identified in county or state plans or studies.

The project does not substantially affect scenic vistas nor view planes.

- The pedestrian bridge will abut the seaward side of the existing Kalakaua Avenue Bridge and will have the same profile and architectural design as the existing bridge. This design preserves the existing view planes and scenic vistas.
- The sidewalk improvements have no effect upon the existing view planes and scenic vistas.

13. Requires substantial energy consumption.

The project does not require substantial energy consumption.

- During the construction period, a substantial amount of energy will not be required for the construction activities.
- After the construction is completed, the pedestrian bridge and sidewalk improvements will have a negligible effect upon energy consumption.

2.1 List of Permits and Approvals

The following permits and approvals are being obtained for this project:

- Department of the Army Nationwide Permit No. 25 (Structural Discharges)
- Coastal Zone Management Programs Federal Consistency Determination from the State Office of Planning, Department of Business Economic Development and Tourism
- Section 401 Water Quality Certification from the State Department of Health Clean Water Branch
- National Pollutant Discharge Elimination System General Permit for Construction Activity Dewatering from the State Department of Health Clean Water Branch
- Stream Channel Alteration Permit from the State Department of Land and Natural Resources Commission on Water Resource Management
- Bridges and Causeway Permit from the United States Coast Guard
- Waikiki Special District approval and Flood Fringe District Certification from the City and County of Honolulu Department of Planning and Permitting

A summary of each permit's processing status through August 1, 1998 is provided in Table 2.1 below. Copies of the latest permitting agency correspondence are included in Appendix A.

TABLE 2.1
SUMMARY OF PERMITS/APPROVALS
KALAKAUA AVENUE BRIDGE IMPROVEMENTS

PERMIT/APPROVAL & ISSUING AGENCY

STATUS (thru August 1, 1998)

Nationwide Permit No. 25
Structural Discharges
 Department of the Army

Final Permit issued July 13, 1998

Bridges and Causeways Permit
 United States Coast Guard, 14th District

Advanced approval letter issued
 on February 25, 1998. No permit required.

Coastal Zone Management Programs
Federal Consistency Review
 Office of Planning
 State Department of Business, Economic
 Development and Tourism

Concurrence letter received May 20, 1998

Section 401 Water Quality Certification
 Clean Water Branch
 State Department of Health

Certification issued July 13, 1998

National Pollutant Discharge
Elimination System General Permit
Discharges Associated with Construction
Activity Dewatering
 Clean Water Branch
 State Department of Health

Initial submittal to DOH in August 1998.

Community Noise Permit for
Construction Activities
 Noise and Radiation Branch
 State Department of Health

Application to be started in August 1998

Solid Waste Disclosure for Construction Sites
 Solid and Hazardous Waste Branch
 State Department of Health

Application to be started in August 1998

TABLE 2.1
SUMMARY OF PERMITS/APPROVALS
KALAKAUA AVENUE BRIDGE IMPROVEMENTS

PERMIT/APPROVAL & ISSUING AGENCY

STATUS (thru August 1, 1998)

Stream Channel Alteration Permit

Commission on Water Resource Management
 State Department of Land & Natural Resources

Resubmittal to DLNR in August 1998.
 Approval expected by September/October 1998.

Flood Fringe District Certification

Regulatory Branch
 Department of Planning and Permitting
 City and County of Honolulu

Resubmittal in August 1998
 Approval expected in September 1998

Waikiki Special District

Minor Permit
 Department of Planning and Permitting
 City and County of Honolulu

Submittal date to be determined

Work Within City & County Right-of-Way

Department of Transportation Services
 City and County of Honolulu

Application to be started in August 1998

3 PROJECT DESCRIPTION

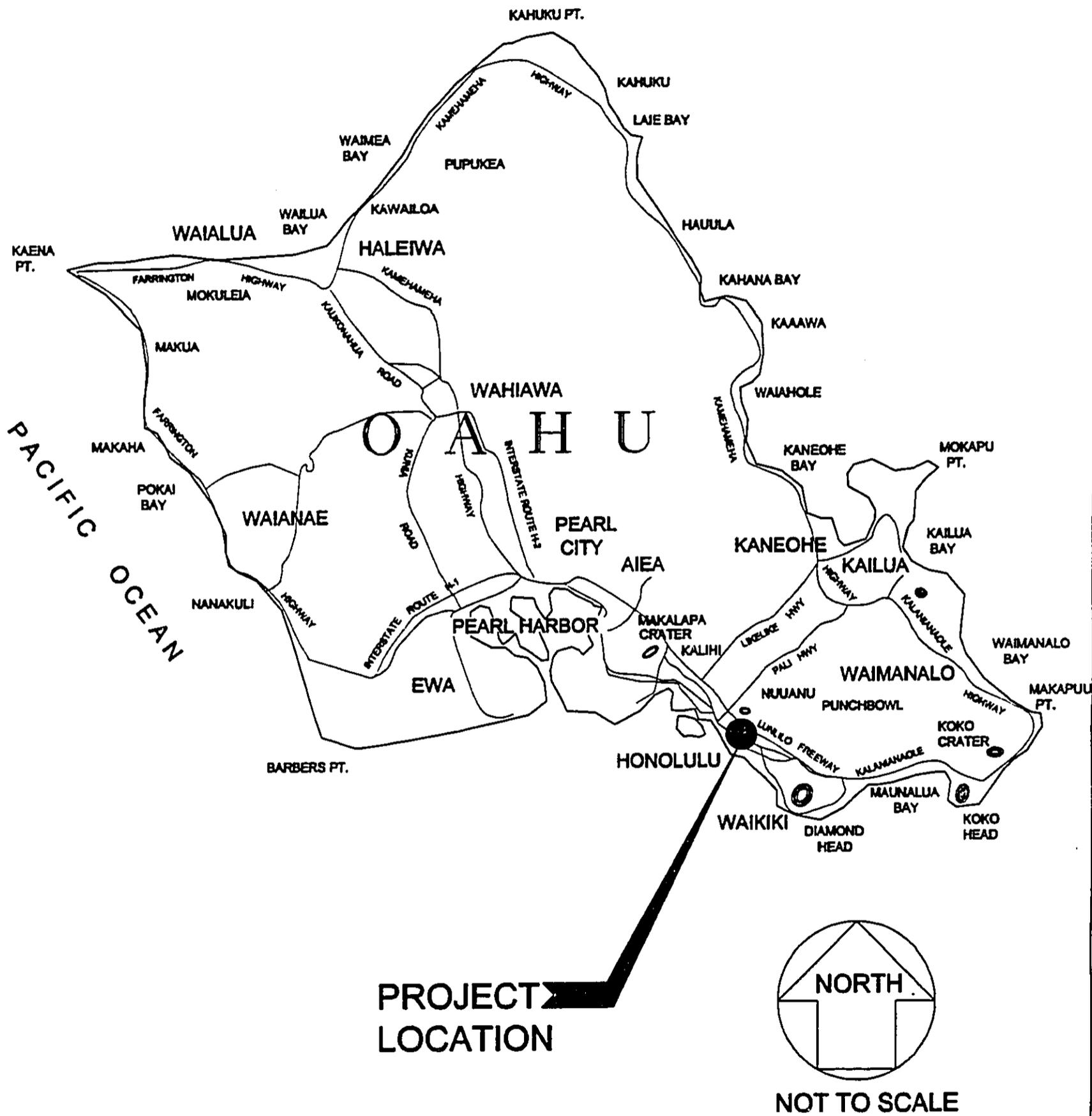
3.1 *Purpose of the Proposed Action*

The State of Hawaii Department of Transportation-Highways Division proposes to construct a new pedestrian bridge and sidewalk improvements adjacent to the existing Kalakaua Avenue Bridge, near the intersection of Kalakaua Avenue and Ala Wai Boulevard on the island of Oahu, as shown in Figures 3-1 and 3-2..

The purpose of this project is to:

- Increase safety for pedestrians traveling along Kalakaua Avenue across the Ala Wai Canal. The new bridge will increase safety by moving pedestrian traffic away from the vehicle travel lanes crossing the existing Kalakaua Avenue Bridge.
- Reconstruct portions of the existing sidewalk at the Kalakaua Avenue-Ala Wai Boulevard intersection to conform with the Americans with Disabilities Act (ADA). The project will reconstruct the sidewalk at the southwest corner of the intersection to conform with ADA slope requirements.
- Improve pedestrian access along Kalakaua Avenue crossing the Ala Wai Canal, between Waikiki and the Hawaii Convention Center.

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KALAKAUA PEDESTRIAN BRIDGE

FIGURE

ISLAND MAP

3-1

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3.2 Description of the Proposed Action

The project proposes to construct a new pedestrian bridge and sidewalk improvements adjacent to the existing Kalakaua Avenue Bridge.

3.2.1 Pedestrian Bridge

The new pedestrian bridge will be a three-span concrete structure approximately 180 feet long and 14 feet wide abutting the seaward side of the existing Kalakaua Avenue Bridge. The pedestrian bridge is designed to match the profile and architectural style of the existing bridge, as shown in Figure 1-1.

- The pedestrian bridge structure will include an approximate 10-foot wide walking surface, a new concrete railing wall on the seaward side, new pedestrian lighting upon the bridge, pedestrian connections to the existing Ala Wai Promenade and to the existing Ala Wai Boulevard sidewalk, and a utilities chase within the interior of the bridge structure to accommodate future installation or relocation of subsurface utilities.
- The State of Hawaii Department of Land and Natural Resources Historic Preservation Division was consulted to determine the project's impacts upon the historical character of the existing Kalakaua Avenue Bridge, the Ala Wai Promenade, and the Ala Wai Canal and its stonewalls. Based on these consultations, the new bridge employs an architectural design matching the existing bridge and a structural design allowing the new bridge to support itself independently of the existing bridge. The design also minimizes impacts as the bridge walkway joins to the Promenade and the sidewalks along the canal sides. As a result, the State Historic Preservation Division has determined that the project has no adverse effect upon the historical character of the Kalakaua Avenue Bridge and no effect to the historical character of the Ala Wai Promenade, the Ala Wai Canal and its stonewalls.

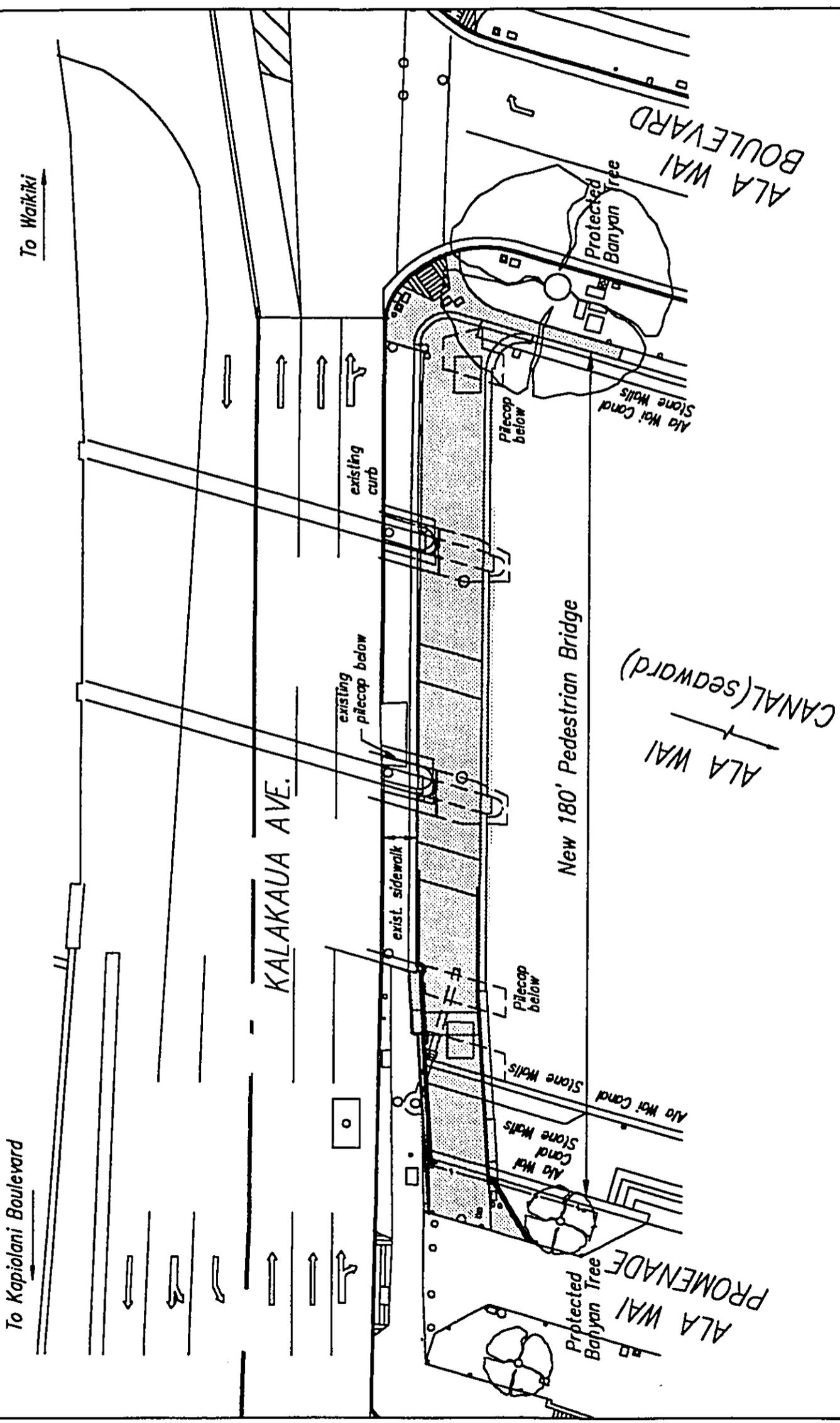
- The project has considered the effects to the recreational water activities which utilize the Ala Wai Canal. The arches of the pedestrian bridge structure are designed to match the existing bridge arches in width and height over the water surface. After the construction is completed, the pedestrian bridge will not hinder the passage of water recreational activities through the arches and beneath the bridge.

3.2.2 Sidewalk Improvements

The project will construct sidewalk improvements at the southwest corner of the Kalakaua Avenue-Ala Wai Boulevard intersection and at the bridge walkway connection to the Ala Wai Promenade. These areas are shown in Figure 3-3.

- The existing sidewalk at the southwest corner of the Kalakaua Avenue-Ala Wai Boulevard intersection does not conform with the slope requirements of the Americans with Disabilities Act. The project will reconstruct the sidewalk to conform with these slope requirements and connect the bridge walkway to the reconstructed sidewalk.
- The bridge walkway will connect to the existing walkway of the Ala Wai Promenade. A new pedestrian handrail will also be installed.
- The “exceptional” banyan trees along the Ala Wai Promenade and the Ala Wai Boulevard are within the project area and will be affected by the construction activities. The City and County of Honolulu Department of Parks and Recreation’s Arborist Advisory Committee and The Outdoor Circle have been consulted in the development of appropriate treatment and protection plans for these trees.

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KALAKAUA AVENUE BRIDGE IMPROVEMENTS

FIGURE 3-3

PEDESTRIAN BRIDGE PLAN

To provide clearance for construction machinery and to install underground electrical conduits for the pedestrian lighting, the canopies of three trees will be selectively trimmed and the roots of one tree will be pruned. To avoid inadvertent damage to the trees, protective barricades and root protection will be installed. This work will be performed under the guidance of a certified arborist. In addition to the above measures, the project will provide watering of the trees during the construction period.

3.2.3 Ownership and Maintenance

After the completion of the project construction, the State of Hawaii Department of Transportation proposes to turn the ownership and maintenance of the pedestrian bridge over to the City and County of Honolulu. The bridge will become a part of the Kalakaua Avenue right-of-way which is currently under the jurisdiction of the City and County of Honolulu.

3.2.4 Construction Methods for Bridge Improvements

The bridge will be supported by new abutments at each end and two interior piers. Bridge foundation work in the water consists of drilled or driven concrete piles at both bridge abutments and at the two interior piers. The above-water work will consist of the bridge structure including columns, beams, bridge deck, utilities chase and pedestrian lighting. A roughly 20-foot long portion of the existing bridge railing and end scroll at the Waikiki end will be removed to allow pedestrian access on to the new walkway and will be replicated in the new railing of the pedestrian bridge.

Probable construction sequencing for the in-water work portion of the project will involve the construction of a sheet pile cofferdam around the submerged pile caps, drilling and/or driving of concrete piles within the cofferdam to refusal below the sediment layer, and possible dewatering within the cofferdam during concrete pile cap construction. Throughout much of this in-water work, the contractor may utilize various

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barges, platforms, cranes, pile driving equipment, mooring lines, cables, hoses, or pumps either on shore, from floating platforms or combinations of both. Silt screens will be deployed around the perimeter of any equipment which could create a sediment plume during its use. The silt screens will remain in place for the duration of that activity and will not be removed until the water quality within the silt screen containment area has returned to its pre-construction condition. The contractor will be required to coordinate his equipment movement, storage and silt screen activities with affected recreational users of the canal to ensure minimal impacts to their schedule and to ensure public safety.

The contractor is expected to utilize nearby parking areas for construction staging and as access points to launch areas for construction watercraft. Possible areas include the parking lane on Ala Wai Boulevard from the Kalakaua Avenue intersection to the Lipeepe Street intersection; the Ala Wai Boulevard cul-de-sac near the Ala Moana Bridge; and the Ala Wai Yacht Harbor. Conditions will be placed on the Contractor during the bridge construction to mitigate impacts to: traffic and pedestrian movement, nearby "exceptional trees", recreational canal users, historic resources and water quality.

3.3 Estimated Construction Schedule and Cost

The construction schedule is undetermined at the time of this final environmental assessment. The start of funding is pending construction availability.

The estimated construction cost of the project is in the range of \$5 million.

The State Department of Transportation is requesting the Federal Highway Administration to provide federal funds for the construction. The construction start is pending the availability of these funds.

4 CONSULTATION

The following agencies and community groups listed in Table 4.1 were sent early assessment letters during the preparation of the Draft Environmental Assessment.

In addition to the 73 early assessment letters distributed, meetings with 37 City, State and Federal jurisdictional agencies, neighborhood boards, athletic organizations, community organizations, condominium associations and boards, and elected officials were conducted between December 1997 and March 1998 to both inform the public of the proposed action and solicit public input. Public meetings are summarized in Appendix B. Figure 4-1 graphically portrays the locations of the neighborhood board and public informational meetings held within the affected neighborhood board boundaries. Figure 4-2 shows the different condominiums and businesses in the immediate vicinity of the project site which could be impacted during construction, but would reap the more immediate benefits of the improved pedestrian access and safety that the bridge is expected to provide.

4.1 Agency Consultations

Eighteen City, State and Federal agencies were sent early assessment letters for the project in early-February. Additional meetings were held in late-February to clarify comments and concerns, particularly for the agencies that had permitting jurisdiction over the proposed action.

4.2 Community Consultations

The community consultation effort in early-February consisted of meetings with utility companies, neighborhood boards, and nearby condominium associations, businesses, community organizations and athletic organizations. Elected officials and their staff from the State Legislature and City Council were also briefed.

**TABLE 4.1
EARLY-ASSESSMENT LIST
KALAKAUA AVENUE BRIDGE IMPROVEMENTS**

CITY:

1	Honolulu Board of Water Supply
2	Building Department
3	Department of Land Utilization
4	Department of Planning
5	Department of Parks and Recreation
6	Department of Public Works
7	Department of Wastewater Management
8	Department of Transportation Services
9	Honolulu Fire Department
10	Honolulu Police Department
11	Office of Waikiki Development

STATE:

12	DBEDT
13	Department of Health
14	Department of Land and Natural Resources
15	Office of State Planning
16	University of Hawaii Environmental Center

FEDERAL:

17	U.S. Army Corps of Engineers
18	U.S. Fish & Wildlife Service

UTILITIES:

19	HECO
20	GTE-HTEL
21	GASCO
22	Oceanic Cable

NEIGHBORHOOD BOARDS:

23	Neighborhood Board No. 8 - McCully/Moiliili
24	Neighborhood Board No. 9 - Waikiki
25	Neighborhood Board No. 11 - Ala Moana/Kakaako
26	Neighborhood Board No. 5 - Diamond Head/Kapahulu/St. Louis Heights

TABLE 4.1 (continued)
EARLY-ASSESSMENT LIST
KALAKAUA AVENUE BRIDGE IMPROVEMENTS

ATHLETIC ORGANIZATIONS

27	Hawaiian Canoe Racing Association
28	Oahu Canoe Racing Association
29	Waikiki Yacht Club
30	Hawaii Yacht Club
31	Ala Wai Boat Club
32	Honolulu Rowing Club
33	Hawaii Canoe and Kayak Team
34	American Canoe Association

COMMUNITY ORGANIZATIONS:

35	Waikiki Community Center
36	Ala Wai Watershed Community Network
37	Ala Wai Gateway Tenants Association
38	Waikiki Improvement Association
39	Friends/Neighbors of the Ala Wai
40	Waikiki Residents Association
41	The Outdoor Circle
42	Hawaii State Council
43	Hawaii Convention Park Council
44	Hawaii Transportation Association
45	Hawaii Visitors Bureau
46	International Association for Exposition Management
47	American Lung Association
48	Hawaii Hotel Association
49	Waikiki Health Center
50	Waikiki Area Action Association
51	Kapiolani Park Preservation Society
52	Convention Center Authority

TABLE 4.1 (continued)
EARLY-ASSESSMENT LIST
KALAKAUA AVENUE BRIDGE IMPROVEMENTS

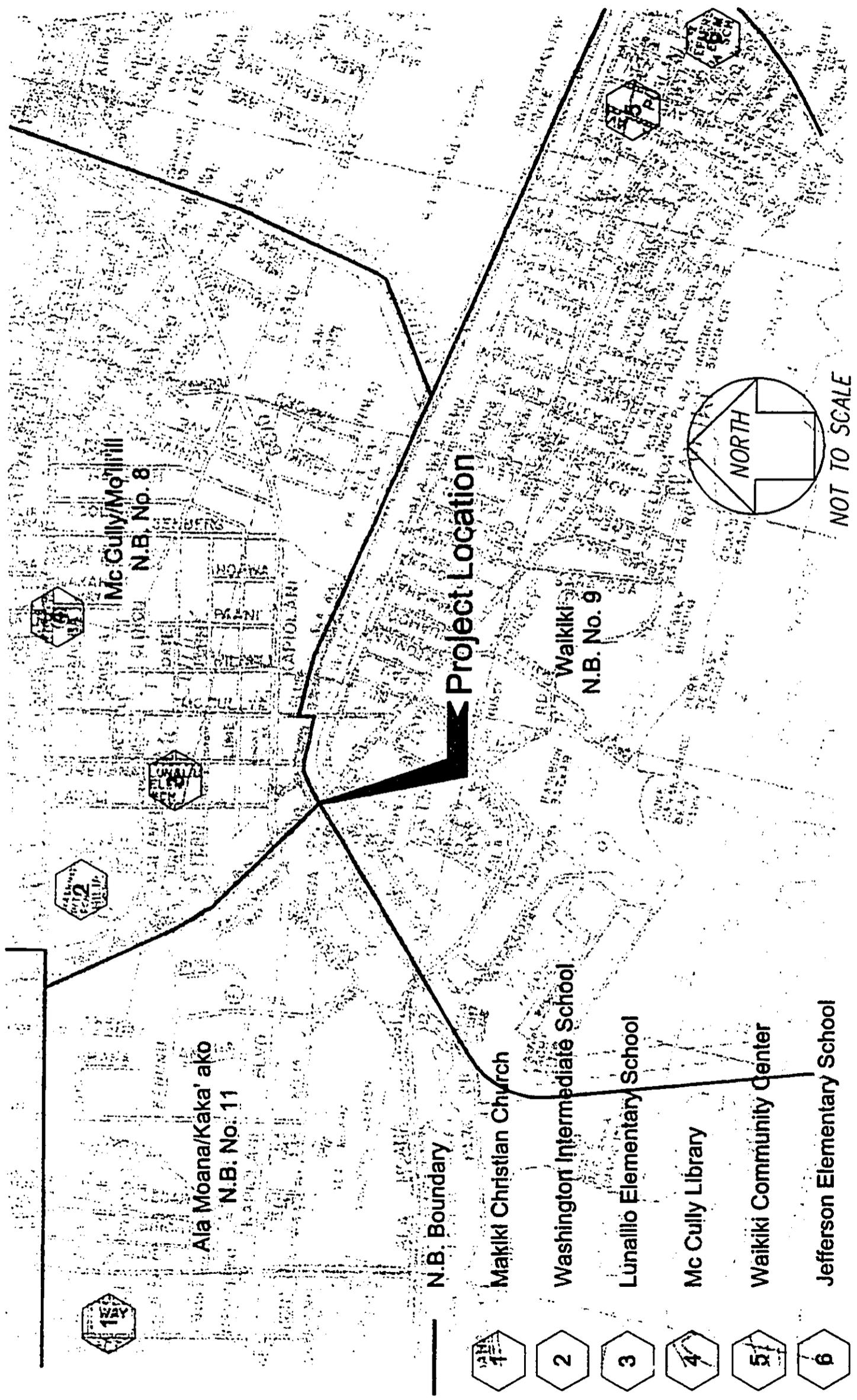
CONDOMINIUM ASSOCIATIONS/BUSINESSES

53	1717 Ala Wai
54	Waikiki Landmark
55	Century Center
56	Marina Towers Condominium
57	Hard Rock Café
58	Outrigger Maile Skycourt Hotel
59	Ilikai Hotel
60	Hilton Hawaiian Village
61	Hale Koa Hotel
62	The Convention Center Authority

ELECTED OFFICIALS:

63	Senator Les Ihara (10th, Kapahulu-Waiālae)
64	Senator Brian Taniguchi (11th, McCully-Moiliili)
65	Senator Carol Fukunaga (12th, Makiki-Ala Moana)
66	Representative Brian Yamane (19th, Waikiki, Kaimuki, D.H.)
67	Representative Scott Saiki (20th, Kapahulu-Moiliili)
68	Representative Galen Fox (21st, Waikiki-Ala Wai)
69	Representative Terry Yoshinaga (22nd, Moiliili-McCully-Pawaa)
70	Representative Kenneth Hiraki (25th, Downtown-Ala Moana)
71	Councilmember Duke Bainum
72	Councilmember Andy Mirikitani
73	Councilchairperson John Desoto

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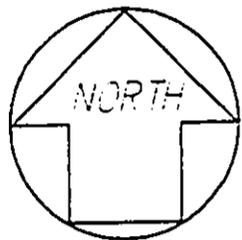
KALAKAUA PEDESTRIAN BRIDGE

FIGURE

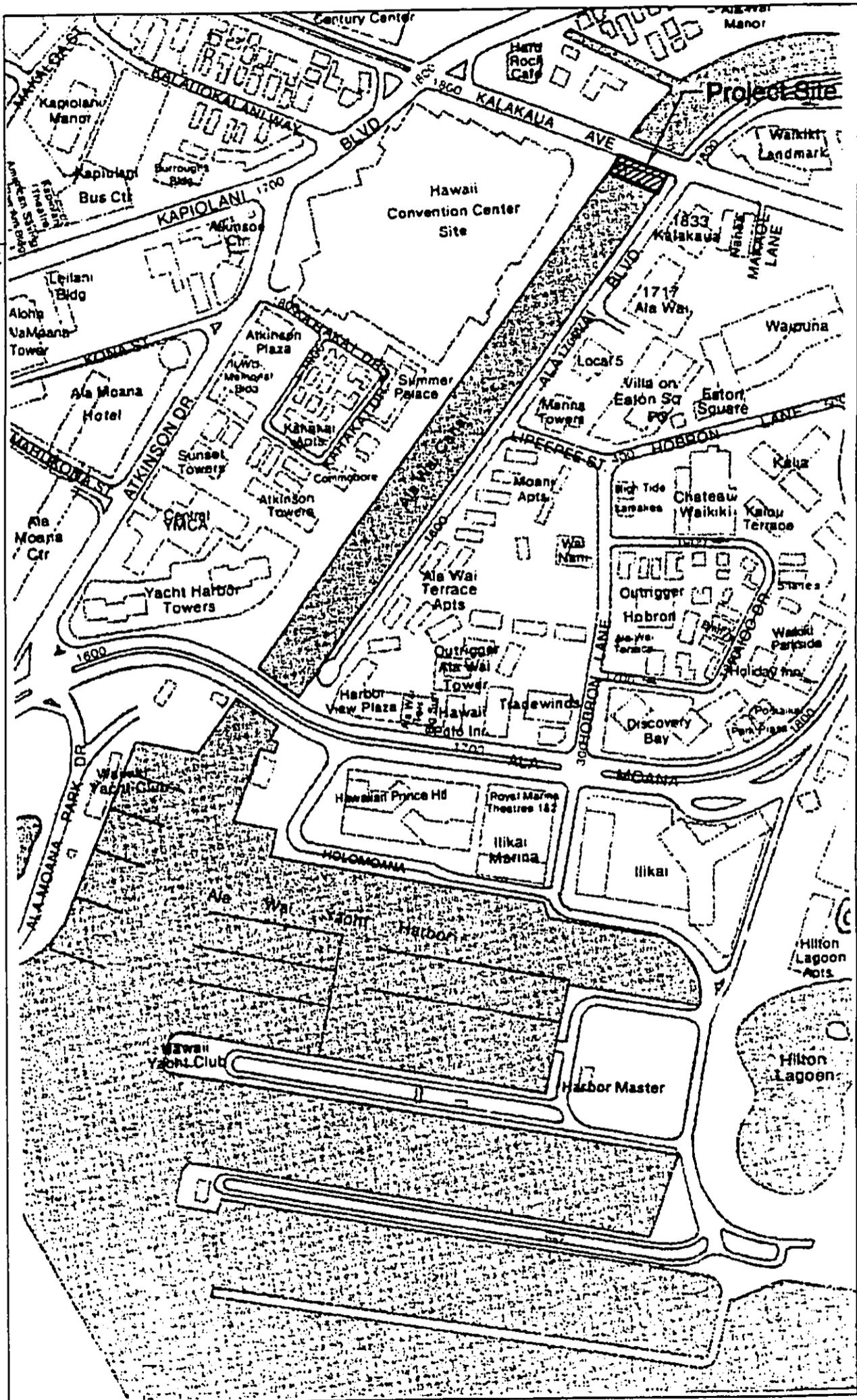
COMMUNITY MEETINGS MAP

4-1

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NOT TO SCALE



KALAKAUA PEDESTRIAN BRIDGE

FIGURE

COMMUNITY CONSULTATION MAP

4-2

4.3 Comments to the Draft Environmental Assessment

Comments were received from approximately 20 agencies, government officials and private entities within the 30-day comment period (April 23, 1998 – May 26, 1998). At the request of Councilmember Duke Bainum's office (District IV), additional copies of the draft environmental assessment were sent to the following condominium associations on June 16, 1998: Waipuna, Villa on Eaton Square, Kalia, Wailana and Discovery Bay. No responses were received from these additional mailouts. A summary of the responding entities is shown in Table 4.2 with their primary concerns identified. The actual comment and response letters from the 30-day comment period are included as Appendix C.

**TABLE 4.2
DRAFT EA RESPONSE SUMMARY
KALAKAUA AVENUE BRIDGE IMPROVEMENTS**

CITY:	RESPONSE:
1. Honolulu Board of Water Supply	No objections
2. Building Department	Did not submit comments
3. Department of Land Utilization	No further comments
4. Department of Planning	No further comments
5. Department of Parks and Recreation	Expressed concerns over impacts to recreational user schedules and safety.
6. Arborist Advisory Committee	emphasized the need for protective measures for exceptional trees
7. Department of Public Works	requested resolution of right-of-way issues & Americans with Disabilities Act compliance in pedestrian areas
8. Department of Wastewater Management	Requested clarification on impacts to existing force mains in roadway
9. Department of Transportation Services	did not submit comments
10. Honolulu Fire Department	did not submit comments
11. Honolulu Police Department	did not submit comments
12. Office of Waikiki Development	Requested coordination of canoeing activities
STATE:	RESPONSE:
13. DBEDT	Emphasized the need for Federal Consistency compliance
14. Department of Health	Concerns for impacts due to construction noise. identified permitting requirements
15. Department of Land and Natural Resources	Requested marine inventory methodology
16. Office of State Planning	Emphasized the need for Federal Consistency compliance
17. University of Hawaii Environmental Center	did not submit comments

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**TABLE 4.2
DRAFT EA RESPONSE SUMMARY
KALAKAUA AVENUE BRIDGE IMPROVEMENTS**

18. Office of Environmental Quality Control	Various concerns included: protection of Exceptional Trees, impacts due to construction noise, contractor staging area plan, no bike usage on sidewalks, coordination with the dredging project, coordination with affected utilities, expansion of significance criteria.
FEDERAL:	
19. U.S. Army Corps of Engineers	no further comments
20. U.S. Fish & Wildlife Service	did not submit comments
UTILITIES:	
21. Hawaiian Electric Company	no comments at this time
22. GTE-Hawaiian Telephone	asked to coordinate utility relocation
23. The Gas Company	did not submit comments
24. Oceanic Cable	did not submit comments
NEIGHBORHOOD BOARDS:	
25. Neighborhood Board No. 8 - McCully/Moiliili	did not submit comments
26. Neighborhood Board No. 9 - Waikiki	did not submit comments
27. Neighborhood Board No. 11 - Ala Moana/Kakaako	did not submit comments
28. Neighborhood Board No. 5- Diamond Head/Kapahulu/St. Louis Heights	did not submit comments
ATHLETIC ORGANIZATIONS	
29. Hawaiian Canoe Racing Association	did not submit comments
30. Oahu Canoe Racing Association	did not submit comments
31. Waikiki Yacht Club	did not submit comments
32. Hawaii Yacht Club	did not submit comments
33. Ala Wai Boat Club	did not submit comments
34. Honolulu Rowing Club	did not submit comments
35. Hawaii Canoe and Kayak Team	did not submit comments
36. American Canoe Association	did not submit comments

TABLE 4.2
DRAFT EA RESPONSE SUMMARY
KALAKAUA AVENUE BRIDGE IMPROVEMENTS

COMMUNITY ORGANIZATIONS:	RESPONSE:
37. Waikiki Community Center	did not submit comments
38. Ala Wai Watershed Community Network	did not submit comments
39. Ala Wai Gateway Tenants Association	did not submit comments
40. Waikiki Improvement Association	did not submit comments
41. Friends/Neighbors of the Ala Wai	did not submit comments
42. Waikiki Residents Association	did not submit comments
43. The Outdoor Circle	emphasized the need for protective measures for exceptional trees
44. Hawaii State Council	did not submit comments
45. Hawaii Convention Park Council	did not submit comments
46. Hawaii Transportation Association	did not submit comments
47. Hawaii Visitors Bureau	did not submit comments
48. International Association for Exposition Management	did not submit comments
49. American Lung Association	did not submit comments
50. Hawaii Hotel Association	did not submit comments
51. Waikiki Health Center	did not submit comments
52. Waikiki Area Action Association	did not submit comments
53. Kapiolani Park Preservation Society	did not submit comments
54. Convention Center Authority	expressed strong project support
CONDO AOAOS/BUSINESSES	RESPONSE:
55. 1717 Ala Wai	did not submit comments
56. Waikiki Landmark	did not submit comments
57. Century Center	did not submit comments
58. Marina Towers Condominium	did not submit comments
59. Hard Rock Café	did not submit comments
60. Outrigger Maile Skycourt Hotel	did not submit comments
61. Ilikai Hotel	did not submit comments
62. Hilton Hawaiian Village	did not submit comments
63. Hale Koa Hotel	did not submit comments
64. The Convention Center Authority	did not submit comments
65. Waipuna	did not submit comments

TABLE 4.2
DRAFT EA RESPONSE SUMMARY
KALAKAUA AVENUE BRIDGE IMPROVEMENTS

66. Villa on Eaton Square	did not submit comments
67. Kalia	did not submit comments
68. Wailana	did not submit comments
69. Discovery Bay	did not submit comments
ELECTED OFFICIALS:	RESPONSE:
70. Senator Les Ihara (10th, Kapahulu-Waiālae)	did not submit comments
71. Senator Brian Taniguchi (11th, McCully-Moiliili)	did not submit comments
72. Senator Carol Fukunaga (12th, Makiki-Ala Moana)	did not submit comments
73. Representative Brian Yamane (19th, Waikiki, Kaimuki, D.H.)	did not submit comments
74. Representative Scott Saiki (20th, Kapahulu-Moiliili)	did not submit comments
75. Representative Galen Fox (21st, Waikiki-Ala Wai)	did not submit comments
76. Representative Terry Yoshinaga (22nd, Moiliili-McCully-Pawaa)	did not submit comments
77. Representative Kenneth Hiraki (25th, Downtown-Ala Moana)	did not submit comments
78. Councilchairperson John Desoto	did not submit comments
79. Councilmember Duke Bainum	Expressed concerns for construction noise impacts, staging area locations
80. Councilmember Andy Mirikitani	Requested inclusion of specific planning initiatives being studied by the City, expressed concern for the protection of the Exceptional Trees

5 AFFECTED ENVIRONMENT, POTENTIAL IMPACTS AND MITIGATIVE MEASURES

5.1 Land Use Conditions

5.1.1 State Land Use Commission

Under State Land Use Law (Chapter 205 Hawaii Revised Statutes), all of the lands in the State are designated either Urban, Rural, Agricultural or Conservation. The project site is classified under the Urban designation. The proposed project is consistent with the current designation.

5.1.2 Coastal Zone Management/Special Management Areas

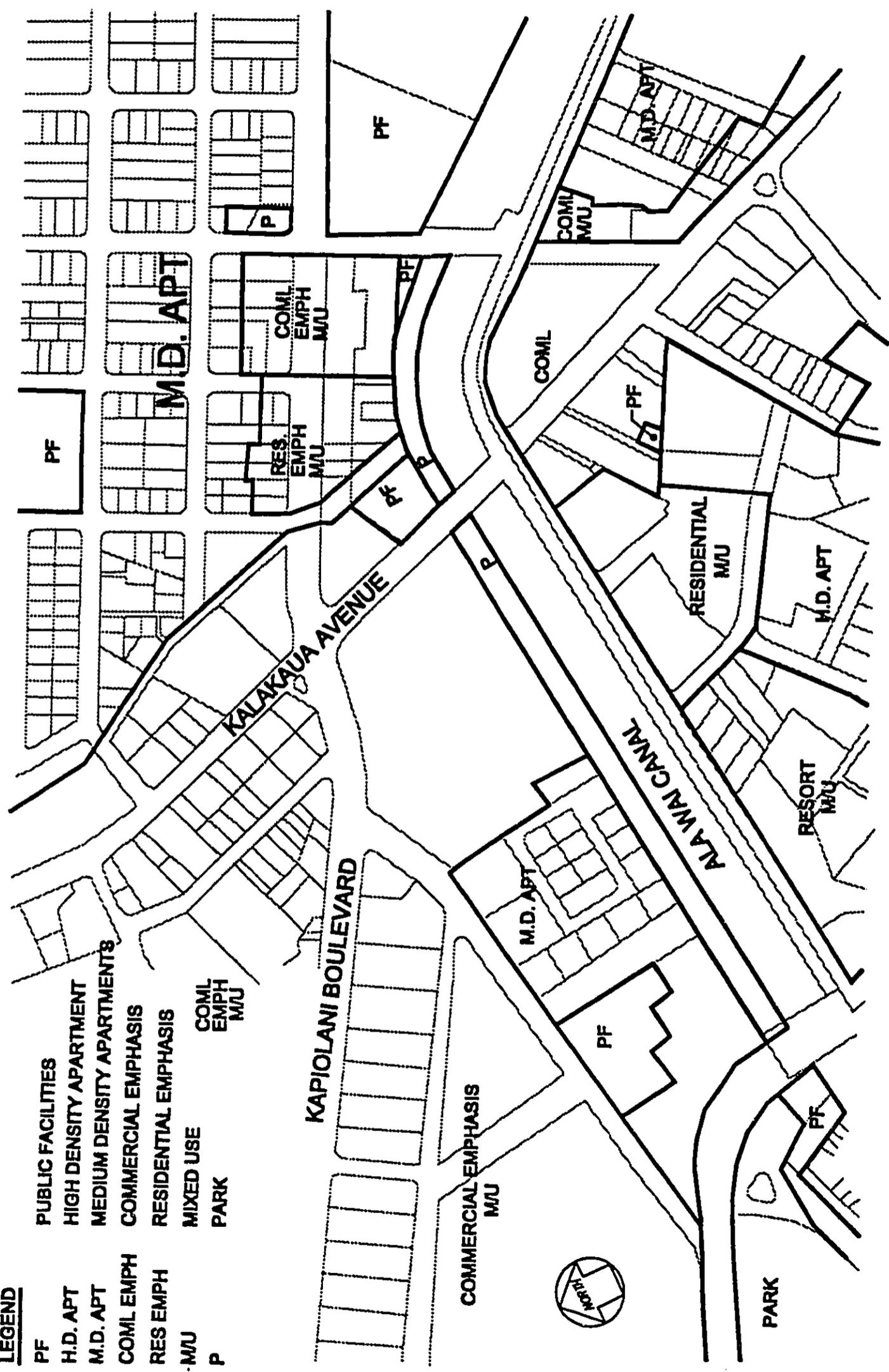
Under Chapter 205A of the Hawaii Revised Statutes, the Department of Business Economic Development and Tourism Office of Planning manages the State's Coastal Zone Management program. Under the program, the State oversees the appropriate development of coastal areas and the preservation and protection of coastal natural resources. A Federal Consistency Determination with the State's Coastal Zone Management Program was received May 20, 1998 from the Department of Business Economic Development and Tourism's Office of Planning for the project.

County oversight is provided through a Special Management Area (SMA) program. However, the project site is outside of the SMA boundary.

5.1.3 City & County of Honolulu Land Use and Zoning

Detailed land use designations are provided on the City and County of Honolulu's Development Plan Land Use (Figure 5-1) and Public Facilities (Figure 5-2) maps, the implementing tools for the City's General Plan. The project site is located on the Primary Urban Center set of maps. The project site over the Ala Wai Canal is designated as Preservation Land Use on the Development Plan Land Use map. The project site also falls within the Public Precinct of the Waikiki Special District. The land area at the Kapiolani Boulevard end of the site is the Ala Wai Promenade (Park) and Hawaii Convention Center (Commercial Emphasis Mixed Use). Surrounding land use

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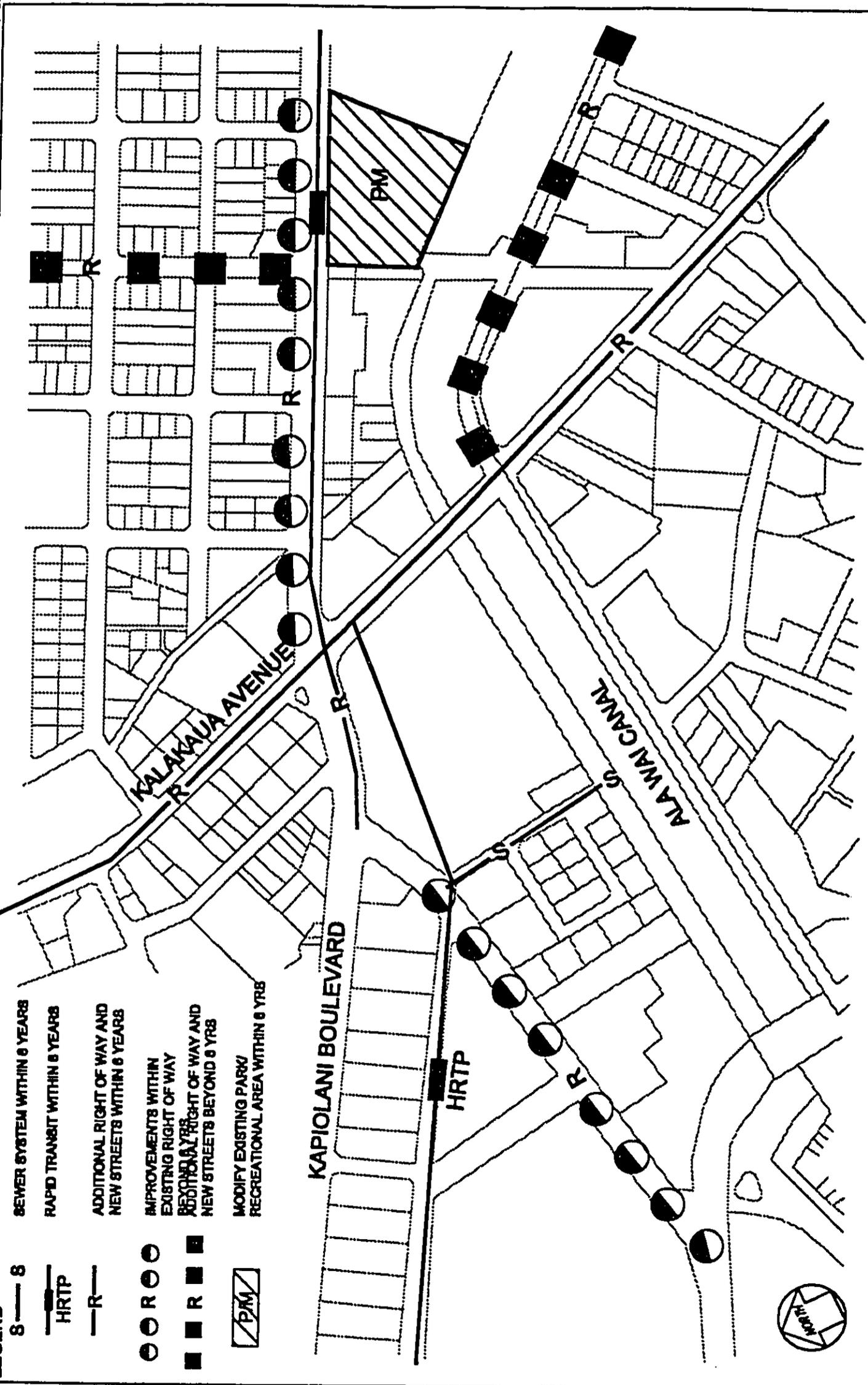
- LEGEND**
- PF PUBLIC FACILITIES
 - H.D. APT HIGH DENSITY APARTMENTS
 - M.D. APT MEDIUM DENSITY APARTMENTS
 - COML EMPH COMMERCIAL EMPHASIS
 - RES EMPH RESIDENTIAL EMPHASIS
 - MIXED USE MIXED USE
 - PARK PARK
 - COML EMPH MU COMMERCIAL EMPHASIS MIXED USE
 - M.D. APT MU MEDIUM DENSITY APARTMENTS MIXED USE
 - RES EMPH MU RESIDENTIAL EMPHASIS MIXED USE
 - H.D. APT MU HIGH DENSITY APARTMENTS MIXED USE
 - RESORT MU RESORT MIXED USE

KALAKAUWA AVENUE BRIDGE IMPROVEMENTS
CITY & COUNTY OF HONOLULU
DEVELOPMENT PLAN LAND USE MAP

FIGURE

5-1

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KALAKAUA AVENUE BRIDGE IMPROVEMENTS

FIGURE 5-2

CITY & COUNTY OF HONOLULU

DEVELOPMENT PLAN PUBLIC FACILITIES MAP

designations at the Waikiki end include Residential Mixed Use and Commercial. The proposed project will be consistent with the current land use and zoning (Figure 5-3) designations as designated by the City Department of Land Utilization's Land Use Ordinance.

The Development Plan Public Facilities Map identifies a rapid transit system to the area and improvements to the existing right-of-way over the Kalakaua Bridge within six years. Both projects could impact the use and capacity of the proposed pedestrian bridge. However, neither project is currently defined enough to evaluate these impacts.

Currently, there are three studies being conducted by the City concerning the existing land use designations at or surrounding the project site: the Primary Urban Center Development Plan Revision Program by the Planning Department; the Urban Honolulu Bikeway System Master Plan by the Department of Transportation Services; and a Planning Department study for the area surrounding the Hawaii Convention Center and the Ala Wai Canal.

The Primary Urban Center Development Plan Revision Program has been soliciting public input in 1997 through a series of Saturday workshops and evening neighborhood board appearances. The Honolulu 2020 vision being developed will provide basic policy direction for pertinent issues like housing development, job growth, regional land use patterns, infrastructure networks, open space/parks, visitor industry, and protection and enhancement of the surrounding communities, like Waikiki, McCully-Moiliili and Ala Moana-Kakaako.

The Urban Honolulu Bikeway System Master Plan envisions a comprehensive bikeway system for urban Honolulu, with a good portion of the planning area falling between Kalihi and Kahala. This system may include a future bike lane within the Kalakaua Avenue right-of-way, which may require a widening of the existing roadway and/or reduction in the number or width of the existing lanes over the bridge. A more detailed discussion of a potential bike lane scenario is included in Section 5.9.4 Bike Routes.

The City Planning Department study of the area surrounding the Hawaii Convention Center and the Ala Wai Canal examines the future impacts of the Hawaii Convention Center and the redevelopment and renovation of projects (e.g. Ala Moana shopping center) on the surrounding communities with an emphasis on how to best preserve their residential character. This study is expected to provide input to the Primary Urban Center Development Plan Revision Program effort from the perspective of the Waikiki, McCully-Moiliili and Ala Moana-Kakaako communities.

5.1.4 Flood Hazard Determination

As a part of the requirements for the Waikiki Special District permit from the City Department of Land Utilization (DLU), a flood hazard determination must be requested by a licensed professional engineer.

Based on Flood Insurance Rate Map (FIRM) Community Panel Number 150001 0120 C, the project site is currently located in Zone "AO", Special Flood Hazard Areas Inundated by 100-Year Flood with flood depths determined to be within 1 to 3 feet (see Figure 5-4).

A flood hazard determination request is being processed with existing FIRM information to verify that the project will not result in an increase in regulatory flood elevations due to a modification of the affected waterway's cross sectional area at the bridge. At the DLU's request, appropriate technical materials are currently being provided to the City and County of Honolulu Department of Public Works for concurrent technical review. The provided information will show that the new bridge structure will not change the cross sectional area of the waterway. Thus, no increase in the regulatory flood elevation is anticipated. This information is expected to be submitted in the month of August 1998.

5.2 Topography and Soils

5.2.1 Project Site Topography

The project site topography ranges from approximately (-) 10 feet Mean Sea Level (MSL) at the canal mud line to (+) 12 feet at the crest of the existing bridge. The sidewalk area at the Kapiolani Boulevard end ranges from 6 feet to just under 7 feet MSL. The sidewalk area at the Waikiki end ranges from 10 feet to 10.5 feet MSL. Over the years, the entire boundary of the project site has been improved with sidewalks, curbs, gutters, pavement, subsurface and overhead utilities and planter strips.

5.2.2 Project Site Soils

The geotechnical investigation report for this project indicated loose and very soft lagoonal deposits at the canal bottom in the vicinity of the mid-span pier locations, and fill underlain by loose and very soft to soft lagoonal deposits at the abutment locations.

5.2.3 Ala Wai Canal Sediments

Several projects in the vicinity of the Kalakaua Avenue Bridge Improvements site have conducted sediment analyses in the 1990s, the most comprehensive effort being put in for the proposed Ala Wai Canal dredging project. A final report (Belt Collins Hawaii, May 1998) prepared for that dredging project examined the possible land and sea-based disposal options for the quality of sediment expected to be dredged from the canal. The report concluded that the dredged spoils to be removed from the canal bottom could be categorized as a "solid waste" (i.e. approved for land disposal) instead of as a "hazardous waste" (40 CFR 261). The State Department of Health Solid and Hazardous Waste Branch has been contacted for concurrence that similar spoils from this bridge project can be landfilled for disposal.

Dredging operations could start as soon as the end of 1999 and could last for up to one year. The State will coordinate its bridge project activities with the dredging operations.

Sediment analyses were performed for the Board of Water Supply's McCully/Waikiki 30-Inch Water Main by Brewer Environmental Services in 1991, roughly 200-300 yards upstream of Kalakaua Bridge. Toxic Characteristics Leaching Procedure analyses of four samples below the McCully Street Bridge found no concentrations that equaled or exceeded the respective Toxic Characteristic Leaching Procedure levels set by the United States Environmental Protection Agency for eight heavy metals: arsenic, cadmium, lead, selenium, barium, chromium, mercury and silver.

Construction methods employed on this project will require the use of Best Management Practices to minimize the disturbance of existing sediments within the impacted area. Consequently, there are no anticipated long-term impacts to the canal sediment as a result of the construction of the new pedestrian bridge.

5.3 Ala Wai Canal Water Quality

Ala Wai Canal water quality has long been considered of poor quality due primarily to pollutants from the contributing watersheds of Makiki, Manoa, St. Louis Heights, Palolo, Moiliili, Kapahulu, and parts of Kaimuki and Diamond Head. Collectively, the entire watershed encompasses an estimated 16.3 square miles (Noda & Associates, October 1992) of heavily urbanized area. The runoff from Palolo, Manoa and Makiki Streams is consistently laden with urban pollutants (fertilizers, detergents, motor oil, street particulates, animal feces, etc.) which either sink to the canal bottom or collect in the relatively quiescent waters. Although connected to the sea via the Ala Wai Yacht Basin, and therefore subject to diurnal tidal influence, relatively little flushing takes place in the upper reaches of the canal.

The State Department of Health classifies the canal as a "Class 2" Inland Water, the lowest inland water classification (HAR 11-54-03) and "Class A" Marine Waters. Water quality studies by OI Consultants (1992), Noda & Associates (October, 1992), and AECOS (1994) have indicated that the canal occasionally does not meet the recreational standards (HAR 11-54-08) of no more than 7 enterococci per 100 mls (average for marine waters) or 200 fecal coliform per 100 mls (average for non-marine waters). Monitoring data compiled by DOH at the Ala Moana Bridge and the McCully Bridge

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through August 1997 confirmed that this condition still exists. Nevertheless, the canal is actively used for recreational boating, kayaking, canoe competition and training, and fishing and crabbing.

DOH-regulated water quality parameters which could be significantly affected by the in-water work of this project include turbidity and total suspended solids (TSS). Turbidity ranged from a low of less than 1 nephelometric turbidity units (NTU) to a high of 12 NTU between January 1996 and August 1997. TSS readings between the same period ranged from a low of 1 mg/L to a high of 12 mg/L.

This project will employ Department of Health-approved best management practices to mitigate water quality impacts during the in-water construction. The in-water work will involve the driving of steel sheet piles, drilling and driving of concrete piles below the sediment layer, construction dewatering, and the placement of construction equipment and work platforms within the canal. The best management practices include the placement of silt screens to contain construction discharges, visual monitoring, water sampling for turbidity, and reporting to the Department of Health.

Permits, and approvals, issued for the project impose conditions to protect the ambient water quality. These permits are listed below with the issuing agency:

1. Section 401 Water Quality Certification, issued by the State of Hawaii Department of Health.
2. National Pollutant Discharge Elimination System General Permit for Construction Activity Dewatering, issued by the State of Hawaii Department of Health.
3. Stream Channel Alteration Permit, issued by the State of Hawaii Department of Land and Natural Resources Commission on Water Resource Management.

4. Department of the Army Nationwide Permit No. 25 for Structural Discharges, issued by the United States Army Corps of Engineers.
5. Federal Consistency Determination, issued by the State of Hawaii Department of Business, Education, Development and Tourism.
6. Bridges and Causeway Determination, issued by the United States Coast Guard.

This project is anticipated to acceptably mitigate short-term impacts to the ambient water quality through the use of best management practices. Long-term water quality impacts are not anticipated due to the project construction.

5.4 Flora & Fauna

The area surrounding the project site is fully developed and, in general, does not support a wide variety of flora or fauna.

Significant items within the project area are the banyan trees planted along the Ala Wai Boulevard and the Ala Wai Promenade. These trees, originally planted in 1936 by The Outdoor Circle, were placed on the City and County of Honolulu's Exceptional Tree List in 1992. Under the State of Hawaii's Exceptional Tree Act, these trees are deemed "exceptional" due to their "historic or cultural value, age, rarity, location, size, aesthetic quality, or endemic status."

The project will affect at least three of these "exceptional" banyan trees. Selective trimming of the tree branches will be necessary to provide clearance and access for construction machinery. Root pruning of one tree will be necessary to allow the installation of underground electrical conduits for the new pedestrian lighting. These trees will not be transplanted or removed.

To minimize construction impacts, the City and County of Honolulu Department of Parks and Recreation's Arborist Advisory Committee and The Outdoor Circle have been consulted to develop appropriate treatment and protection plans for these trees. Based on these consultations, work to trim and prune these trees will be performed under the following conditions:

- Treatment and protection plans for these trees shall be submitted to the Arborists Advisory Committee for approval prior to the start of construction.
- Trimming and pruning of the trees shall be performed under the guidance of a certified arborist.
- Potential project contractors shall be notified of the requirements to protect the trees during any pre-bid or pre-construction meetings.
- To protect against inadvertent damage to the trees, protective barricades and steel plates with mulch underlay shall be installed within the work areas.
- The trees within the work area shall be watered during the construction period.
- Should additional trees be affected, the Arborist Advisory Committee will be again consulted for appropriate treatment measures.

The site's local terrestrial fauna and avifauna consist of rats, stray cats, common mynah and pigeons, finches and doves.

Marine resources in the vicinity of the existing bridge have been previously surveyed by Jacquelin N. Miller in a report published by the Department of Oceanography, University of Hawaii Manoa Campus, Ecological Studies of the Biota of the Ala Wai Canal (March 1975). These resources include tilapia, mullet, barracuda and various bottom dwelling invertebrates and crustaceans. All are considered typical of the project's urbanized

marine setting. Due to the continuous sediment loading of the canal and the less than pristine water quality near the bridge, no significant coral colonies or marine life habitats are expected to be disturbed or displaced as a result of the pile driving or pier construction work.

According to the U.S. Fish and Wildlife there are no rare, threatened or endangered species of flora or fauna on the site. No short or long term adverse impacts are anticipated due to the construction and permanent placement of the proposed pedestrian bridge.

5.5 Air Quality

5.5.1 Ambient Air Quality

Ambient air quality is regulated by the National Ambient Air Quality Standards under Title 40 Code of Federal Regulations Part 50. The National Ambient Air Quality Standards are administered locally by the State Department of Health Clean Air Branch under Title 11 Chapter 59 Hawaii Administrative Rules. The standards base the quality of the ambient air on limits for the following parameters: particulate matter larger than 10 microns, sulfur dioxide, nitrogen dioxide, carbon monoxide, ozone and lead. The State limits are more stringent than the Federal version.

An Air Quality Impact Report performed for the Hawaii Convention Center Final Environmental Impact Statement by J.W. Morrow of Environmental Management Consultants (June 1995) found carbon monoxide levels in the Kapiolani/Kalakaua and Kapiolani/Atkinson intersections below the State limits during peak rush hour periods. No other air quality parameters were sampled. The State Department of Health Clean Air Branch does keep air quality records of the various National Ambient Air Quality Standards parameters at various sites around the island. Carbon Monoxide data compiled between 1991 and 1993 at the Waikiki Station (2131 Kalakaua Avenue) showed no incidences of the Federal and State ambient air quality standards being exceeded over the 1,000-plus samples.

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Air quality impacts during construction will be the result of dust and equipment emissions. However, the minimal amount of grading required of this project is expected to keep the dust to a minimum. Mitigation will include watering of exposed areas where needed. Equipment emissions can be minimized through proper equipment maintenance. In-water work is not expected to create an air quality emissions problem. The bridge project is not expected to contribute to any long term air quality problems once it is built.

5.5.2 Noise

The current noise levels at the bridge were studied for the Hawaii Convention Center by Y. Ebisu & Associates (1994). Existing traffic noise contours developed from traffic noise sampling at seven nearby locations indicated that noise levels range from roughly 65 dB to 75 dB near the bridge project site. An increase of between 1 dB and 6dB is expected to occur at some of these sites during large (10,000+ person) conventions. The proposed pedestrian bridge itself is not expected to add to these noise levels in the future.

Temporary short term construction impacts will include noise from the movement and operation of construction equipment. Noise levels due to pile driving or drilling operations is estimated to range between 110 and 130 decibels; will be intermittent during working hours depending on the progress of the work; and will have a duration of approximately two months. Noise from other construction activities is estimated to range between 70 and 110 decibels and will be prevalent during working hours for the approximately ten month duration of the project. These operations must comply with the Department of Health Title 11 Chapter 46 "Community Noise Control for Oahu". The contractor will be required to obtain a noise permit for this project and will be directed to maintain communication with the nearby community to mitigate public concerns.

Continuous communications between contractor and the local community will be strongly encouraged through, for example, the use of periodic progress meetings, establishment of a "hotline" to voice concerns, or identification of an accessible contractor's representative to answer questions or hear concerns.

No long term noise impacts are anticipated as a result of the pedestrian bridge being constructed.

5.6 Recreational Resources

5.6.1 Water Activities

The Ala Wai Canal is currently used by a number of athletic organizations and local residents for boating, canoe racing, flat water kayaking, fishing, and crabbing. These activities will not be negatively impacted by the pedestrian bridge once construction is completed since the proposed pedestrian bridge will match the existing bridge in width and height clearances for passage beneath the bridge.

During construction, however, portions of the bridge section usually available for water traffic may be blocked from access to maintain public safety around the construction site. Mitigative measures include public notice to affected groups along the Ala Wai Canal and temporary signage to warn canal users of the construction closings and their durations. The Contractor will be required to provide a minimum amount of access under the bridge to allow canal users access to and from the ocean. In addition, the Contractor will be required to be aware of the different schedules of the different water activities which use the canal on a daily and seasonal basis (high school canoe practices, canoe races on weekends, etc.) and maintain communications between these affected groups to ensure a minimum disruption of both recreational activities and the construction schedule. Public safety on adjacent lands and thoroughfares, and in the affected waters, will be emphasized. Jurisdiction over public safety for recreational uses in the canal falls on the United States Coast Guard 14th District and the State Department of Land and Natural Resources Division of Boating and Ocean Recreation. Both agencies will be consulted for either a permit or plan approval prior to construction.

5.6.2 Parks

Parks located near the project include the Ala Wai Field and the Ala Wai Promenade. The Ala Wai Promenade is an approximately 70 feet wide by 2,000 feet long land parcel bordering the northern boundary of the Ala Wai Canal, from Ala Moana Boulevard to McCully Street. It is a park-like strip of land, containing extensive pedestrian walkway

and landscape improvements, and serves as pedestrian access between Ala Moana Boulevard and Kalakaua Avenue. It also serves as a pedestrian route to the Hawaii Convention Center; the main stairway entry into the Hawaii Convention Center is accessed via the Ala Wai Promenade. The Ala Wai Field is a roughly 28 acre park bordering the northern boundary of the Ala Wai Canal, from McCully Street to University Avenue. The field is used for many recreational activities including: canoe launch facility and clubhouse, baseball, soccer, softball, jogging, parking lot and exercise station.

Project impacts could affect the use and access to these recreational resources if the contractor proposes to use park areas for construction staging, storage, or as a launching site for water-bound construction equipment. The contractor will be required to obtain approval to use park lands from the City and County of Honolulu Department of Parks and Recreation.

5.7 Archaeological, Cultural & Historic Resources

Recent archaeological studies done in the area by ERC Environmental and Energy Services Company, Inc. (1991), Rosendahl (1994) and Cultural Surveys Hawaii (1995) have adequately summarized the rich history of the Waikiki area from early settlement periods to the present, and have indicated a potential for encountering subsurface archaeological deposits in the Waikiki area.

Any excavations in the canal and at the abutments which encounter such deposits will be required to cease work immediately and notify the State of Hawaii Department of Land and Natural Resources Historic Preservation Division. Work will not recommence until a qualified archaeologist has examined the nature of the deposits for cultural significance. Mitigation measures will likely be specified at that time.

The Kalakaua Avenue Bridge, the Ala Wai Canal, and the Ala Wai Promenade are eligible for listing in the National Register of Historic Places. The Ala Wai Canal is currently on the Hawaii Register of Historic Places.

The pedestrian bridge construction is not expected to affect the historical character of the existing bridge. The new pedestrian bridge will be an independent, free-standing structure separated by a non-structural expansion joint. Once constructed, the new pedestrian bridge may serve to protect the existing historic structure that it abuts from weathering and deterioration. No long term impacts to the existing historic bridge, canal or promenade are expected as a result of the pedestrian bridge construction.

5.8 Socioeconomic Environment

5.8.1 Project Area Land Uses and Demographics

Previous studies completed by Earthplan (1995) evaluated the primary, secondary and cumulative social impacts of the Hawaii Convention Center project on a study area comprised of the Ala Moana-Kakaako, McCully-Moiliili and Waikiki communities. The three communities are characterized by a mixture of commercial, industrial, recreational, walk-up apartments, high density high rises and single family residential uses.

The McCully-Moiliili area consists of primarily low rise walk-up apartments, single family residences, and scattered high rise buildings. Commercial businesses tend to be of the smaller, neighborhood-oriented type with several exceptions near Kapiolani Boulevard and McCully Street, along Kalakaua Avenue and King Street.

A high density urban environment is particularly predominant in the Waikiki area near this proposed project. Residential high rise towers in the area are located on Ala Wai Boulevard, Atkinson Drive, Ala Moana Boulevard, Ena Road, Hobron Lane and Lipeepee Street. The densest collection of large hotels and visitor-related commercial activities in the State occurs along Kalakaua Avenue, between Ala Wai Boulevard and Kapahulu Avenue.

The Ala Moana-Kakaako area features several prominent areas and attractions to both residents and visitors alike. The Kakaako area continues to undergo revitalization, from the older industrial and commercial areas of the Mauka Area to the shoreside cargo,

recreational and commercial activities stretching from Kewalo Basin to Honolulu Harbor in the Makai Area. The Ala Moana Beach Park, the Ala Moana Shopping Center and the Neal Blaisdell Center are major gathering areas for recreational, cultural and commercial activities. Residential communities range from high rises and walk-up apartments between Kalakaua Avenue and Keamoku Street to low rise and single family residential dwellings further west towards Pensacola Street and the downtown area.

Demographically, the three communities totaled roughly 59,000 residents in 1990 (State of Hawaii Data Book 1993-94), nearly half (48%) residing in the McCully-Moiliili area. The population density in McCully-Moiliili was found to be the highest (48 residents/acre) versus Waikiki (40 residents/acre) and Ala Moana-Kakaako (12 resident/acre).

Long term impacts of the bridge project are expected to be beneficial to both the immediate local community and to visitors through enhanced pedestrian safety and improved accessibility for the disabled. In addition, the bridge by design will maintain significant public views, enhance public pedestrian access and pedestrian safety

5.9 Utilities, Transportation, and Services

5.9.1 Existing Utility Systems

Existing subsurface utilities on or near the existing bridge include sewer force mains, drainage infrastructure, waterlines, electrical and telephone, and cable TV. The City and County of Honolulu Department of Transportation Services has traffic signal light wiring through the Ala Wai Boulevard intersection. The pedestrian bridge project does not include substantial improvements to existing utilities. Some irrigation piping on the Kapiolani side of the bridge is expected to be relocated within the promenade area. In addition, several traffic signal boxes may need to be grade adjusted as a part of the Americans with Disabilities Act improvements at the Waikiki end of the new bridge. The appropriate City agencies will be contacted for plan review and approvals regarding utility adjustments. Existing subsurface and overhead utilities in the Kalakaua Avenue right-of-way are not expected to be impacted by this project.

5.9.2 Roadway Network

Major east-west roadways in the area include Beretania Street, King Street, Kapiolani Boulevard, Ala Wai Boulevard, Ala Moana Boulevard and Kuhio Avenue within the project vicinity. Major north-south thoroughfares include Keeamoku Street, Atkinson Drive and McCully Street. The project area is confined to Kalakaua Avenue between Kapiolani Boulevard and Ala Wai Boulevard, and by design and function will not significantly affect the levels of service of nearby thoroughfares in the long term.

Short term traffic impacts are not expected to be significant. However, the contractor will be required to follow or develop a suitable traffic control plan which accounts for normal daily traffic patterns and volumes, pedestrian and bicycle safety, passage of emergency vehicles, and provides contingency options should unanticipated traffic patterns develop due to large events in Waikiki. The traffic control plans will be coordinated with the City and County of Honolulu Department of Transportation Services.

5.9.3 Pedestrian Routes

Pedestrian routes exist along all of the major roadway systems cited in Section 5.8.3 above. Major pedestrian routes are expected to converge near either the Ala Moana Boulevard Bridge, the McCully Bridge or the proposed Kalakaua Avenue pedestrian bridge. Although the bridge is not expected to increase pedestrian volumes, the enhancement of the existing walking conditions with a wider, separated pedestrian travelway will beneficially impact walking routes through this area.

This project's benefits will be most apparent during the larger events being held at the new Hawaii Convention Center. Pedestrian traffic analyses done for the Hawaii Convention Center EIS estimated that the percentage of walking conferees during events in the 10,000-person to 14,000-person range could range between 10 percent for the larger event to 35 percent for the smaller event. The morning peak hour volume for the smaller event conferees walking across the Kalakaua Bridge was estimated to be 970 pedestrians, resulting in undesirable estimated Levels of Service between D and E at the Waikiki and Convention Center ends of the existing bridge, respectively. The pedestrian

bridge improvements provide an increase in available walking width and moves pedestrian traffic away from the immediate proximity of the automobile and bus traffic crossing the Kalakaua Avenue Bridge.

5.9.4 Bike Routes

The City is currently preparing an Urban Honolulu Bikeway System Master Plan which indicates a future bicycle facility crossing the Ala Wai Canal at the existing Kalakaua Avenue Bridge.

A portion of the City plan proposes a "Lei of Parks" bikeway connecting various parks between Aloha Tower and Diamond Head. Among the roadways utilized in this plan are Kalakaua Avenue, Ala Moana Boulevard, Ala Wai Boulevard, Beretania Street and Monsarrat Avenue.

Bike travel along the proposed "Lei of Parks" bikeway crossing the existing Kalakaua Avenue Bridge currently shares vehicle lanes with motorized traffic. A bike lane, generally signed and striped for exclusive use by bicyclists, is not currently designated crossing the Kalakaua Avenue Bridge.

Travel upon the pedestrian bridge will be subject to City ordinances similar to other sidewalks in the Waikiki Special District. At this time, the City ordinances limit travel upon Waikiki sidewalks to pedestrian traffic including wheelchairs. Although bicycle traffic is not currently allowed on the Waikiki sidewalks, bicyclists are able to share use of the Waikiki roadways with other vehicles.

5.9.5 Police, Fire & Emergency Services

The project area is confined to Kalakaua Avenue between Kapiolani Boulevard and Ala Wai Boulevard, and by design and function will not significantly affect the level of police, fire or emergency services going over the bridge. The appropriate service agencies have been contacted for their concurrence in this matter.

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6 ALTERNATIVES TO THE PROPOSED ACTION

6.1 *No Action*

The No Action Alternative would not improve pedestrian safety and access crossing the Ala Wai Canal on the existing Kalakaua Avenue Bridge . Pedestrians would continue to utilize the existing 6.5-foot wide sidewalk adjacent to automobile and bus traffic. The current steep grade and cross slope at the southwest corner of the Kalakaua Avenue-Ala Wai Boulevard intersection would need to be remediated by another project.

6.2 *Delayed Action*

Delay in the construction of the proposed bridge would continue the potential for accidents involving injuries to pedestrians struck by automobile or bus traffic.

6.3 *Bridge Location Alternatives*

Bridge location alternatives were evaluated with the intent of providing improved access and pedestrian safety for existing and future pedestrian volumes crossing over the Ala Wai Canal on Kalakaua Avenue. Canal crossings at Ala Moana Boulevard and McCully Street were considered too circuitous a route to get to the Kapiolani Boulevard side of Kalakaua Avenue. A pedestrian bridge over the canal between Ala Moana Boulevard and Kalakaua Avenue, roughly across from the Lipeepee Street-Ala Wai Boulevard intersection, was discussed in the Hawaii Convention Center Final Environmental Impact Statement. This alternative was put off for further study or until the Hawaii Convention Center needs required one and funds could be appropriated. Ultimately, none of the evaluated alternatives provided a solution which enhanced pedestrian safety for existing and future pedestrian volumes crossing over the Ala Wai Canal on Kalakaua Avenue.

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REFERENCES

REFERENCES

1. R.M. Towill Corporation. *Environmental Assessment for the McCully/Waikiki 30-Inch Water Main*. Prepared for Board of Water Supply, City and County of Honolulu. December 1991.
2. Nordic/PCL (Wilson Okamoto & Associates). *Final Environmental Impact Statement, Volume I, II & III*. Prepared for Convention Center Authority, State of Hawaii. July 1995.
3. Wilbur Smith Associates. *Hawaii Convention Center Transportation Management Plan Study Final Report*. Prepared for Hawaii Convention Center (Spectacor Management Group). November 1997.
4. Department of General Planning, City and County of Honolulu. *The General Plan for the City and County of Honolulu*. 1992.
5. Department of Land Utilization, City and County of Honolulu. *Waikiki Special District Design Guidelines*. Undated.
6. Ala Wai Canal Watershed Water Quality Improvement Project Steering Committee. *Management and Implementation Plan, Volume I*. December 1997.
7. Belt Collins Hawaii & Sea Engineering Incorporated. *Ala Wai Canal Dredging Conceptual Design and Environmental Assessment – Feasibility Assessment (DRAFT)*. Prepared for City and County of Honolulu Department of Transportation Services. December 1997.
8. R.M. Towill Corporation. *Bike Plan Hawaii, A State of Hawaii Master Plan (Summary)*. Prepared for Highways Division – Department of Transportation, State of Hawaii. April 1994.
9. State of Hawaii Department of Health Clean Air Branch. *Hawaii Air Quality Data 1991-1993*. Undated.
10. Office of State Planning, Hawaii Coastal Zone Management Program, *Management Plan, Volume I*, June 1996.
11. Miller, Jacquelin N., *Ecological Studies of the Biota of the Ala Wai Canal*. Hawaii Institute of Marine Biology Technical Report No. 32. March 1975

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APPENDIX A
PROJECT PERMITTING CORRESPONDENCE

0000 0024 2464

***U.S. COAST GUARD 14TH DISTRICT BRIDGES & CAUSEWAYS
CORRESPONDENCE***

0000 0024 2465

U.S. Department
of Transportation

United States
Coast Guard



Commander
Fourteenth Coast Guard District

300 Ala Moana Boulevard
Honolulu, HI 96850-4982
Staff Symbol: oan
Phone: (808)541-2315
FAX: (808)541-2309

16590
Serial 32051
25 Feb 1998

Mr. Michael Miyahira
Akinaka & Associates, LTD
250 North Beretania Street
Suite 300
Honolulu, HI 96817-4716

RECEIVED

FEB 27 1998

AKINAKA & ASSOCIATES, LTD.

Dear Mr. Miyahira:

We have reviewed your letter of February 18, 1998, in which you are requesting a U. S. Coast Guard bridge permit to construct a pedestrian bridge across the Ala Wai Canal at Kalakaua Avenue, Honolulu, Hawaii.

Under 33 CFR part 115.70, the Commandant of the U. S. Coast Guard has given advanced approval to the locations and plans of bridges to be constructed across reaches of waterways navigable in law, but not actually navigated other than logs, log rafts, rowboats, canoes and small motorboats. A review of your site plans was conducted and is considered to meet the criteria for advanced approval. Therefore, no Coast Guard permit need be issued.

Thanks for taking the time to write and inform us.

Sincerely,

A handwritten signature in black ink that reads "T. D. Hooper".

T. D. HOOPER
Commander, U. S. Coast Guard
Chief, Aids to Navigation Branch
By direction

0000 0024 2466



AKINAKA & ASSOCIATES, LTD.
CONSULTING ENGINEERS
Civil Engineering • Land Planning

FILE COPY

250 NORTH BERETANIA STREET, SUITE 300, HONOLULU, HAWAII 96817-4716 • TELEPHONE (808) 536-7721 • FAX (808) 521-2153 • E-mail: akinaka@aloha.net

RYU Y. AKINAKA L.P.E.
RYO S. MORITA L.P.E.
ADON T. YAMASATO L.P.E.
DAEL M. MIYAHIRA L.P.E.
RYU K. MURANAKA L.P.E.

SALVADOR M. QUITORIANO L.P.E.
BENJAMIN M. GANAL L.P.E.
JOSEPH S. KEANE, JR.
MARIAN N. NAKAMA L.P.E.
CRAIG K. MATSUMOTO
STATE & INFANTE

February 18, 1998

Commander, Aids to Navigation
14th Coast Guard District
Prince Kuhio Federal Building
300 Ala Moana Boulevard, Rm 9139
Honolulu, Hawaii 96850-4982

Project: Kalakaua Pedestrian Bridge
TMK: 2-3-34
Waikiki, Oahu, Hawaii

Subject: Bridge and Causeway Permit Application
Title 33 Code of Federal Regulations, Parts 114 and 115
Reference: November 3, 1997 fax from OM1 Ed Lawrence

Applicant: State of Hawaii Department of Transportation
Highways Division
Mr. Pericles Manthos, Administrator
Contact: Edmund Yoshida

Authorized
Agent: Akinaka & Associates, Ltd.

Dear Petty Officer Lawrence:

The State Department of Transportation – Highways Division is proposing to construct a pedestrian bridge and sidewalk improvements abutting the existing Kalakaua Bridge (TMK: 2-3-34), between Kapiolani Boulevard and Ala Wai Boulevard in Waikiki, Oahu (see Exhibit 1).

Project Objectives:

The purpose of the pedestrian bridge is to 1) increase pedestrian safety over the bridge through better separation of the pedestrian/bicycle and vehicular traffic flow; 2) reconstruct the existing non-ADA conforming condition of the sidewalk at the Waikiki end of the bridge (Ala Wai Boulevard intersection); and 3) located at of five designated “gateways” or major entrances into Waikiki (Waikiki Special District Design Guidelines, Department of Land Utilization City & County of Honolulu) the bridge enhancements will provide the intended “... sense of arrival to a special place by providing coordinated open space, landscaping and architectural features.” In addition, the bridge by design maintains significant public views, protects the existing historic bridge structure, enhances public pedestrian access, and effectively blends function and

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Commander, Aids to Navigation
14th Coast Guard District
February 18, 1998
Page 2

pedestrian safety while linking both sides of the Waikiki (Ala Wai) Promenade as an important public space in the City's Waikiki Special District.

The existing bridge is located on Kalakaua Avenue, between Kapiolani Boulevard and Ala Wai Boulevard. The existing 56-foot wide travelway across the bridge consists of two Kapiolani-bound lanes (expanding to three on the Kapiolani Boulevard end) and three Waikiki-bound lanes. The 180-foot long, three-span bridge, constructed in 1929, is currently on the State's Register of Historic Places and will require State Historic Preservation Division review and concurrence for any improvements made to it.

Project Description:

The proposed Kalakaua Pedestrian Bridge project will include a three-span, concrete pedestrian bridge abutting and reflecting in profile, length, size and architectural detail the existing historic bridge as shown in Exhibits 2 and 3. This new construction does not affect the structural integrity of the existing bridge since the new pedestrian bridge will be an independent, free-standing structure separated by a non-structural expansion joint. The separation of pedestrians from vehicular traffic through the use of the existing bridge railing is designed to be an improvement over the existing condition. Grade and slope adjustment improvements to the existing sidewalk at the Ala Wai Boulevard end of the new bridge will be designed to remedy the current non-conforming ADA conditions. Miscellaneous improvements include walkway lighting and landscaping.

The bridge will be supported by new abutments on each end of the bridge and be pile supported in two location in the middle. A detailed description of this in-water project work is provided as plan Exhibit 4 with Text Attachments.

The preliminary project schedule is provided as Exhibit 5.

Alternatives to the Proposed Project:

The following project alternatives are being evaluated in an Environmental Assessment being prepared in accordance with Chapter 343 HRS and Title 11 Chapter 200 HAR. The Draft EA is expected to be published in the OEQC's Environmental Notice in late-March 1998.

No Action

The No Action Alternative would not satisfy the proposed project's purpose of improving pedestrian safety and access over the Ala Wai Canal on Kalakaua Avenue. Pedestrians would continue to utilize the existing 8-foot wide sidewalk adjacent to five busy lanes in Kalakaua Avenue, at the gateway to Waikiki. The opportunity to remediate the current steep grade and cross slope at the mauka-ewa corner of the Kalakaua Avenue/Ala Wai Boulevard would be lost, thus leaving the City vulnerable to legal action due to non-ADA conforming sidewalk.

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Commander, Aids to Navigation
14th Coast Guard District
February 18, 1998
Page 3

Delayed Action

A delay in the construction of the proposed bridge would increase the possibility of a pedestrian accident occurring over the existing bridge sidewalk and at the Waikiki end of the bridge. This is due to the larger volume of expected foot traffic across the existing bridge as the first convention events open. The longer the bridge is delayed while the convention center is open, the longer this larger volume of pedestrian traffic is exposed to the current condition on the bridge (see non-ADA conforming discussion above).

Bridge Relocation Alternatives

Bridge location alternatives were evaluated with the intent of providing improved access and pedestrian safety for existing and future pedestrian volumes crossing over the Ala Wai Canal on Kalakaua Avenue. Canal crossings at Ala Moana Boulevard and McCully Street are considered too circuitous a route to get to the Kapiolani Boulevard side of Kalakaua Avenue. A pedestrian bridge over the canal between Ala Moana Boulevard and Kalakaua Avenue, roughly across from the Lipepee Street/Ala Wai Boulevard intersection, was discussed in the Hawaii Convention Center (HCC) FEIS. This alternative was put off for further study or until HCC needs required one and funds could be appropriated. Ultimately, none of the evaluated alternatives provided a logical solution which enhanced pedestrian safety for existing and future pedestrian volumes crossing over the Ala Wai Canal on Kalakaua Avenue.

Project Impacts

The project concerns are expected to include construction noise, dust, traffic delays, and water quality impacts. Water recreation activities may be impacted during certain portions of the construction period. Appropriate mitigation measures and project alternatives will be evaluated. In addition, the acquisition of the following Federal, State and County permits and approvals will ensure a thorough evaluation of temporary (construction methods) and permanent impacts to water quality, recreational uses and traffic safety:

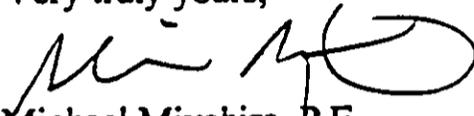
- U.S. Army Corps of Engineers, Nationwide Permit #3 Structures in Artificial Canals and Nationwide Permit #25 Structural Discharges;
- State of Hawaii Department of Health Clean Water Branch, Section 401 Water Quality Certification and NPDES General Permit for Construction Dewatering;
- State of Hawaii Department of Land and Natural Resources Commission on Water Resource Management, Stream Channel Alteration Permit;
- City and County of Honolulu Department of Land Utilization, Flood Fringe Certification;
- Plan approval from City and County of Honolulu Department of Public Works and Department of Transportation Services.

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Commander, Aids to Navigation
14th Coast Guard District
February 18, 1998
Page 4

Thank you for your prompt attention to this permit request. If you have any questions concerning this letter, please call me at 536-7721.

Very truly yours,



Michael Miyahira, P.E.
Akinaka & Associates, Ltd.

cc: Ed Yoshida, HDOT-DS
Norman Nagamine, Nagamine Engineers Inc.

Attachments

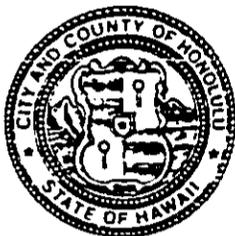
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***CITY AND COUNTY OF HONOLULU
DEPARTMENT OF LAND UTILIZATION
FLOOD FRINGE CERTIFICATION (Waikiki Special District Permit)
CORRESPONDENCE***

0000 0024 2471

DEPARTMENT OF LAND UTILIZATION
CITY AND COUNTY OF HONOLULU

650 SOUTH KING STREET, 7TH FLOOR • HONOLULU, HAWAII 96813
PHONE: (808) 523-4414 • FAX: (808) 527-6743



March 2, 1998

JAN NAOE SULLIVAN
DIRECTOR

LORETTA K.C. CHEE
DEPUTY DIRECTOR

1998--Flood Misc. (MS)
98-01123

RECEIVED

MAR 3 1998

AKINAKA & ASSOCIATES, LTD

Mr. Robert Y. Akinaka, President
Akinaka & Associates, Ltd.
250 North Beretania Street, Suite 300
Honolulu, Hawaii 96817-4716

Dear Mr. Akinaka:

Proposed Kalakaua Avenue Pedestrian Bridge
Ala Wai Canal, Waikiki, Oahu, Hawaii
Owner: State Department of Transportation
Tax Map Key Plats: 2-3-35 & 2-6-13

This is in response to your letter dated February 10, 1998, regarding the flood requirements for this project which entails the construction of a new concrete pedestrian bridge downstream of the existing Kalakaua Avenue Bridge. We understand that the project will abut, but be structurally independent from the existing bridge, as new piles will be driven into the canal bottom to support it.

The Federal Flood Insurance Rate Map (FIRM) Panel No. 150001-0120-C, dated September 4, 1987, indicates that the project will be within a Flood Fringe District identified as Zone AO (Depth 2'), a special flood hazard area subject to shallow flooding. Although Section 7.10-13(a)(11) of the Land Use Ordinance (LUO) exempts bridges from the standards of the flood hazard districts (except from requirement of the floodway district that the improvements do not cause any increase in the regulatory flood elevation), this is only in recognition that the standards are not appropriate for bridges, which are professionally designed structures intended for the most part to be located into and over the water.

The proposed pedestrian bridge will encroach into the Ala Wai Canal, a waterway that drains the Palolo, Manoa and Makiki Streams away from Waikiki and into the ocean. The portion of the Ala Wai Canal at the project site is not designated in the FIRM as a regulatory floodway. However, the federal regulations under the National Flood Insurance Program (NFIP) require in special flood hazard areas where the FIRM does not identify a regulatory floodway, that the flood carrying capacity within the altered or relocated portion of any watercourse be maintained.

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Mr. Robert Y. Akinaka
1998--Flood Misc. (MS)
Page 2

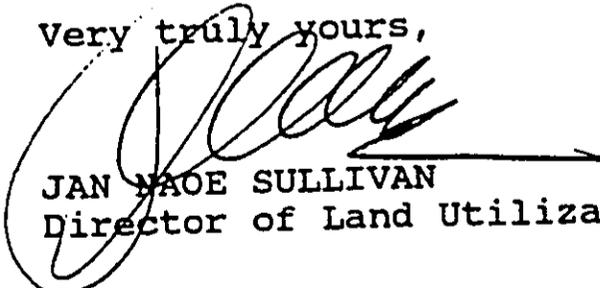
Therefore, to comply with NFIP requirements for this project, we will require that you submit adequate documentation by studies and data demonstrating to the best available technical knowledge and information, that the flood carrying capacity of the waterway will be maintained. Upon receipt of the aforementioned documents, they will be transmitted to the City Department of Public Works for their review and comments.

Be aware that before proceeding with construction, this project may be subject to compliance with some or all of the following:

1. Waikiki Special District requirements, pursuant to Section 7.80 of the Land Use Ordinance. For questions regarding Waikiki Special District permits, please contact Mr. Patrick Seguirant of our Urban Design Branch at 527-5369.
2. Stream Channel Alteration Permit (SCAP), issued by the Commission on Water Resource Management, State Department of Land and Natural Resources (DLNR).
3. U.S. Army Corps of Engineers' Nationwide Permit Program for work within waters of the United States.
4. Environmental assessment requirement of Chapter 343, Hawaii Revised Statutes, which applies to a State project. Documentation shall be provided to our department of the Chapter 343 determination for this project.

The above listing is not intended to be comprehensive. This project may require other federal, state and local approvals and permits not identifiable at this time. Should you have any questions on this letter, please contact Mr. Mario Siu-Li of our staff at 523-4247.

Very truly yours,


JAN MOE SULLIVAN
Director of Land Utilization

JNS:ky
(pedbridge.msl)
cc: Department of Public Works
Urban Design Branch

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AKINAKA & ASSOCIATES, LTD.

CONSULTING ENGINEERS
Civil Engineering • Land Planning

250 NORTH BERETANIA STREET, SUITE 300, HONOLULU, HAWAII 96817-4716 • TELEPHONE (808) 536-7721 • FAX (808) 521-2153 • E-mail: akinaka@aloha.net

BERT Y. AKINAKA, L.P.E.
NRY S. MORITA, L.P.E.
ELDON T. YAMASATO, L.P.E.
CHAE L. M. MIYAHIRA, L.P.E.
ERRY K. MURANAKA, L.P.E.

SALVADOR M. QUITORIANO, L.P.E.
BENJAMIN M. GANAL, L.P.E.
JOSEPH S. KEANE, JR.
MARIAN N. NAKAMA, L.P.E.
CRAIG K. MATSUMOTO
STATE A. INFANTE

February 10, 1998

Jan Naoe T. Sullivan, Director
Department of Land Utilization
650 South King Street
Honolulu, Hawaii 96813

Project: Kalakaua Pedestrian Bridge
TMK: 1-1-63: 02
Waikiki, Oahu, Hawaii

Owner: State of Hawaii
Department of Transportation
Highways Division

Permitting
Consultant: Akinaka & Associates, Ltd.
Michael Miyahira, P.E.

Subject: Request for Flood Hazard District (Flood Fringe) Determination

Dear Ms. Sullivan:

In accordance with Article 7 Special District Regulations, Section 7.10 Flood Hazard Districts, we are requesting a Flood Fringe District determination from the City based on the information provided in the enclosed Flood Fringe District Certification application and the enclosed project exhibits. Additionally, we are requesting consideration of the project as an "exempt" improvement under Section 7.10-13 Exemptions, (c)(11).

The Kalakaua Pedestrian Bridge project is being designed to improve pedestrian access and safety on Kalakaua Avenue as it spans the Ala Wai Canal. The proposed bridge will include a three-span, concrete pedestrian bridge abutting and matching in profile, length, size and architectural detail the existing historic bridge. The proposed bridge will, however, be structurally independent. Sidewalk improvements will be made at the mauka-ewa corner of the Kalakaua Avenue/Ala Wai Boulevard intersection to conform to ADA standards.

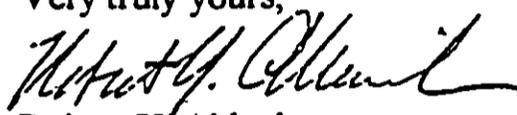
The proposed bridge will sit on piles driven into the canal bottom. Matching columns and pile caps will provide a smooth, uninterrupted transition to the existing piers from the canal bottom to above the water line.

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Jan Naoe T. Sullivan, Director
February 10, 1998
Page 2

If you have any questions concerning this letter, please call the undersigned or Michael Miyahira at 536-7721.

Very truly yours,



Robert Y. Akinaka
President

Attachments:

Flood Fringe District Certification Application
Project Exhibits

FLOODAPP.DOC

0000 0024 2475

FLOOD FRINGE DISTRICT CERTIFICATION
(Pursuant to Section 7.10 of the Land Use Ordinance)

Projects, Developments and Substantial Improvements

Project Description: KALAKAUA AVENUE PEDESTRIAN BRIDGE

Address: KALAKAUA AVENUE

City: HONOLULU State: HAWAII Zip: _____

Tax Map Key: 2-3-34

Section I - Flood Insurance Rate Map Information

COMMUNITY NO.	PANEL NO	SUFFIX	DATE OF FIRM	FIRM ZONE	REGULATORY FLOOD ELEV (in AO Zone use depth)	COMMUNITY ESTIMATED REG. FLOOD ELEVATION ESTABLISHED FOR ZONE A IF AVAILABLE
150001	0120	C	9/4/87	AO	2	N/A

Section II - Elevation Information

Elevation of Lowest Floor.....	<u>N/A</u>	ft.
Regulatory Flood Elevation.....	<u>2</u>	ft.
Depth Number.....	<u>2</u>	ft.
Elevation of Highest Adjacent Grade.....	<u>11.99</u>	ft.
Elevation of Lowest Adjacent Grade.....	<u>6.34</u>	ft.
Elevation to which Structures are Floodproofed.....	<u>N/A</u>	ft.

Section III - Standard Certification Statement

plans, specifications and methods of construction for the proposed project are in accordance with accepted standards of practice for meeting the provisions of the Flood Hazard Districts, and:

comply with the standards and requirements of the Flood Hazard District Regulations of the Land Use Ordinance;

conform to the flood elevations of the Federal Emergency Management Agency Flood Insurance Rate Maps (FIRM); and

are adequate to resist the regulatory flood forces; do not adversely increase flood elevations; and do not adversely affect flooding on surrounding properties;

Section IV - AE, AH, AO and A Zones Certification Statement

certify that based upon development and/or review of structural design, specifications, plans for construction that the design and methods of construction are in accordance with accepted standards of practice for meeting the following provisions:

Residential structures (a) in AE and AH Zones, have the lowest floor (including basement) elevated to or above the regulatory flood level; and (b) in AO Zone have the lowest floor (including basement) elevated above the highest adjacent grade at least as high as the depth number specified in feet on the FIRM;

Non-residential structures (a) in AE and AH Zones, have the lowest floor (including basement) elevated to or above the regulatory base flood level; and in AO Zone have the lowest floor (including basement) elevated above the highest adjacent grade at least as high as the depth number; or, (b) together with attendant utility and sanitary facilities, is designed so that below the regulatory flood level the structure is watertight with walls substantially impermeable to the passage of water and with structural components having the capability of resisting hydrostatic and hydrodynamic loads and effects of buoyancy; and

0000 0024 2476

3.) Fully enclosed areas below the regulatory flood level shall be designed to automatically equalize hydrostatic flood forces on exterior walls by allowing for the entry and exit of floodwaters.

Section V - Certification

Project plans and specifications include:

- 1) the location of flood hazard boundaries;
- 2) existing and proposed elevations of the property in relation to the elevation reference marks on the Federal Flood Maps;
- 3) the flood elevations, velocity and other data from the Federal Flood Maps and study;
- 4) existing and proposed structures, utilities and improvements; and
- 5) proposed flood proofing measures and improvements.

This certification is conditioned upon the actual construction of the project being in strict accordance with the plans and specifications as stamped and signed by me.

Certifier's Name MICHAEL M. MIYAHIRA
 (print or type)

Title ASSISTANT VICE PRESIDENT

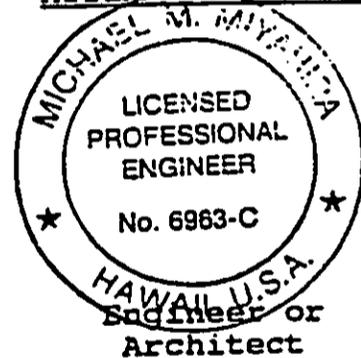
Company Name AKINAKA & ASSOCIATES, LTD.

Street Address 250 NO BERETANIA ST, SUITE 300

City HONOLULU State HI Zip 96817

Signature *[Handwritten Signature]* Date 2/9/98

Affix Seal Below



0000 0024 2478



AKINAKA & ASSOCIATES, LTD.
CONSULTING ENGINEERS
Civil Engineering • Land Planning

250 NORTH BERETANIA STREET, SUITE 300, HONOLULU, HAWAII 96817-4716 • TELEPHONE (808) 536-7721 • FAX (808) 521-2153 • E-mail: akinaka@aloha.net

ERT Y AKINAKA L.P.E.
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BENJAMIN M GANAL L.P.E.
JOSEPH S KEANE L.P.
MARIAN N NAKAMA L.P.E.
CRAIG K MATSUO L.P.E.
STATE A INFINITE

March 9, 1998

Mr. Jonathan K. Shimada, Ph.D.
Director and Chief Engineer
Department of Public Works
City & County of Honolulu
650 South King Street, 11th Floor
Honolulu, Hawaii 96813

Project: Kalakaua Pedestrian Bridge
TMK: 1-1-63: 02
Waikiki, Oahu, Hawaii

Owner: State of Hawaii
Department of Transportation
Highways Division

Subject: DLU Flood Hazard District
(Flood Fringe) Determination Request,
Ref: DLU Response Letter dated March 2, 1998

Attention: Mr. Melvin Takakura,
Chief Drainage Engineer
Division of Engineering

Dear Dr. Shimada:

We are requesting the Department's concurrence with the State's proposed project and DLU Flood Hazard District (Flood Fringe) Determination application, dated February 10, 1998 (DLU response letter dated March 2, 1998 enclosed).

The Kalakaua Pedestrian Bridge project is being designed to improve pedestrian access and safety on Kalakaua Avenue as it spans the Ala Wai Canal (see Exhibit 1). The proposed bridge will include a three-span, concrete pedestrian bridge abutting and matching in profile, length, size and architectural detail the existing historic bridge, as shown in Exhibits 2 and 3. The proposed bridge will, however, be structurally independent.

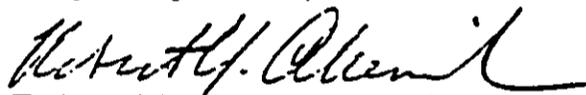
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Dr. Jonathan K. Shimada, Ph.D.
Director & Chief Engineer
March 9, 1998
Page 2

The bridge structure will sit on piles driven into the canal bottom. Matching pile caps, partially submerged piers, columns, and abutment improvements will provide a smooth, uninterrupted transition to the existing piers and abutments from the canal bottom to above the water line. Based on this design, the flood carrying capacity within the altered portion of the watercourse is expected to be maintained. Your concurrence on this matter would be greatly appreciated.

If you have any questions concerning this letter, please call Michael Miyahira at 536-7721.

Very Truly Yours,

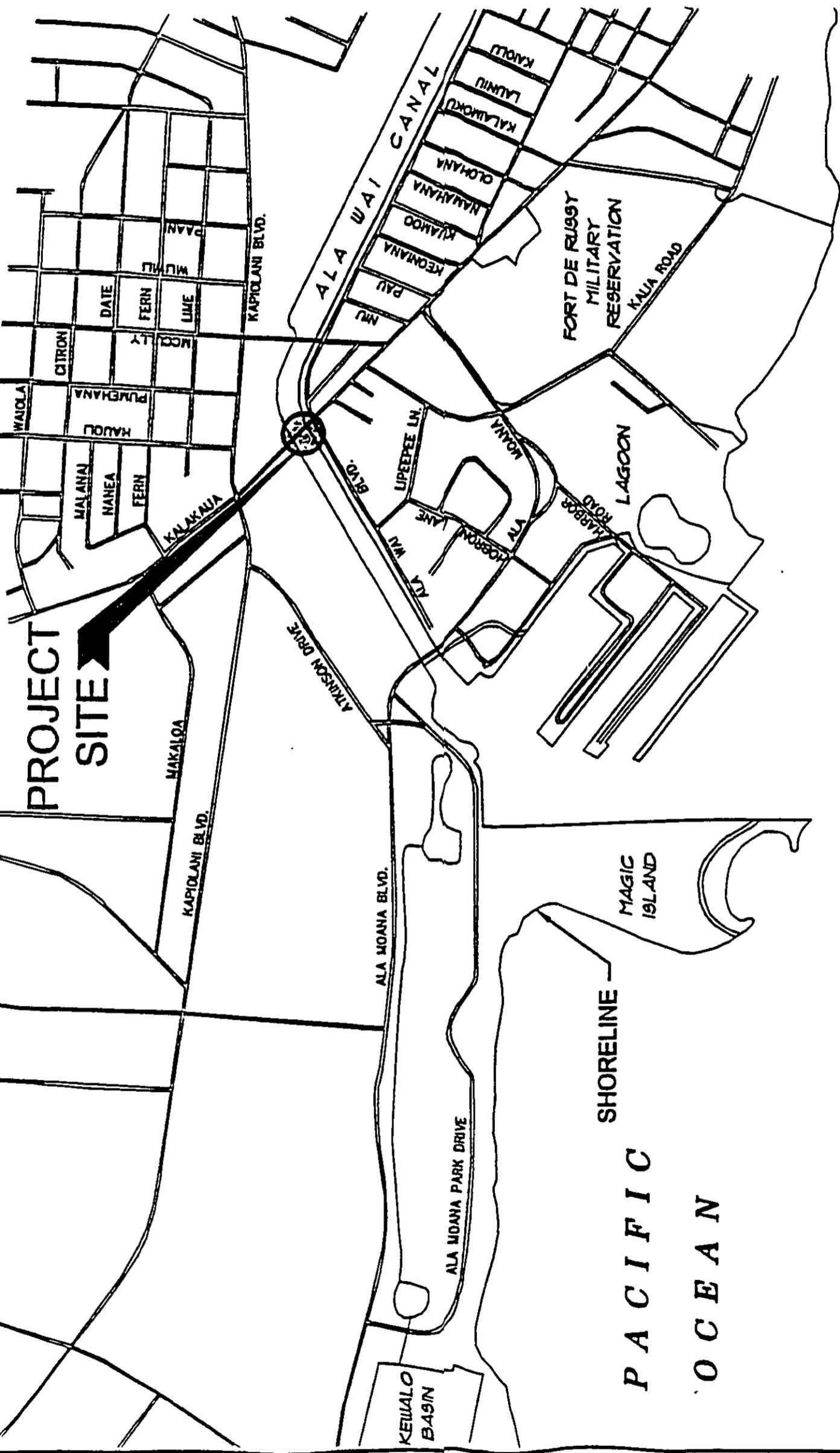


Robert Y. Akinaka, President
Akinaka & Associates, Ltd.

Attachments:

- Project Exhibits 1, 2 & 3
- Flood Fringe District Certification Application response letter from DLU, dated March 2, 1998

0000 0024 2480



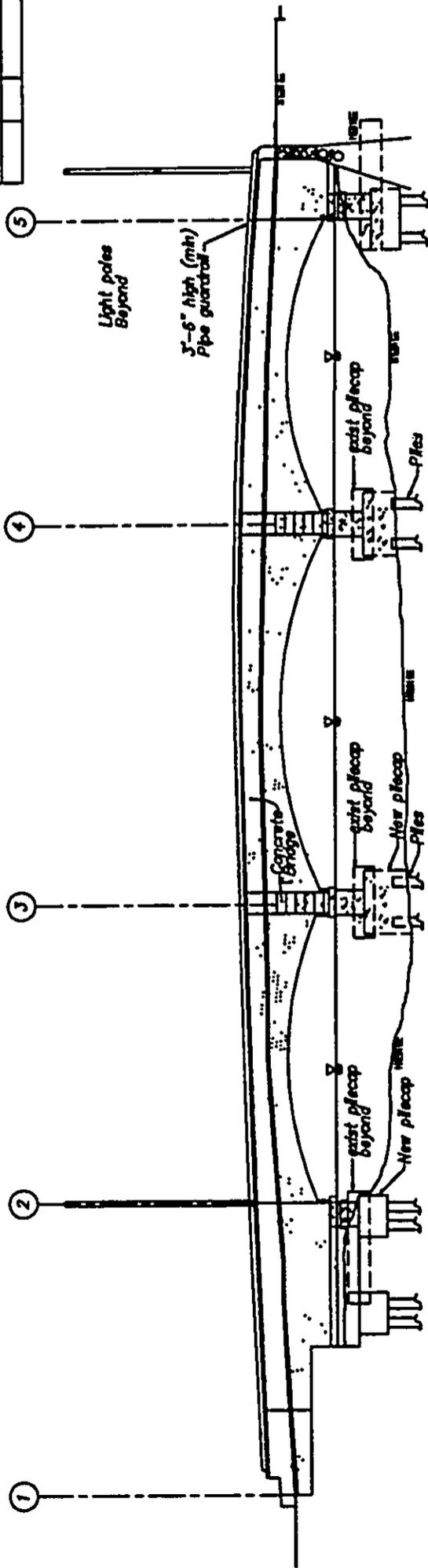
KALAKAUA PEDESTRIAN BRIDGE

EXHIBIT

LOCATION MAP

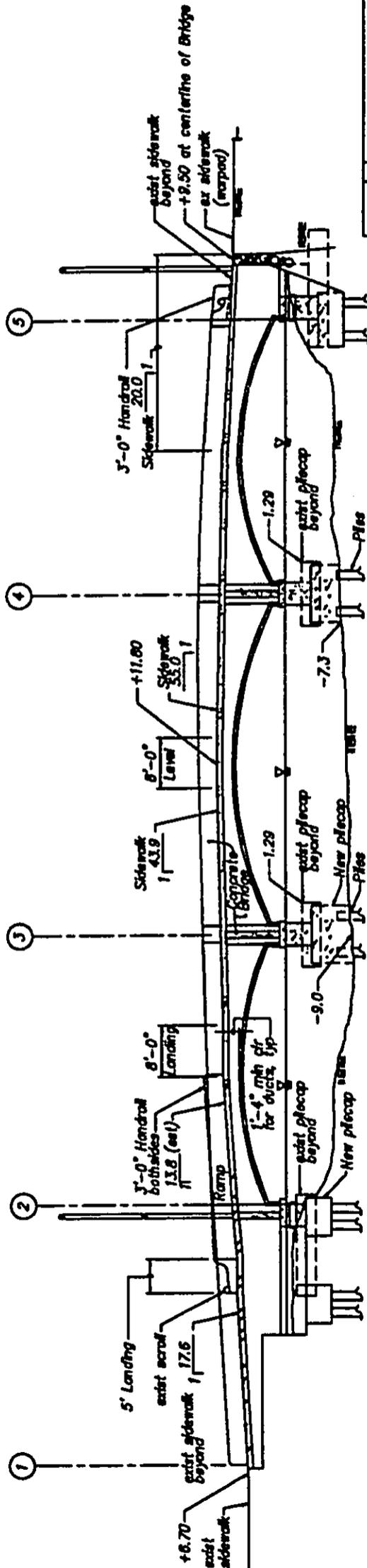
0000 0024 2482

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	TOTAL SHEET NO.	TOTAL SHEETS



1 PEDESTRIAN BRIDGE ELEVATION

SCALE: 1/8"=1'-0"



2 LONGITUDINAL SECTION

SCALE: 1/8"=1'-0"

DATE	DESIGNED BY	CHECKED BY
STATE OF HAWAII DEPARTMENT OF TRANSPORTATION HONOLULU, HAWAII		
KALAKAUA AVENUE BRIDGE IMPROVEMENTS		
Scale: As Shown		Date: Feb. 1998
SHEET No. OF		SHEETS

EXHIBIT 3

GRAPHIC SCALES:
0"=1' 4"=9' 8"=22'
1/8"=1'-0"

0000 0024 2483

UNITED STATES
DEPARTMENT OF THE ARMY NATIONWIDE PERMIT #25
CORRESPONDENCE

0000 0024 2484



REPLY TO
ATTENTION OF

DEPARTMENT OF THE ARMY
U. S. ARMY ENGINEER DISTRICT, HONOLULU
FORT SHAFTER, HAWAII 96858-5440

July 24, 1998

RECEIVED
JUL 23 2 22 PM '98
DEPT. OF TRANSPORTATION
HIGHWAYS DIVISION

Operations Branch

Mr. Pericle Manthos
Administrator, Highways Division
Department of Transportation
State of Hawaii
869 Punchbowl Street
Honolulu, Hawaii 96813-5097

RECEIVED
JUL 23 1998
DEPT. OF TRANSPORTATION
HIGHWAYS DIVISION

98 JUL 30 8:29

RECEIVED

Dear Mr. Manthos:

This is to confirm that Department of the Army (DA) permit authorization for File Number 980000107, for the Kalakaua Avenue Pedestrian Bridge, TMK 2-03-34, Honolulu, Hawaii became valid on July 13, 1998. This is the date that the Department of Health issued their determination regarding the Section 401 Water Quality Certification (WQC). The Office of Planning issued the Coastal Zone Management (CZM) Program federal consistency determination on May 20, 1998. In order to utilize this authorization, you must adhere to all the conditions described in the WQC, CZM and the terms and conditions of the nationwide permit involved.

This verification will remain valid until July 13, 2000. If work has commenced or if you are under a contract to commence work prior to the expiration date, you will have an additional 12 months to complete the activity authorized.

In addition, general condition no. 14 of the Nationwide permits requires a signed certification be submitted upon completion of work. Therefore, please sign, date and return the enclosed compliance certification upon completion of work.

Should you require additional information, please contact Ms. Lolly Silva of my staff at 438-9258, extension 17.

Sincerely,

George P. Young, P.E.
Chief, Operations Branch

Enclosure

0000 0024 2485

-2-

Copies furnished (w/o enclosure):

Clean Water Branch, Environmental Management Division,
State Department of Health, P.O. Box 3378, Honolulu, HI
96801-3386

Department of Business, Economic Development & Tourism,
Office of Planning, P.O. Box 2359, Honolulu, HI 96813

Department of Land and Natural Resources, State Historic
Preservation Division, 33 S. King Street, 6th Floor,
Honolulu, Hawaii 96813

U.S. Fish and Wildlife Service, Environmental Services,
P.O. Box 50088, Honolulu, Hawaii 96850

0000 0024 2486

COMPLIANCE CERTIFICATION

PERMIT NO. _____ DATE OF ISSUANCE _____

Name of Permittee _____

Upon completion of the activity authorized by this permit and any mitigation required by the permit, please sign this certification and return it to the following address:

U.S. Army Corps of Engineers
Honolulu District
Attn: Regulatory Section
Building 230
Fort Shafter, Hawaii 96858-5440

Please note that your permitted activity is subject to a compliance inspection by a U.S. Army Corps of Engineers representative. If you fail to comply with this permit, you are subject to permit suspension, modification or revocation.

I hereby certify that the work authorized by the above referenced permit has been completed in accordance with the terms and conditions of the said permit, and required mitigation was completed in accordance with the permit conditions.

Signature of Permittee

Date

0000 0024 2487



DEPARTMENT OF THE ARMY
U. S. ARMY ENGINEER DISTRICT, HONOLULU
FT. SHAFTER, HAWAII 96836-3440

REPLY TO
ATTENTION OF

April 28, 1998

Operations Branch

RECEIVED

MAY 5 1998

AKINAKA & ASSOCIATES, LTD.

Mr. Pericle Manthos, Administrator
State of Hawaii
Department of Transportation
869 Punchbowl Street
Honolulu, Hawaii 96813

Dear Mr. Manthos:

This is in response to your application for a Department of the Army (DA) permit for the construction of a pedestrian bridge along the existing Kalakaua Avenue Bridge located in Waikiki, Hawaii.

Based upon the information furnished, the proposed work will be authorized by the December 15, 1996 Federal Register, Final Notice of Issuance, Reissuance and Modification of Nationwide Permits (61 FR 65874), B.25, Structural Discharges. No further DA processing is necessary; however this authorization will not be valid until you have obtained the required Section 401 Water Quality Certification (WQC) from the State of Hawaii and a Coastal Zone Federal Consistency determination from the Office of Planning.

Until the above certification or waiver is granted, I am issuing a *Provisional Nationwide Permit* for the proposed work. Excerpts from the regulations that list the conditions of the Nationwide Permit (NWP) are provided for your information and compliance (Enclosure).

Furthermore, the State Historic Preservation Division has concurred that the existing Kalakaua Bridge meets the criteria for listing on the Hawaii and National Register of Historic Places and coordination has been completed with the Advisory Council on Historic Preservation. To ensure that the effects of the new construction will not be adverse, the following conditions are attached and made a part of the DA permit:

a. The new railing will duplicate the design of the existing Kalakaua Avenue Bridge;

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-2-

b. Photo documentation of the finished construction will be submitted by the Permittee to the Corps and State Historic Preservation Office for record purposes.

In addition to these conditions, you are advised of the following:

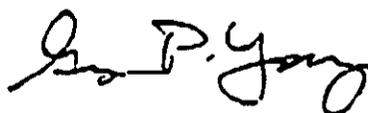
- a. Nationwide permits do not obviate the need to obtain other Federal, State or local authorizations required by law.
- b. Nationwide permits do not grant any property rights or exclusive privileges.
- c. Nationwide permits do not authorize any injury to the property or rights of others, nor any interference with any existing or proposed Federal projects.

Upon issuance of the certification or waiver from the State, this authorization will take effect from the issuance date and will remain valid for two years, unless the NWP authorization is modified, suspended or revoked. If during this two-year period the NWP authorization is reissued without modification or if the activity complies with any subsequent modification of the NWP authorization, this authorization will continue to remain valid for the two-year period. However, if during this two-year period, the NWP authorization expires, is suspended, or revoked, or is modified that the activity would no longer comply with the terms and conditions of the NWP, the provisions of 33 CFR Part 330, Section 330.6(b) will apply.

If the State denies the WQC and CZM determination for this project, then this NWP will be denied without prejudice.

If you have any questions, please call Ms. Lolly Silva of my staff at 438-9258, extension 17. Please refer to File Number 98C000107 on any future correspondence regarding this matter.

Sincerely,



George P. Young, P.E.
Chief, Operations Branch

0000 0024 2489

-3-

Enclosure

Copies Furnished (without enclosure):

Clean Water Branch, Environmental Management Division,
Hawaii State Department of Health, P.O. Box 3378,
Honolulu, Hawaii 96801-3386

U.S. Fish and Wildlife Service, Environmental Services,
300 Ala Moana Blvd., Rm. 3108, Honolulu, HI 96850

Department of Land and Natural Resources, State Historic
Preservation Division, 33 S. King Street, 6th Floor,
Honolulu, Hawaii 96813

Office of Planning, Department of Business, Economic
Development & Tourism, Coastal Zone Management Program,
P.O. Box 2359, Honolulu, Hawaii 96804

U.S. Environmental Protection Agency, Region IX, Honolulu
branch, P.O. Box 50003, Honolulu, Hawaii 96850

0000 0024 2490

AKINAKA & ASSOCIATES, LTD. - Consulting Engineers

250 North Beretania Street, Suite 300
Honolulu, Hawaii 96817-4716

FAX: (808) 521-2153
Phone: (808) 536-7721
E-mail: akinaka@aloha.n

Date: 4/21/98

To: District Engineer (PODCO-O)
U.S. Army Corps of Engineers
Building 230
Ft. Shafter, Hawaii 96858

Sent Via:

- Mail
- Delivery
- Pick-up
- DHL / Aloha Air
- FAX

Attention: Ms. Lolly Silva

Project: Kalakaua Pedestrian Bridge
Contract No.
A&A Job: NEI 98-01

Subject: Supplemental DA Permit information

We are sending you herewith:

No. of Copie	Description
1	Completed ENG FORM 4345, Sep 91 and accompanying Figures 1, 2 & 3

FOR: Information / Files Review / Comments Review / Approval Approval / Signature

ACTION Revise / Re-Submitt Per your request

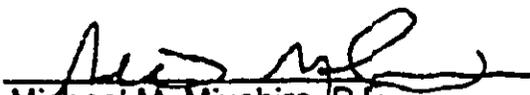
Remarks:

Lolly:

Submitting supplemental information per your request.

Let me know if you need anything else.

Very Truly Yours,


 Michael M. Miyahira, P.E.
 Project Manager

0000 0024 2491

APPLICATION FOR DEPARTMENT OF THE ARMY PERMIT
(33 CFR 325)

OMB APPROVAL NO. 0710-0003
Expires 30 September 1992

...ing burden for this collection of information is estimated to average 5 hours per response for the majority of cases, including the time for reviewing instructions, existing data sources, gathering and maintaining the data needed, and completing and reviewing the collection of information. Applications for larger or more complex or those in ecologically sensitive areas, could take up to 500 hours. Send comments regarding this burden estimate or any other aspect of this collection of information, suggestions for reducing this burden, to Department of Defense, Washington Headquarters Service, Directorate for Information Operations and Projects, 1215 Jefferson Highway, Suite 1204, Arlington, VA 22202-4302; and to the Office of Management and Budget, Paperwork Reduction Project (0710-0003), Washington, DC 20503. Please RETURN your completed form to either of these addresses. Completed application must be submitted to the District Engineer having jurisdiction over the of the proposed activity.

Department of the Army permit program is authorized by Section 10 of the Rivers and Harbors Act of 1899, Section 404 of the Clean Water Act and Section 103 of the Marine, Research and Sanctuaries Act. These laws require permits authorizing activities in or affecting navigable waters of the United States, the discharge of dredged or fill into waters of the United States, and the transportation of dredged material for the purpose of dumping it into ocean waters. Information provided on this form will be used in making the application for a permit. Information in this application is made a matter of public record through issuance of a public notice. Disclosure of the information provided is voluntary; however, the data requested are necessary in order to communicate with the applicant and to evaluate the permit application. If necessary information is not provided, the permit application cannot be processed nor can a permit be issued.

...of original drawings or good reproducible copies which show the location and character of the proposed activity must be attached to this application (see sample drawings and instructions) and be submitted to the District Engineer having jurisdiction over the location of the proposed activity. An application that is not completed in full will be returned.

APPLICATION NUMBER (To be assigned by Corps)

FILE COPY

NAME AND ADDRESS OF APPLICANT

State of Hawaii
Department of Transportation
Highways Division
Charles Manthos, Administrator
Telephone no. during business hours

() CONTACT: Edmund Yoshida (Residence)
() 587-2122 (Office)

3. NAME, ADDRESS, AND TITLE OF AUTHORIZED AGENT

N/A

Telephone no. during business hours

AC () _____ (Residence)
AC () _____ (Office)

Statement of Authorization: I hereby designate and authorize _____ to act in my behalf as my agent in the processing of this permit application and to furnish, upon request, supplemental information in support of the application.

SIGNATURE OF APPLICANT	DATE
------------------------	------

DETAILED DESCRIPTION OF PROPOSED ACTIVITY

ACTIVITY

The State Department of Transportation - Highways Division (Proposing Agency/Approving Agency) is proposing to construct a 180-foot long pedestrian bridge and sidewalk improvements (State Project No. HWY-0-13-98) abutting the existing Kalakaua Avenue Bridge, currently a City and County of Honolulu right-of-way between Kapiolani Boulevard and Ala Wai Boulevard, as shown in Figures 1 and 2.

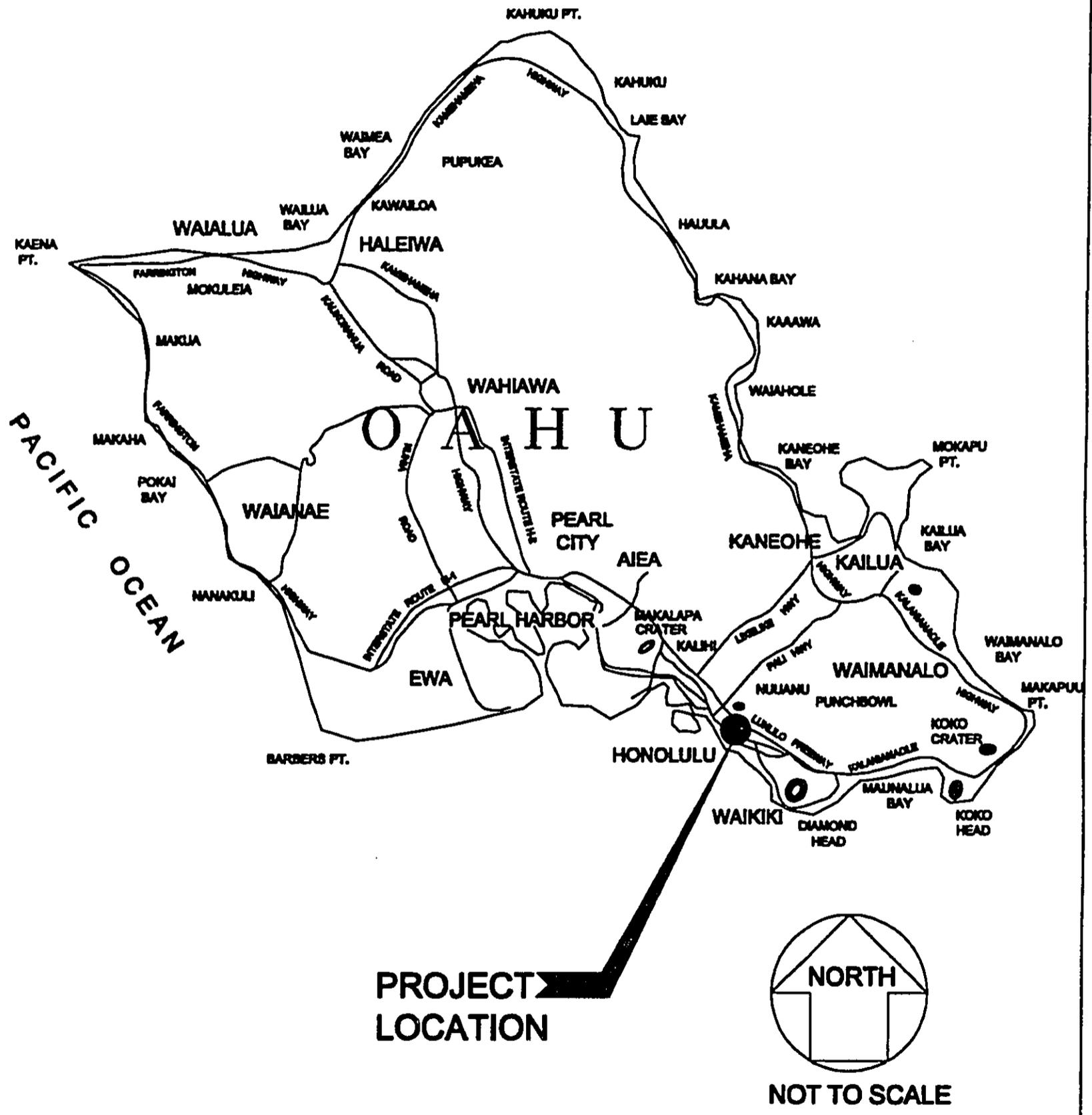
PURPOSE

The purpose of the pedestrian bridge is to 1) increase pedestrian safety over the bridge through better separation of the pedestrian/bicycle and vehicular traffic flow; 2) reconstruct the existing non-ADA conforming condition of the sidewalk at the Waikiki end of the bridge (Ala Wai Boulevard intersection, as shown in Figure 3.

CHARGE OF DREDGED OR FILL MATERIAL

The "discharge activity" which triggers the Federal DA Nationwide Permit #3 Structures in Artificial Canals and NWP#25 Structural Discharges, consists primarily of the placement of sheet piling, tremie concrete and cast-in-place concrete structures in the Ala Wai Canal.

0000 0024 2492



KALAKAUA AVENUE BRIDGE IMPROVEMENTS

FIGURE

ISLAND MAP

1

0000 0024 2495



AKINAKA & ASSOCIATES, LTD.
CONSULTING ENGINEERS
Civil Engineering • Land Planning

250 NORTH BERETANIA STREET, SUITE 300, HONOLULU, HAWAII 96817-4716 • TELEPHONE (808) 536-7721 • FAX (808) 521-2153 • E-mail: akinaka@aloha.net

ROBERT Y. AKINAKA, L.P.E.
HENRY S. MORITA, L.P.E.
HELDON T. YAMASATO, L.P.E.
MICHAEL M. MIYAHIRA, L.P.E.
BARRY K. MURANAKA, L.P.E.

SALVADOR M. QUITCRIANO, L.P.E.
BENJAMIN M. GANAL, L.P.E.
JOSEPH S. KEENE, L.P.E.
MARIAN N. YAKAMA, L.P.E.
CRAIG K. WATSON, L.P.E.
STATE & COUNTY

January 15, 1998

District Engineer (PODCO-O)
U.S. Army Corps of Engineers
Building 230
Ft. Shafter, HI 96858

Attention: Ms. Lolly Silva,
Environmental Protection Specialist
Regulatory Section

Subject: Request for Determination of DA Permit Applicability
Kalakaua Pedestrian Bridge, Waikiki, Oahu, Hawaii (TMK: 2-3-34)
State of Hawaii, Department of Transportation, Highways Division
Consultant: Norman Nagamine Engineers
Civil/Environmental Consultant: Akinaka & Associates, Ltd.

Dear Ms. Silva:

On behalf of the State of Hawaii Department of Transportation Highways Division, we are requesting a determination of our subject project's applicability to DA Section 404 requirements.

A foundation construction plan is included as Exhibit 1.

A detailed description of the pedestrian bridge construction methods and sequencing is included in Exhibit 2.

Based on Exhibits 1 and 2, and our 01/05/98 meeting with your staff, we have the following understanding of the DA permitting requirements for this project:

The placement of pilings for the bridge project does not constitute a discharge of fill material. Therefore, a Section 10 Permit is applicable, not a Section 404 Permit. A Section 10 Letter of Permission (LOP) would be issued for such an action. An application needs to be processed on the appropriate Eng Form 4345

0000 0024 2496

Ms. Lolly Silva
January 15, 1998
Page 2

The construction of a cast-in-place pile cap on top of the piles does constitute a discharge of fill material based on the use of concrete and sheet piling during construction. The total pile cap volume (concrete) is expected to be on the order of 90 cubic yards. Therefore, the action does not qualify under Nationwide Permit (NWP) #18 Minor Discharges (<25 CY). Rather, Nationwide Permit #25 Structural Discharges may be more applicable. NWP #25 comes with the following permit provisions:

- Pre-Construction Notification is not required;
- Federal Consistency Determination (CZM) required;
- Blanket Section 401 Certification is expected to be provided by DOH Clean Water Branch. A BMP Plan and Water Quality Monitoring Plan may be required by DOH.

Soil boring work to be completed by the soils engineers may require a NWP #6 Survey Activities permit. NWP #6 does not require a CZM, and a Blanket Section 401 WQC has been issued by the DOH Clean Water Branch. For permit processing, a BMP Plan is submitted to the DOH through the USACOE first. This permit is expected to be processed by the soils consultant.

The use of a floating platform for taking soil borings or pile driving does not trigger a DA permit.

In summary, overall permit processing requirements by A&A may include a Section 10 Permit for the pile-supported structure and a NWP #25 Structural Discharges for the pile cap/sheet pile work. USACOE will require just one application to issue both permits (application form not normally needed for NWPs).

Your comments and determination for this project are humbly requested. If you have any questions concerning this request, please call the undersigned or Michael Miyahira at 536-7721.

Very Truly Yours,

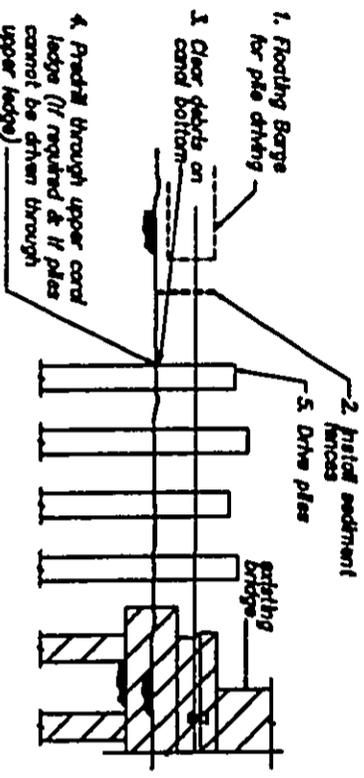
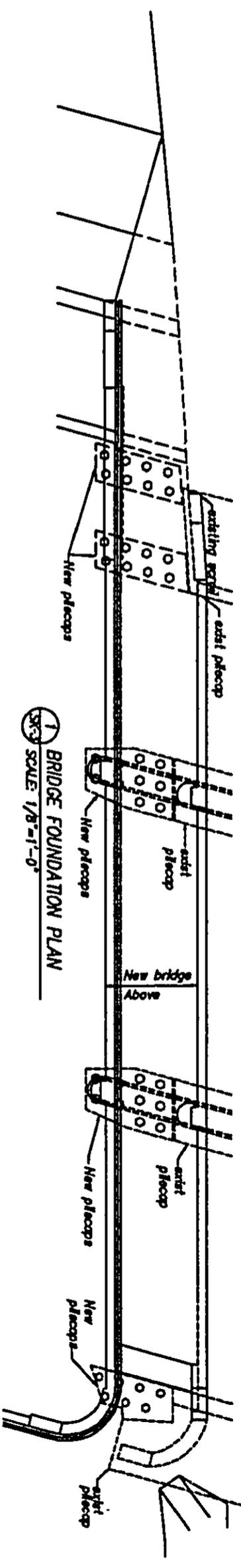


Robert Y. Akinaka
President

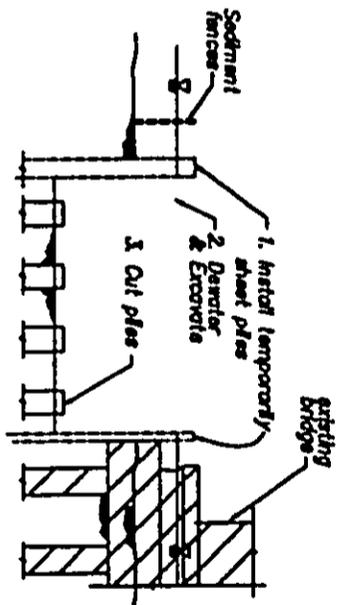
ATTACHMENTS:

- Exhibit 1 - Foundation Construction Plan, January 1998, Nagamine Engineers
- Exhibit 2 - "Construction Methods for the Foundations in the Ala Wai Canal", January 8, 1998, Nagamine Engineers

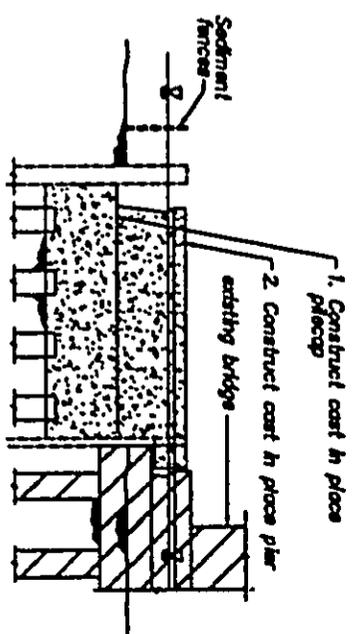
TOTL SHEETS	SHEET NO.	TOTAL SHEETS	PROJ. YEAR	FIELD NO.	STATE	FED. ROAD DIST. NO.



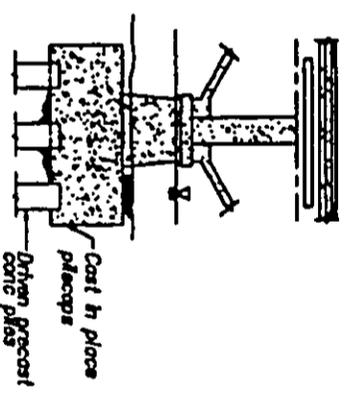
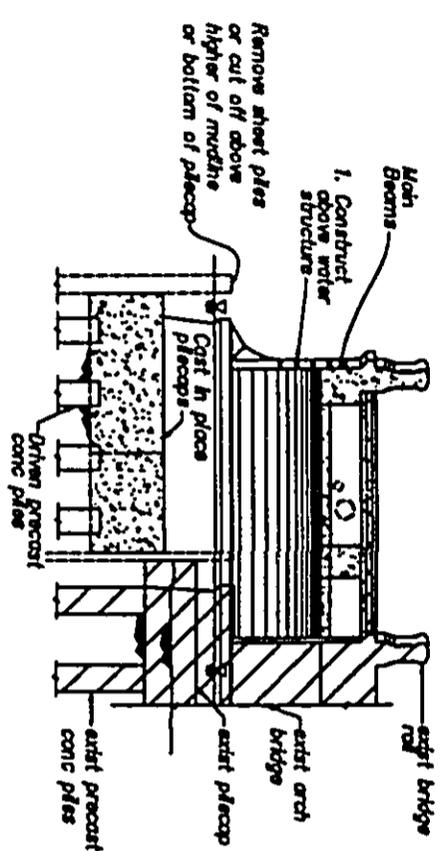
2 CONSTRUCTION IN WATER-PHASE 1
SCALE: 1/4"=1'-0"



3 CONSTRUCTION IN WATER-PHASE 2
SCALE: 1/4"=1'-0"



4 CONSTRUCTION IN WATER-PHASE 3
SCALE: 1/4"=1'-0"



GRAPHIC SCALES:
0 1/2" = 6' 0"
1/8" = 1'-0"
0 1/4" = 3' 0"
1/4" = 1'-0"

DATE	REVISION	BY

STATE OF MISSISSIPPI
DEPARTMENT OF TRANSPORTATION
KALAKAUA AVENUE
BRIDGE IMPROVEMENTS
FOUNDATION CONSTRUCTION

5642 0024 0000

0000 0024 2498

EXHIBIT 2

January 8, 1998

Project: Kalakaua Avenue Bridge Improvements

Subject: Construction Methods for the Foundations in the Ala Wai Canal

1. Foundation Description

The foundations will be supported on driven precast concrete piles. The foundations consist of an abutment at each end of the bridge and two piers in the middle of the Ala Wai Canal. The abutments and piers will be supported by pile caps which connect the piles to the piers. The foundations will be located within the horizontal shadow of the existing bridge foundations so the canal drainage is not affected.

2. Soil Exploration

The soil exploration program will be conducted by taking one soil boring at each of the four foundations. The borings will be taken by drilling and/or coring using a truck and/or trailer mounted drilling rig. The soil cuttings from the borings will be placed back in the bore holes. The drilling rig will be mounted on a small barge 20' x 30' in size and located directly over the bore holes. The borings at the abutment locations will be drilled from the shore line of the canal. Each bore hole would be 6 inches in diameter and 100' + deep below the mud line or ground surface.

3. Piles

The piles which are anticipated are 16-1/2" octagonal precast prestressed concrete piles. The piles will be installed by being hammered into the ground by pile driving equipment. The pile driving equipment is anticipated to be located over floating barges. It is possible that the pile driving may occur from the shore and existing bridge by reaching out with the equipment.

Prior to pile driving, any debris on the canal bottom at the proposed pile locations will need to be cleared.

If the foundation requires locating the bottom of the piles at a deeper coral ledge anticipated to be found approximately 100 feet or so below the surface, predrilling through an upper shallower ledge may be required to penetrate the upper ledge anticipated to be found at approximately 40' to 50' below the surface. This predrilling will occur prior to driving the piles. It is unknown at this time if the piles can be driven through the upper coral ledge without predrilling, or if the upper coral ledge is competent and can be used as the support for the piles. If the upper ledge is competent to support the bridge, no predrilling will be required.

Sediment/silt fences will be installed around each pile cap location prior to pile driving.

4. Pile Caps

It is anticipated that temporary sheet piling will be used to construct the pile caps. Sheet piles will be driven around the perimeter of each pile cap to create a relatively

0000 0024 2499

EXHIBIT 2

(CONTINUED)

watertight cofferdam. The top of the sheet piles will be above water level. The interior will dewatered and the soil excavated to the bottom of the pile cap. The dewatering effluent will be filtered and treated prior to discharge. The piles will be cut to length and the pile caps constructed with cast in place concrete. The bottom of the pile caps will be based on scour requirements and will be lower than the existing pile caps. The tops of the pile caps might be raised to be at the mud line, or higher to match the existing bridge pile cap elevations. The existing pile caps are located 1' below the low water elevation (per existing drawings) and are visible when the water is not too muddy. The volume of the pile caps is estimated to be 90 cubic yards.

5. Abutments and Piers

The piers and abutments will be located over the pile caps. They will be constructed of cast in place concrete. The tops of the piers and abutments will be above water.

After the abutments and piers are constructed, the sheet piles will be cut off at the mud line or the bottom of the pile caps so it will not be visible after construction is completed. As an option, the Contractor may elect to pull the sheets.

0000 0024 2500

STATE OF HAWAII
DEPARTMENT OF BUSINESS ECONOMIC DEVELOPMENT & TOURISM
OFFICE OF PLANNING
FEDERAL CONSISTENCY DETERMINATION
CORRESPONDENCE

0000 0024 2501

Ref. No. P-7453

May 20, 1998

RECEIVED
JUN 19 1998
AKINAKA & ASSOCIATES, LTD.

MEMORANDUM

TO: Kazu Hayashida, Director
Department of Transportation

FROM: Rick Egged *R. Egged*
Director, Office of Planning

SUBJECT: Hawaii Coastal Zone Management (CZM) Program Federal Consistency Review
for Kalakaua Avenue Bridge Improvements, Waikiki, Project No. HWY-O-13-98;
Department of the Army Permit File No. 980000107

Your proposal to construct a pedestrian bridge abutting the existing Kalakaua Avenue Bridge has been reviewed for consistency with Hawaii's CZM Program. We concur with your CZM assessment and finding that the activity is consistent based on the following conditions.

1. The project shall be in compliance with requirements of the Commission on Water Resource Management, Department of Land and Natural Resources.
2. The project shall be in compliance with State water quality standards and requirements of the Department of Health.
3. According to the Draft Environmental Assessment, Section 5.6.1, dated April 1998, construction impacts to water recreational activities in the Ala Wai Canal will be mitigated by providing access under the bridge to allow canal users access to and from the ocean. Also, public notice will be given to affected groups along the Ala Wai Canal and temporary signs will warn canal users of the construction closings and their duration. The contractor will be required to be aware of the different schedules of the different water activities which use the canal and maintain communications between these affected groups to ensure a minimum disruption of both recreational activities and the construction schedule.

CZM consistency concurrence is not an endorsement of the project nor does it convey approval with any other regulations administered by any State or County agency. Thank you for your cooperation in complying with Hawaii's CZM Program. If you have any questions, please call John Nakagawa of our CZM Program at 587-2878.

0000 0024 2502

AMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

KAZU HAYASHIMA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINAII
GLENN M. OKIMOTO

IN REPLY REFER TO:

HWY-DS
2.8592

March 27, 1998

TO: DOUGLAS TOM, MANAGER
COASTAL ZONE MANAGEMENT PROGRAM
OFFICE OF PLANNING
DEPARTMENT OF BUSINESS, ECONOMIC DEVELOPMENT, & TOURISM

FROM: *for* PERICLES MANTHOS, ADMINISTRATOR *Ferry Jayson*
HIGHWAYS DIVISION
DEPARTMENT OF TRANSPORTATION

SUBJECT: COASTAL ZONE MANAGEMENT FEDERAL CONSISTENCY DETERMINATION

The Department of Transportation, Highways Division, is designing a new pedestrian bridge abutting the existing Kalakaua Avenue Bridge. The proposed project is entitled "Kalakaua Avenue Bridge Improvements, Project No. HWY-O-13-98" and is located within TMK 2-3-34.

We determine that the project is consistent with the Hawaii CZM Program. The following statement is provided as required by the Department of Business, Economic Development, & Tourism, Office of Planning:

- The proposed activity is consistent with and will be conducted in a manner consistent to the maximum extent practicable with the Hawaii Coastal Zone Management Program.

The following items are attached in support of our federal consistency determination:

- Attachment 1 CZM Checklist Assessment Form
- Attachment 2 Project Description
- Attachment 3 U.S. Coast Guard Correspondence
- Attachment 4 Supplemental Information

Please reply whether DBEDT concurs with this determination. If additional information is required, please contact our project manager, Edmund Yoshida, at 587-2122. Thank you.

Attachments

0000 0024 2503

**KALAKAUA AVENUE BRIDGE IMPROVEMENTS
PROJECT NO. HWY-O-13-98**

**WAIKIKI, OAHU, HAWAII
TMK 2-3-34**

CZM CHECKLIST ASSESSMENT FORM

ATTACHMENT 1

0000 0024 2504

HAWAII CZM PROGRAM
ASSESSMENT FORM

RECREATIONAL RESOURCES

Objective: Provide coastal recreational opportunities accessible to the public.

Policies

- 1) Improve coordination and funding of coastal recreation planning and management.
- 2) Provide adequate, accessible, and diverse recreational opportunities in the coastal zone management area by:
 - a) Protecting coastal resources uniquely suited for recreational activities that cannot be provided in other areas;
 - b) Requiring replacement of coastal resources having significant recreational value, including but not limited to surfing sites and sandy beaches, when such resources will be unavoidably damaged by development; or requiring reasonable monetary compensation to the State for recreation when replacement is not feasible or desirable;
 - c) Providing and managing adequate public access, consistent with conservation of natural resources, to and along shorelines with recreational value;
 - d) Providing an adequate supply of shoreline parks and other recreational facilities suitable for public recreation;
 - e) Encouraging expanded public recreational use of County, State, and Federally owned or controlled shoreline lands and waters having recreational value;
 - f) Adopting water quality standards and regulating point and non-point sources of pollution to protect and where feasible, restore the recreational value of coastal waters;
 - g) Developing new shoreline recreational opportunities, where appropriate, such as artificial reefs for surfing and fishing; and
 - h) Encouraging reasonable dedication of shoreline areas with recreational value for public use as part of discretionary approvals or permits by the land use commission, board of land and natural resources, County planning commissions; and crediting such dedication against the requirements of section 46-6.

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Check either "Yes" or "No" for each of the following questions.

	<u>Yes</u>	<u>No</u>
1. Will the proposed action involve or be near a dedicated public right-of-way?	<u>X</u>	<u> </u>
2. Does the project site abut the shoreline?	<u> </u>	<u>X</u>
3. Is the project site near a State or County park?	<u>X</u>	<u> </u>
4. Is the project site near a perennial stream?	<u>X</u>	<u> </u>
5. Will the proposed action occur in or affect a surf site?	<u> </u>	<u>X</u>
6. Will the proposed action occur in or affect a popular fishing area?	<u> </u>	<u>X</u>
7. Will the proposed action occur in or affect a recreational or boating area?	<u>X</u>	<u> </u>
8. Is the project site near a sandy beach?	<u> </u>	<u>X</u>
9. Are there swimming or other recreational uses in the area?	<u> </u>	<u>X</u>

Discussion

SEE ATTACHED SUPPLEMENTAL INFORMATION

HISTORIC RESOURCES

Objective: Protect, preserve, and where desirable, restore those natural and man-made historic and pre-historic resources in the coastal zone management area that are significant in Hawaiian and American history and culture.

Policies

- 1) Identify and analyze significant archaeological resources;
- 2) Maximize information retention through preservation of remains and artifacts or salvage operations; and
- 3) Support State goals for protection, restoration, interpretation, and display of historic resources.

Check either "Yes" or "No" for each of the following questions.

	<u>Yes</u>	<u>No</u>
1. Is the project site within a historic/cultural district?	<u>X</u>	—
2. Is the project site listed on or nominated to the Hawaii or National register of historic places?	<u>X</u>	—
3. Does the project site include undeveloped land which has not been surveyed by an archaeologist?	—	<u>X</u>
4. Has a site survey revealed any information on historic or archaeological resources?	—	<u>X</u>
5. Is the project site within or near a Hawaiian fishpond or historic settlement area?	—	<u>X</u>

Discussion

SEE ATTACHED SUPPLEMENTAL INFORMATION

SCENIC AND OPEN SPACE RESOURCES

Objective: Protect, preserve and, where desirable, restore or improve the quality of coastal scenic and open space resources.

Policies

- 1) Identify valued scenic resources in the coastal zone management area;
- 2) Insure that new developments are compatible with their visual environment by designing and locating such developments to minimize the alteration of natural landforms and existing public views to and along the shoreline;
- 3) Preserve, maintain and, where desirable, improve and restore shoreline open space and scenic resources; and
- 4) Encourage those developments which are not coastal dependent to locate in inland areas.

Check either "Yes" or "No" for each of the following questions.

	<u>Yes</u>	<u>No</u>
1. Does the project site abut a scenic landmark?	<u>X</u>	<u> </u>
2. Does the proposed action involve the construction of a multi-story structure or structures?	<u> </u>	<u>X</u>
3. Is the project site adjacent to undeveloped parcels?	<u> </u>	<u>X</u>
4. Does the proposed action involve the construction of structures visible between the nearest coastal roadway and the shoreline?	<u> </u>	<u>X</u>
5. Will the proposed action involve construction in or on waters seaward of the shoreline? On or near a beach?	<u> </u>	<u>X</u>

Discussion

SEE ATTACHED SUPPLEMENTAL INFORMATION

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COASTAL ECOSYSTEMS

Objective: Protect valuable coastal ecosystems from disruption and minimize adverse impacts on all coastal ecosystems.

Policies

-) Improve the technical basis for natural resource management;
-) Preserve valuable coastal ecosystems of significant biological or economic importance;
-) Minimize disruption or degradation of coastal water ecosystems by effective regulation of stream diversions, channelization, and similar land water uses, recognizing competing water needs; and
-) Promote water quantity and quality planning and management practices which reflect the tolerance of fresh water and marine ecosystems and prohibit land and water uses which violate State water quality standards.

Check either "Yes" or "No" for each of the following questions.

	<u>Yes</u>	<u>No</u>
1. Does the proposed action involve dredge or fill activities?	<u>X</u>	—
2. Is the project site within the Shoreline Setback Area (20 to 40 feet inland of the shoreline)?	—	<u>X</u>
3. Will the proposed action require some form of effluent discharge into a body of water?	<u>X</u>	—
4. Will the proposed action require earthwork beyond clearing and grubbing?	—	<u>X</u>
5. Will the proposed action include the construction of special waste treatment facilities, such as injection wells, discharge pipes, or cesspools?	—	<u>X</u>
6. Is an intermittent or perennial stream located on or near the project site?	<u>X</u>	—
7. Does the project site provide habitat for endangered species of plants, birds, or mammals?	—	<u>X</u>
8. Is any such habitat located nearby?	—	<u>X</u>
9. Is there a wetland on the project site?	—	<u>X</u>
10. Is the project site situated in or abutting a Natural Area Reserve?	—	<u>X</u>

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- 11. Is the project site situated in or abutting a Marine Life Conservation District? X
- 12. Is the project site situated in or abutting an estuary? X

Discussion

SEE ATTACHED SUPPLEMENTAL INFORMATION

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ECONOMIC USES

Objective: Provide public or private facilities and improvements important to the State's economy in suitable locations.

Policies

- 1) Concentrate in appropriate areas the location of coastal dependent development necessary to the State's economy;
- 2) Insure that coastal dependent development such as harbors and ports, visitor industry facilities, and energy generating facilities are located, designed, and constructed to minimize adverse social, visual, and environmental impacts in the coastal zone management area; and
- 3) Direct the location and expansion of coastal dependent developments to areas presently designated and used for such development and permit reasonable long-term growth at such areas, and permit coastal dependent development outside of presently designated areas when:
 - a) Utilization of presently designated locations is not feasible;
 - b) Adverse environmental effects are minimized; and
 - c) Important to the State's economy.

Check either "Yes" or "No" for each of the following questions.

	<u>Yes</u>	<u>No</u>
1. Does the project involve a harbor or port?	—	<u>X</u>
2. Is the project site within a designated tourist destination area?	<u>X</u>	—
3. Does the project site include agricultural lands or lands designated for such use?	—	<u>X</u>
4. Does the proposed activity relate to commercial fishing or seafood production?	—	<u>X</u>
5. Does the proposed activity relate to energy production?	—	<u>X</u>
6. Does the proposed activity relate to seabed mining?	—	<u>X</u>

Discussion

SEE ATTACHED SUPPLEMENTAL INFORMATION

COASTAL HAZARDS

Objective: Reduce hazard to life and property from tsunami, storm waves, stream flooding, erosion, and subsidence.

Policies

- 1) Develop and communicate adequate information on storm wave, tsunami, flood erosion, and subsidence hazard;
- 2) Control development in areas subject to storm wave, tsunami, flood, erosion, and subsidence hazard;
- 3) Ensure that developments comply with requirements of the Federal Flood Insurance Program; and
- 4) Prevent coastal flooding from inland projects.

Check either "Yes" or "No" for each of the following questions.

	<u>Yes</u>	<u>No</u>
1. Is the project site on or abutting a sandy beach?	—	<u>X</u>
2. Is the project site within a potential tsunami inundation area as depicted on the National Flood Insurance Program flood hazard map?	—	<u>X</u>
3. Is the project site within a potential flood inundation area according to a flood hazard map?	<u>X</u>	—
4. Is the project site within a potential subsidence hazard area according to a subsidence hazard map?	—	<u>X</u>
5. Has the project site or nearby shoreline areas experienced shoreline erosion?	—	<u>X</u>

Discussion

SEE ATTACHED SUPPLEMENTAL INFORMATION

MANAGING DEVELOPMENT

Objective: Improve the development review process, communication, and public participation in the management of coastal resources and hazards.

Policies

- 1) Effectively utilize and implement existing law to the maximum extent possible in managing present and future coastal zone development;
- 2) Facilitate timely processing of application for development permits and resolve overlapping or conflicting permit requirements; and
- 3) Communicate the potential short- and long-term impacts of proposed significant coastal developments early in their life cycle and in terms understandable to the general public to facilitate public participation in the planning and review process.

Check either "Yes" or "No" for each of the following questions.

	<u>Yes</u>	<u>No</u>
1. Will the proposed activity require more than two (2) permits or approvals?	<u>X</u>	—
2. Does the proposed activity conform with the State and County land use designations for the site?	X	—
3. Has or will the public be notified of the proposed activity?	<u>X</u>	—
4. Has a draft or final environmental impact statement or an environmental assessment been prepared?	<u>X</u>	—

Discussion

SEE ATTACHED SUPPLEMENTAL INFORMATION

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**KALAKAUA AVENUE BRIDGE IMPROVEMENTS
PROJECT NO. HWY-O-13-98**

**WAIKIKI, OAHU, HAWAII
TMK 2-3-34**

PROJECT DESCRIPTION

ATTACHMENT 2

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PROJECT DESCRIPTION

Subject: CZM Federal Consistency Determination

Project: Kalakaua Avenue Bridge Improvements
Project No. HWY-O-13-98

Location: Kalakaua Avenue crossing at Ala Wai Canal
Waikiki, Oahu, Hawaii (TMK: 2-3-34)

Applicant: State of Hawaii
Department of Transportation
Highways Division
869 Punchbowl Street
Honolulu, Hawaii 96813

Contact: Mr. Edmund Yoshida, Project Manager
Ph: 587-2122 Fax: 587-2342

The State Department of Transportation – Highways Division is proposing to construct a pedestrian bridge and sidewalk improvements abutting the existing Kalakaua Bridge (TMK: 2-3-34), between Kapiolani Boulevard and Ala Wai Boulevard in Waikiki, Oahu (see Exhibits 1 & 2).

Project Objectives:

The purpose of the pedestrian bridge is to 1) increase pedestrian safety over the bridge through better separation of the pedestrian/bicycle and vehicular traffic flow; 2) reconstruct the existing non-ADA conforming condition of the sidewalk at the Waikiki end of the bridge (Ala Wai Boulevard intersection); and 3) located at of five designated “gateways” or major entrances into Waikiki (Waikiki Special District Design Guidelines, Department of Land Utilization City & County of Honolulu) the bridge enhancements will provide the intended “... sense of arrival to a special place by providing coordinated open space, landscaping and architectural features.” In addition, the bridge by design maintains significant public views, protects the existing historic bridge structure, enhances public pedestrian access, and effectively blends function and pedestrian safety while linking both sides of the Waikiki (Ala Wai) Promenade as an important public space in the City’s Waikiki Special District.

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Project Description:

The existing 56-foot wide Kalakaua Avenue Bridge consists of two northbound (Kapiolani-bound) lanes, three southbound (Waikiki-bound) lanes, and 6-1/2 foot wide sidewalks. The nearly 200-foot long, three-span bridge, constructed in 1929, is eligible for listing on the National Register of Historic Places. The Ala Wai Canal which is spanned by the existing bridge was listed on the Hawaii State Register of Historic Places (SRHP) in 1992 and was determined to be eligible for listing in the National Register of Historic Places (NRHP) in 1985. Consequently, the project requires State Historic Preservation Division (SHPD) review and concurrence for any improvements made adjacent to the existing bridge.

The proposed Kalakaua Avenue Bridge Improvements project will construct a three-span, concrete pedestrian bridge, approximately 14-feet wide, abutting and reflecting in profile, length, size and architectural detail the existing Kalakaua Avenue Bridge as shown in Exhibit 3. This new construction will not affect the structural integrity of the existing bridge since the new pedestrian bridge will be an independent, free-standing structure separated by a non-structural expansion joint. Grade and slope adjustment improvements to the existing sidewalk at the Ala Wai Boulevard end of the new bridge will be designed to remedy the current non-conforming ADA conditions. The bridge may include a limited amount of space within its structure to address future utility needs over the bridge. The State proposes to turn the ownership and maintenance of the bridge over to the City and County of Honolulu after it is constructed.

Construction of Bridge Improvements

The bridge will be supported by new abutments on each end and will be pile supported at two locations in the middle. Bridge foundation work in the water consists of drilled or driven concrete piles at both bridge end abutments and at two locations in mid-span. A concrete pier cap will be poured to match the existing Kalakaua Avenue bridge piers as closely as possible. Finally, the above-water work will consist of the bridge structure itself including columns, abutments, railings and pedestrian lighting.

Estimated Construction Schedule

Construction of the new pedestrian bridge and sidewalk improvements is estimated to commence in September 1998 and be completed by July 1999.

Alternatives Considered

No Action

The No Action Alternative would not satisfy the proposed project's purpose of improving pedestrian safety and access over the Ala Wai Canal on Kalakaua Avenue. Pedestrians would continue to utilize the existing 6.5-foot wide sidewalk adjacent to five busy lanes in Kalakaua Avenue, at the gateway to Waikiki. The opportunity to remediate the current steep grade and cross slope at the mauka-ewa corner of the Kalakaua Avenue/Ala Wai Boulevard would be lost.

Delayed Action

A delay in the construction of the proposed bridge would increase the possibility of a pedestrian accident occurring over the existing bridge sidewalk and at the Waikiki end of the bridge. This is due to the larger volume of expected foot traffic across the existing bridge due to expected area growth and occasional convention center events. The longer the bridge is delayed, the longer this larger volume of pedestrian traffic is exposed to the current condition on the bridge (see non-ADA conforming discussion above).

Bridge Location Alternatives

Bridge location alternatives were evaluated with the intent of providing improved access and pedestrian safety for existing and future pedestrian volumes crossing over the Ala Wai Canal on Kalakaua Avenue. Canal crossings at Ala Moana Boulevard and McCully street were considered too circuitous a route to get to the Kapiolani Boulevard side of Kalakaua Avenue. A pedestrian bridge over the canal between Ala Moana Boulevard and Kalakaua Avenue, roughly across from the Lipeepe Street/Ala Wai Boulevard intersection, was discussed in the Hawaii Convention Center FEIS. This alternative was put off for further study or until HCC needs required one and funds could be appropriated. Ultimately, none of the evaluated alternatives provided a solution which

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enhanced pedestrian safety for existing and future pedestrian volumes crossing over the Ala Wai Canal *on Kalakaua Avenue*.

Project Impacts

The project concerns are expected to include construction noise, dust, traffic delays, and water quality impacts. Water recreation activities may be impacted during certain portions of the construction period. Appropriate mitigation measures and project alternatives will be evaluated.

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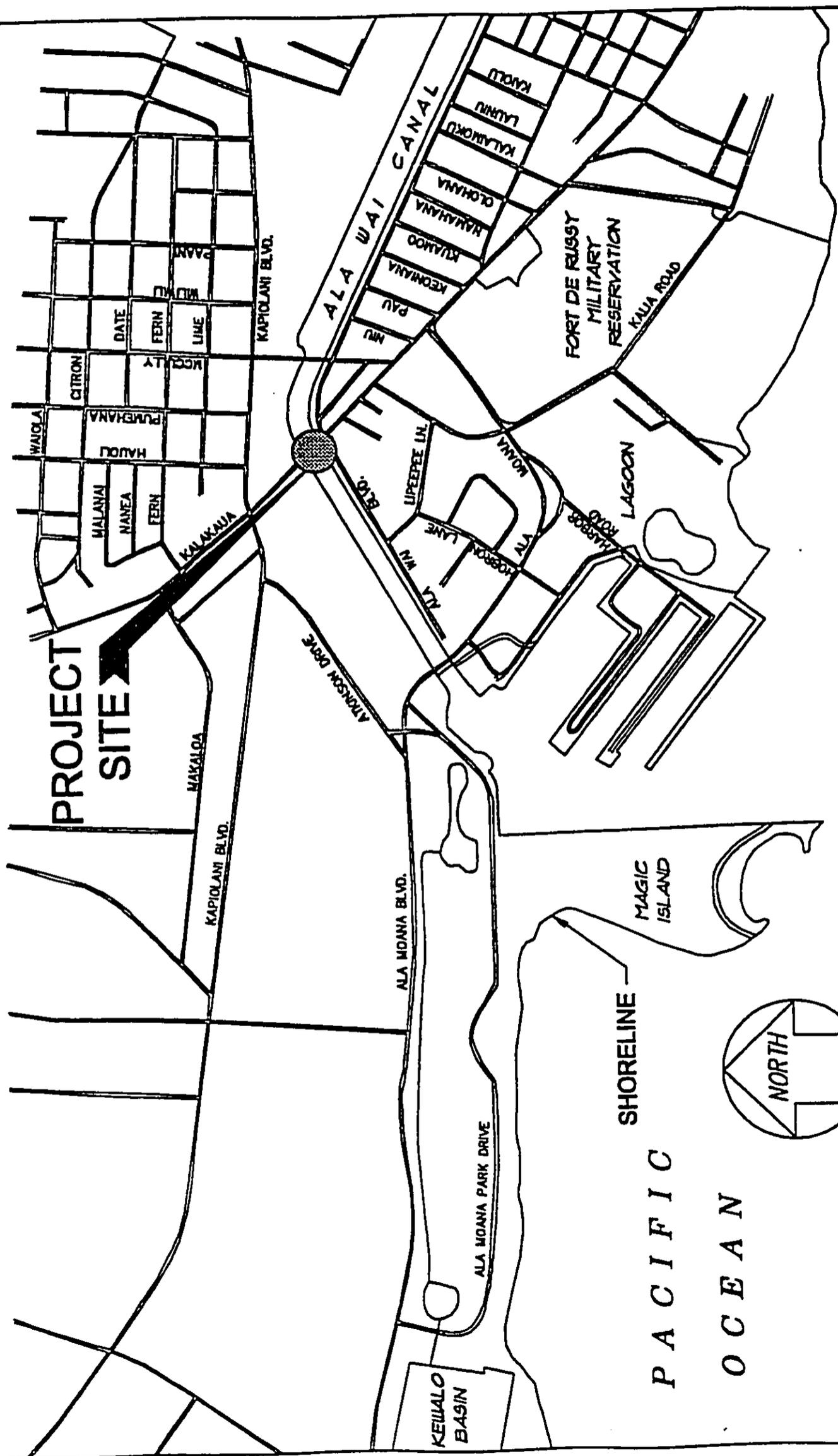


EXHIBIT	
KALAKAUA AVENUE BRIDGE IMPROVEMENTS	
AREA MAP	
2	

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**KALAKAUA AVENUE BRIDGE IMPROVEMENTS
PROJECT NO. HWY-O-13-98**

**WAIKIKI, OAHU, HAWAII
TMK 2-3-34**

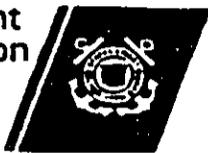
U.S. COAST GUARD CORRESPONDENCE

ATTACHMENT 3

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U.S. Department
of Transportation

United States
Coast Guard



Commander
Fourteenth Coast Guard District

300 Ala Moana Boulevard
Honolulu, HI 96850-4982
Staff Symbol: oan
Phone: (808)541-2315
FAX: (808)541-2309

16590
Serial 32051
25 Feb 1998

Mr. Michael Miyahira
Akinaka & Associates, LTD
250 North Beretania Street
Suite 300
Honolulu, HI 96817-4716

RECEIVED

FEB 27 1998

AKINAKA & ASSOCIATES, LTD.

Dear Mr. Miyahira:

We have reviewed your letter of February 18, 1998, in which you are requesting a U. S. Coast Guard bridge permit to construct a pedestrian bridge across the Ala Wai Canal at Kalakaua Avenue, Honolulu, Hawaii.

Under 33 CFR part 115.70, the Commandant of the U. S. Coast Guard has given advanced approval to the locations and plans of bridges to be constructed across reaches of waterways navigable in law, but not actually navigated other than logs, log rafts, rowboats, canoes and small motorboats. A review of your site plans was conducted and is considered to meet the criteria for advanced approval. Therefore, no Coast Guard permit need be issued.

Thanks for taking the time to write and inform us.

Sincerely,

A handwritten signature in cursive script that reads "T. D. Hooper".

T. D. HOOPER
Commander, U. S. Coast Guard
Chief, Aids to Navigation Branch
By direction

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AKINAKA & ASSOCIATES, LTD.
CONSULTING ENGINEERS
Civil Engineering • Land Planning

FILE COPY

250 NORTH BERETANIA STREET, SUITE 300, HONOLULU, HAWAII 96817-4716 • TELEPHONE (808) 536-7721 • FAX (808) 521-2153 • E-mail: akinaka@aloha.net

ROBERT Y. AKINAKA, L.P.E.
HENRY S. MORITA, L.P.E.
HELDON T. YAMASATO, L.P.E.
MICHAEL M. MIYAHIRA, L.P.E.
BARRY K. MURANAKA, L.P.E.

SALVADOR M. QUITORIANO, L.P.L.S.
BENJAMIN M. GANAL, L.P.L.S.
JOSEPH S. KEANE, L.P.E.
MARIAN N. NAKAMA, L.P.E.
CRAIG K. MATSUMOTO
STATE A. INFANTE

February 18, 1998

Commander, Aids to Navigation
14th Coast Guard District
Prince Kuhio Federal Building
300 Ala Moana Boulevard, Rm 9139
Honolulu, Hawaii 96850-4982

Project: Kalakaua Pedestrian Bridge
TMK: 2-3-34
Waikiki, Oahu, Hawaii

Subject: Bridge and Causeway Permit Application
Title 33 Code of Federal Regulations, Parts 114 and 115
Reference: November 3, 1997 fax from OMI Ed Lawrence

Applicant: State of Hawaii Department of Transportation
Highways Division
Mr. Pericles Manthos, Administrator
Contact: Edmund Yoshida

Authorized
Agent: Akinaka & Associates, Ltd.

Dear Petty Officer Lawrence:

The State Department of Transportation – Highways Division is proposing to construct a pedestrian bridge and sidewalk improvements abutting the existing Kalakaua Bridge (TMK: 2-3-34), between Kapiolani Boulevard and Ala Wai Boulevard in Waikiki, Oahu (see Exhibit 1).

Project Objectives:

The purpose of the pedestrian bridge is to 1) increase pedestrian safety over the bridge through better separation of the pedestrian/bicycle and vehicular traffic flow; 2) reconstruct the existing non-ADA conforming condition of the sidewalk at the Waikiki end of the bridge (Ala Wai Boulevard intersection); and 3) located at one of five designated “gateways” or major entrances into Waikiki (Waikiki Special District Design Guidelines, Department of Land Utilization City & County of Honolulu) the bridge enhancements will provide the intended “... sense of arrival to a special place by providing coordinated open space, landscaping and architectural features.” In addition, the bridge by design maintains significant public views, protects the existing historic bridge structure, enhances public pedestrian access, and effectively blends function and

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Commander, Aids to Navigation
14th Coast Guard District
February 18, 1998
Page 2

pedestrian safety while linking both sides of the Waikiki (Ala Wai) Promenade as an important public space in the City's Waikiki Special District.

The existing bridge is located on Kalakaua Avenue, between Kapiolani Boulevard and Ala Wai Boulevard. The existing 56-foot wide travelway across the bridge consists of two Kapiolani-bound lanes (expanding to three on the Kapiolani Boulevard end) and three Waikiki-bound lanes. The 180-foot long, three-span bridge, constructed in 1929, is currently on the State's Register of Historic Places and will require State Historic Preservation Division review and concurrence for any improvements made to it.

Project Description:

The proposed Kalakaua Pedestrian Bridge project will include a three-span, concrete pedestrian bridge abutting and reflecting in profile, length, size and architectural detail the existing historic bridge as shown in Exhibits 2 and 3. This new construction does not affect the structural integrity of the existing bridge since the new pedestrian bridge will be an independent, free-standing structure separated by a non-structural expansion joint. The separation of pedestrians from vehicular traffic through the use of the existing bridge railing is designed to be an improvement over the existing condition. Grade and slope adjustment improvements to the existing sidewalk at the Ala Wai Boulevard end of the new bridge will be designed to remedy the current non-conforming ADA conditions. Miscellaneous improvements include walkway lighting and landscaping.

The bridge will be supported by new abutments on each end of the bridge and be pile supported in two location in the middle. A detailed description of this in-water project work is provided as plan Exhibit 4 with Text Attachments.

The preliminary project schedule is provided as Exhibit 5.

Alternatives to the Proposed Project:

The following project alternatives are being evaluated in an Environmental Assessment being prepared in accordance with Chapter 343 HRS and Title 11 Chapter 200 HAR. The Draft EA is expected to be published in the OEQC's Environmental Notice in late-March 1998.

No Action

The No Action Alternative would not satisfy the proposed project's purpose of improving pedestrian safety and access over the Ala Wai Canal on Kalakaua Avenue. Pedestrians would continue to utilize the existing 8-foot wide sidewalk adjacent to five busy lanes in Kalakaua Avenue, at the gateway to Waikiki. The opportunity to remediate the current steep grade and cross slope at the mauka-ewa corner of the Kalakaua Avenue/Ala Wai Boulevard would be lost, thus leaving the City vulnerable to legal action due to non-ADA conforming sidewalk.

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Commander, Aids to Navigation
14th Coast Guard District
February 18, 1998
Page 3

Delayed Action

A delay in the construction of the proposed bridge would increase the possibility of a pedestrian accident occurring over the existing bridge sidewalk and at the Waikiki end of the bridge. This is due to the larger volume of expected foot traffic across the existing bridge as the first convention events open. The longer the bridge is delayed while the convention center is open, the longer this larger volume of pedestrian traffic is exposed to the current condition on the bridge (see non-ADA conforming discussion above).

Bridge Relocation Alternatives

Bridge location alternatives were evaluated with the intent of providing improved access and pedestrian safety for existing and future pedestrian volumes crossing over the Ala Wai Canal on Kalakaua Avenue. Canal crossings at Ala Moana Boulevard and McCully Street are considered too circuitous a route to get to the Kapiolani Boulevard side of Kalakaua Avenue. A pedestrian bridge over the canal between Ala Moana Boulevard and Kalakaua Avenue, roughly across from the Lipeepe Street/Ala Wai Boulevard intersection, was discussed in the Hawaii Convention Center (HCC) FEIS. This alternative was put off for further study or until HCC needs required one and funds could be appropriated. Ultimately, none of the evaluated alternatives provided a logical solution which enhanced pedestrian safety for existing and future pedestrian volumes crossing over the Ala Wai Canal on Kalakaua Avenue.

Project Impacts

The project concerns are expected to include construction noise, dust, traffic delays, and water quality impacts. Water recreation activities may be impacted during certain portions of the construction period. Appropriate mitigation measures and project alternatives will be evaluated. In addition, the acquisition of the following Federal, State and County permits and approvals will ensure a thorough evaluation of temporary (construction methods) and permanent impacts to water quality, recreational uses and traffic safety:

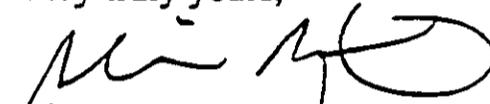
- U.S. Army Corps of Engineers, Nationwide Permit #3 Structures in Artificial Canals and Nationwide Permit #25 Structural Discharges;
- State of Hawaii Department of Health Clean Water Branch, Section 401 Water Quality Certification and NPDES General Permit for Construction Dewatering;
- State of Hawaii Department of Land and Natural Resources Commission on Water Resource Management, Stream Channel Alteration Permit;
- City and County of Honolulu Department of Land Utilization, Flood Fringe Certification;
- Plan approval from City and County of Honolulu Department of Public Works and Department of Transportation Services.

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Commander, Aids to Navigation
14th Coast Guard District
February 18, 1998
Page 4

Thank you for your prompt attention to this permit request. If you have any questions concerning this letter, please call me at 536-7721.

Very truly yours,



Michael Miyahira, P.E.
Akinaka & Associates, Ltd.

cc: Ed Yoshida, HDOT-DS
Norman Nagamine, Nagamine Engineers Inc.

Attachments

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**KALAKAUA AVENUE BRIDGE IMPROVEMENTS
PROJECT NO. HWY-O-13-98**

**WAIKIKI, OAHU, HAWAII
TMK 2-3-34**

SUPPLEMENTAL INFORMATION

ATTACHMENT 4

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**CZM SUPPLEMENTAL INFORMATION
KALAKAUA PEDESTRIAN BRIDGE (TMK: 2-3-34)
WAIKIKI, OAHU, HAWAII
(APPLICANT: State Department of Transportation – Highways Division)**

DATE: 4/15/98

RECREATIONAL RESOURCES

Ala Wai Canal is currently used by a number of athletic organizations and local residents for boating, canoe racing, flat water kayaking, fishing, and crabbing. Non-contact activities immediately along the canal include golfing, athletic field use, walking and jogging. These activities will not be negatively impacted by the pedestrian bridge once construction is completed since the proposed pedestrian bridge will match the existing bridge in length, height (clearance), and architectural appearance.

During construction, however, a portion of the bridge section usually available for water traffic may be blocked from access to maintain public safety around the construction site. Mitigative measures include public notice to affected groups along the Ala Wai Canal and temporary signage to warn canal users of the construction closings and their durations. The contractor may be required to provide a minimum amount of access under the bridge during work hours to maintain a safe flow of traffic. In addition, the contractor will be required to be aware of the different schedules of the different water activities which use the canal on a daily and seasonal basis (high school canoe practices, canoe races on weekends, etc.) and maintain communications between these affected groups to ensure a minimum disruption of both recreational activities and the construction schedule. Public safety on adjacent lands and thoroughfares, and in the affected waters, will be emphasized. Jurisdiction over public safety for recreational uses in the canal falls on the U.S. Coast Guard 14th District and the State DLNR Division of Boating and Ocean Recreation. Both agencies will be consulted for either a permit or plan approval prior to construction.

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Recreational resources located in the immediate vicinity of the project include Ala Wai Park and the Ala Wai Canal Promenade. The Ala Wai Park is a roughly 28 acre park which stretches from Ala Wai Elementary School, near Iolani School, to McCully Avenue at Kapiolani Boulevard. The park itself is used for many activities including: canoe launch facility and clubhouse, baseball field, parking lot and exercise station. Except for the canoe launch area at the Kapahulu Avenue end of the canal, the Ala Wai Park canoe launch area is in closest proximity to the bridge project.

Project impacts could affect these recreational resources if the contractor proposes to use a portion of the launching area for construction staging, storage, or as a launching facility for water-bound construction equipment.

The contractor will be required to coordinate his use of recreational resources with the affected recreational groups and agencies (e.g. City and County of Honolulu Department of Parks and Recreation) to minimize the recreational resource area's impacts.

HISTORIC RESOURCES

Recent archaeological studies done in the area by ERC Environmental and Energy Services Company, Inc. (1991), Rosendahl (1994) and Cultural Surveys Hawaii (1995) have adequately summarized the rich history of the Waikiki area from early settlement periods to the present, and have at least acknowledged the potential for encountering subsurface archaeological deposits in the Waikiki area.

Any excavations in the canal and at the abutments which encounter such deposits will be required to cease work immediately and notify the DLNR State Historic Preservation Division (SHPO). Work will not recommence until a qualified archaeologist has examined the nature of the deposits for cultural significance. Mitigation measures will likely be specified at that time.

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As one of five designated "gateways" or major entrances into Waikiki, the Kalakaua Bridge and the Ala Wai Canal are both an integral part of the City's Waikiki Special District. The Ala Wai Canal & Promenade are both considered to be eligible for the National Register of Historic Places. The Ala Wai Canal is currently on the Hawaii Register of Historic Places.

The pedestrian bridge construction is not expected to affect the structural integrity of the existing bridge since the new pedestrian bridge will be an independent, free-standing structure separated by a non-structural expansion joint. Once constructed, the new pedestrian bridge may serve to protect the existing historic structure that it abuts from weathering and deterioration. No long term impacts to the existing historic bridge or canal are expected as a result of the pedestrian bridge being constructed.

SCENIC AND OPEN SPACE RESOURCES

The landward portion of the project site is currently fully improved on both the Kapiolani and Waikiki ends. A row of banyan trees which runs parallel along the southern boundary of the canal starting from the Ala Wai Boulevard/Kalakaua Avenue intersection was placed on the City's Exceptional Tree List in 1992. Under the State's Exceptional Tree Act, these trees, originally planted by the Outdoor Circle in 1936, have been deemed "exceptional" due to their "historic or cultural value, age, rarity, location, size, aesthetic quality, or endemic status." Should the State require that one of these trees be altered or removed, the State will consult with the City's Exceptional Tree Committee and certified arborist for permitting and proper handling of the identified tree. The City's requirements may extend to construction staging areas or large equipment not being allowed near these "exceptional" trees.

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COASTAL ECOSYSTEMS

The site's local terrestrial fauna and avifauna consist of rats, stray cats, common mynah and pigeons, finches and doves.

Marine resources in the vicinity of the bridge include tilapia, mullet, barracuda and various bottom dwelling invertebrates and crustaceans. All are considered typical of the project's urbanized marine setting. Due to the continuous sediment loading of the canal and the less than pristine water quality near the bridge, no significant coral colonies or habitats are expected to be disturbed or displaced as a result of the pile driving or pier construction work.

According to the U.S. Fish and Wildlife Service, there are no rare, threatened or endangered species of flora or fauna on the site. No significant short or long term adverse impacts are anticipated due to the construction and permanent placement of the proposed pedestrian bridge.

ECONOMIC USES

Previous studies completed by Earthplan (1995) evaluated the primary, secondary and cumulative social impacts of the Hawaii Convention Center project on a study area comprised of the areas covered by the Ala Moana/Kakaako Neighborhood Board No. 11, McCully/Moiliili Neighborhood Board No. 8 and Waikiki Neighborhood Board No. 9.

The area is characterized by high density urban living, particularly in the Waikiki sub-area where this project is proposed to be built. Specific high rise towers in the area include Century Center, Waikiki Landmark, 1717 Ala Wai, Marina Towers, the Villa on Eaton Square, Waipuna, Kalia and Wailana.

The Waikiki area's demographics point towards residents preferring a high density urban environment and a proximity to jobs in the resort and commercial center of Waikiki. In addition, the area is a generally older community than the island of Oahu as a whole. Based on 1990 data from the U.S. Department of Commerce, the median age in the Waikiki area was 42.0 years compared to 32.3 years for the island. Among residents 65

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years or older, Waikiki's 23.5% doubled the island's 10.9%. These general demographics indicate an older, retired project area population which may experience undesirable short term construction impacts (additional noise and traffic) during the day.

Conversely, any project benefits directly affect this very project area population as the new pedestrian bridge will enhance the pedestrian safety over the Ala Wai Canal along Kalakaua Avenue and fix a current non-ADA conforming condition just adjacent to many of the area's residences (1717 Ala Wai, etc.).

The construction activities of the project could generate nuisance noise, dust and traffic during the construction hours. For those retired residents or those working night shifts in Waikiki, these impacts could be particularly bothersome since they may be home during the contractor's workday. Mitigation measures include identifying the best daily work periods based on local community input. Continuous communications between contractor and the local community will be strongly encouraged through, for example, the use of periodic progress meetings, establishment of a "hotline" to voice concerns, or identification of an accessible contractor's representative to answer questions or hear concerns.

Long terms impacts of the bridge project are expected to be beneficial to both the immediate local community and to visitors through enhanced pedestrian safety and improved accessibility for the disabled. The pedestrian bridge is expected to provide the intended "... sense of arrival to a special place by providing coordinated open space, landscaping and architectural features." In addition, the bridge by design will maintain significant public views, enhance public pedestrian access, and effectively blend function and pedestrian safety while featuring the Waikiki (Ala Wai) Promenade as an important public space in the City's Waikiki Special District.

COASTAL HAZARDS

Department of Land Utilization, a flood hazard determination must be requested by a licensed professional engineer.

Based on Flood Insurance Rate Map (FIRM) Community Panel Number 150001 0120 C (Federal Emergency Management Agency or FEMA) the project site is currently located in Zone "AO", Special Flood Hazard Areas Inundated by 100-Year Flood with flood depths determined to be within 1 to 3 feet (see Exhibit 4).

MANAGING DEVELOPMENT

A Draft Environmental Assessment has been prepared in accordance with Chapter 343 HRS and is expected to be published in April 1998. The following major permits and approvals will be obtained for this project:

- U.S. Army Corps of Engineers Nationwide Permit No. 3 (Structures in Artificial Canals) and No. 25 (Structural Discharges)
- Federal Consistency Determination (CZM) from the Office of State Planning (OSP)
- Section 401 Water Quality Certification from the State Department of Health Clean Water Branch
- NPDES General Permit for Construction Dewatering from the State Department of Health Clean Water Branch
- Stream Channel Alteration Permit from the Department of Land and Natural Resources Commission on Water Resources Management
- Bridges and Causeway Permit (33 CFR 114 & 115) from the U.S. Coast Guard 14th District
- Waikiki Special District approval and Flood Hazard District Certification from the City and County of Honolulu Department of Land Utilization

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*STATE OF HAWAII
DEPARTMENT OF HEALTH
CLEAN WATER BRANCH
SECTION 401 WATER QUALITY CERTIFICATION
CORRESPONDENCE*

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BENJAMIN J. CAVETANO
GOVERNOR OF HAWAII

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DEPT. OF TRANSPORTATION
HIGHWAYS DIVISION



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STATE OF HAWAII
DEPARTMENT OF HEALTH

P.O. BOX 3378

HONOLULU, HAWAII 96801-3378

July 13, 1998

LAWRENCE BIRKE
DIRECTOR OF HEALTH

In reply, please refer to
EMO/CVS

Mr. Pericles Manthos
Administrator
Highways Division
Department of Transportation
State of Hawaii
869 Punchbowl Street
Honolulu, Hawaii 96813-5097

Attention: Mr. Edmund Yoshida, P.E.
Design Section Project Manager

Dear Mr. Manthos:

Subject: Section 401 Water Quality Certification (WQC)
Determination for Kalakaua Avenue Bridge Improvements
Kalakaua Pedestrian Bridge, Project No. HWY-0-13-98
Waikiki, Honolulu, Oahu, TMK: 2-3-34
File No. WQC 0000409 / Army File No. 980000107

The Department of Health (Department) acknowledges receipt of the supplemental information for your Section 401 WQC application and has completed its review for the subject project.

The project will involve construction of a pedestrian bridge and sidewalk improvements abutting the existing Kalakaua Bridge between Kapiolani Boulevard and Ala Wai Boulevard in Waikiki, Oahu. The proposed Kalakaua Pedestrian Bridge project will include a three-span, concrete pedestrian bridge abutting and reflecting in profile, length, size and architectural detail the existing historic bridge. The bridge will be supported by new abutments on each end of the bridge and be pile supported in two locations in the Ala Wai Canal.

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Mr. Pericles Manthos
July 13, 1998
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In a letter dated April 28, 1998, the U.S. Army Corps of Engineers (COE), Honolulu Engineer District determined that the construction of this project can be authorized by the COE's Nationwide Permit (NWP) authority NWP No. 25 for Structural Discharges (Army File No. 980000107).

Based on the findings above, the original application dated March 27, 1998, the revised application dated May 20, 1998, supplemental information received on June 15, 1998, and revised application dated July 2, 1998, the Department has determined that the potential impact with respect to water quality concerns resulting from the proposed pedestrian bridge construction may be considered to be minor.

Therefore, in accordance with Section 11-54-09.1.04 of the Hawaii Administrative Rules (HAR), the Department waives the processing requirement for the Section 401 WQC for the subject project with the following conditions:

1. The granting of this determination shall be limited to the discharges associated with the Kalakaua Avenue Bridge Improvements, Project No. HWY-O-13-98, in the Ala Wai Canal:
 - a. Installation and removal of silt containment devices;
 - b. Construction of the pedestrian bridge foundation including piles, pile caps, and abutments; and
 - c. Construction of the bridge structure including columns, abutments, railings and pedestrian lighting.
2. The applicant shall:
 - a. Invite the Department's representative to attend the pre-construction meeting;
 - b. Orally notify the Clean Water Branch on Oahu at (808) 586-4309 at least three (3) working days before construction work is to begin;

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- c. Notify the Clean Water Branch in writing of termination of the construction activity within 10 days after the completion of the proposed construction activity;
- d. Provide the contractor's name, street address, mailing address if different from the street address, contact person and title, and phone and fax numbers within 30 days after award of the construction contract or 30 days before the start of any construction activities, whichever is earliest;
- e. Submit an updated project construction schedule to the Department within 30 days after award of the construction contract or 30 days before the start of any construction activities, whichever is earliest;
- f. Submit reduced-size copies of the final stamped and signed construction drawings for the project;
- g. Submit a copy of the Notice of Intent for coverage under the State Department of Health's National Pollutant Discharge Elimination System (NPDES) General Permit Authorizing Discharges Associated With Construction Activity Dewatering;
- h. Submit a copy of the application for the State Department of Land and Natural Resources, Commission on Water Resources Management, Stream Channel Alteration Permit;
- i. Immediately report to the Department any spills or other contamination which occur(s) at the project site;
- j. Insure that the contractor complies with the specifications, Best Management Practices (BMPs) plan, Water Quality Monitoring (WQM) Plan, and any other project-related requirements contained in the Section 401 WQC application which provides reasonable assurance that the applicable State Water Quality Standards will not be violated;

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Mr. Pericles Manthos
July 13, 1998
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- k. Review and update the effectiveness and adequacy of the implemented BMPs, applicable WQM plan, and/or environmental protection measures as often as needed:
- (1) The applicant shall modify the plan(s) and/or measure(s) upon request or when instructed by the Director;
 - (2) Any revision(s) to the plan(s) already on file with the Department shall be submitted to the Clean Water Branch for review and comment as the revision(s) arise; and
 - (3) The applicant shall properly address all comment(s) and/or concern(s) to the Director's satisfaction.
1. Conduct water quality, discharge character and/or biological monitoring in accordance with their applicable WQM plan dated July 2, 1998, and all subsequent amendments.
3. All materials placed and to be placed in State waters shall be free of waste metal products, organic materials, debris, and any pollutants at toxic or potentially hazardous concentrations to aquatic life, as identified in Section 11-54-04, HAR:
- a. No construction material or construction-related materials shall be stockpiled, stored, or placed in the aquatic environment or in any way that will disturb the aquatic environment;
 - b. Construction debris and/or any other deleterious material(s) shall be contained and prevented from entering State waters;
 - c. All demolition debris and/or excavated spoils removed from the aquatic environment shall be properly handled or disposed of at the upland approved disposal site. The enclosed Solid Waste Disclosure Form for Construction Sites shall be completed and returned to the Department of Health, Office of Solid Waste Management; and

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Mr. Pericles Manthos
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- d. There shall be no discharge, either directly or indirectly, of any construction dewatering effluent into State waters. The granting of this Section 401 WQC determination does not constitute the approval of discharging any construction dewatering effluent into State waters. The applicant shall obtain an NPDES general permit coverage for any construction dewatering effluent discharge.
4. Work shall be discontinued during flood conditions.
5. All silt containment devices (i.e., silt screens) shall be properly deployed prior to any portion of the construction work. The silt containment devices shall be properly maintained throughout the potential turbidity-producing period of that section of the construction work and shall not be removed until that section of the construction work is completed and the water quality in the affected area has returned to that day's pre-construction condition.
6. The applicant shall allow the Department's representative(s) to make periodic inspections in accordance with HRS Section 342D-8.
7. The applicant shall comply with all new Water Quality Standards as adopted by the Department after the date of this letter.

This determination shall remain valid for two (2) years from the date of this letter or until the earliest of the following occurs:

1. Applicable Department of the Army Nationwide Permit expires or is revised or modified;
2. Proposed activity is completed; or
3. The applicable State Water Quality Standards are revised or modified, unless the activity complies with the revisions or modifications.

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The Department may, on a case-by-case basis and upon the applicant's written request, administratively extend the expiration date of this determination if the Department determines that there are no significant changes to the project scope and the changes will not, either individually or cumulatively, cause adverse impact to the receiving State water quality.

If you have any questions, please call Ms. Joanna L. Seto, P.E., Engineering Section of the Clean Water Branch, at 586-4309.

Sincerely,



THOMAS E. ARIZUMI, P.E., CHIEF
Environmental Management Division

JLS:auc

Enclosure: Solid Waste Disclosure Form for Construction Sites

- c: Lolly Silva, U.S. Army Corps of Engineers, Honolulu Engineer District, Operations Branch (w/o encl.)
- Alex Ho, City and County of Honolulu, Department of Public Works (w/o encl.)
- State Department of Business, Economic Development, and Tourism, Office of Planning, Coastal Zone Management Program (w/o encl.)
- State Department of Land and Natural Resources, Commission on Water Resources Management (w/o encl.)
- State Department of Health, Solid and Hazardous Waste Branch, Office of Solid Waste Management (w/o encl.)

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STATE OF HAWAII
DEPARTMENT OF HEALTH
CLEAN WATER BRANCH

SECTION 401 WATER QUALITY CERTIFICATION (WQC)
APPLICATION

FOR OFFICE USE ONLY	
WQC No.: _____	Assigned to: _____
Date Rec'd: _____	Date Assigned: _____

Prepared By: DEPT. OF TRANSPORTATION
(Owner or Agent)

Date Submitted: July 2, 1998

1.a. Applicant and
Address:

PERICLES MANTHOS, ADMINISTRATOR
STATE OF HAWAII DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION
869 PUNCHBOWL STREET
HONOLULU, HI. 96813

Contact: ED YOSHIDA, HDOT-DS
PH: 587-2122, FAX: 587-2342

b. Agent and
Address:

N / A

Contact:

2. Project Name
and Location:

KALAKAUA AVENUE BRIDGE IMPROVEMENTS
TMK: 2-3-34
WAIKIKI, OAHU, HAWAII

3. Associated
Federal Permit:

DEPARTMENT OF THE ARMY
NATIONWIDE PERMIT #25 STRUCTURAL DISCHARGES

SECTION 401 WATER QUALITY CERTIFICATION APPLICATION

4. Scope of Work

- a. Describe the proposed project including, but not limited to, the construction or operation of facilities which may result in any "discharge" into navigable waters:

The State Department of Transportation – Highways Division is proposing to construct a pedestrian bridge and sidewalk improvements abutting the existing Kalakaua Bridge (TMK: 2-3-34), between Kapiolani Boulevard and Ala Wai Boulevard in Waikiki, Oahu (see Exhibits 1&2).

The proposed Kalakaua Pedestrian Bridge project will include a three-span, concrete pedestrian bridge abutting and reflecting in profile, length, size and architectural detail the existing historic bridge. This new construction does not affect the structural integrity of the existing bridge since the new pedestrian bridge will be an independent, free-standing structure separated by a non-structural expansion joint. The separation of pedestrians from vehicular traffic through the use of the existing bridge railing is designed to be an improvement over the existing condition. The bridge will be supported by new abutments on each end of the bridge and be pile supported at two locations in the Ala Wai Canal.

Grade and slope adjustment improvements to the existing sidewalk at the Ala Wai Boulevard end of the new bridge will be designed to remedy the current non-conforming ADA conditions.

The existing sidewalk at the Waikiki end of the bridge (Kalakaua Avenue / Ala Wai Boulevard intersection) will be removed and replaced with a new 4" thick concrete sidewalk which conforms to slope requirements of the American Disabilities Act. The sidewalk improvements will cover an area of roughly 530 square feet. The convention center end of the bridge will abut the concrete sidewalk of the existing bridge. However, no new sidewalk improvements are planned for this area. Instead, the end of the bridge will utilize a brick paver system which matches that of the existing Ala Wai Promenade. A proposed Contractor staging area is shown in Exhibit 4.

Pre-design geotechnical investigations into the canal bottom's coral and sediment layer were conducted under a Federal Department of the Army Nationwide Permit #6 "Survey Activities" (NWP#980000077) administered by the U.S. Army Corps of Engineers and a Section 401 Water Quality Certification (WQC400) administered by the State Department of Health Clean Water Branch.

- b. Describe the proposed "discharge" activity:

The bridge will be supported by new abutments on each end and will be pile supported at two locations in the Ala Wai Canal. Bridge foundation work in the water consists of drilled or driven concrete piles at both bridge end

SECTION 401 WATER QUALITY CERTIFICATION APPLICATION

abutments and at two locations near mid-span. A concrete pile cap will be poured to match the existing Kalakaua Avenue bridge piers as closely as possible. Finally, the above-water work will consist of the bridge structure itself including columns, abutments, railings and pedestrian lighting. Conditions will be placed on the Contractor during the bridge construction to mitigate impacts to: traffic and pedestrian movement, nearby "exceptional trees", recreational canal users, historic resources and water quality.

- c. **Indicate the location(s) at which such "discharge(s)" may enter state waters (body of water, latitude, longitude, etc.):**

The affected State water is the Ala Wai Canal, located at approximate coordinates:

Latitude: 21° 17' 30"

Longitude: 157° 50' 15"

The project location is more clearly shown on Exhibit 3.

- d. **Specify biological, chemical, thermal and any other pertinent characteristic of the "discharge" activity:**

The "discharge activity" which triggers the Federal DA Nationwide Permit #25 *Structural Discharges*, consists primarily of the placement of sheet piling, tremie concrete and cast-in-place concrete structures in the Ala Wai Canal.

Proper placement practices (American Concrete Institute 304 R-89) using tremie concrete are not expected to contribute to the biological or chemical quality of the adjacent Ala Wai Canal waters. Concrete placement and curing results in a hydrophobic, exothermic reaction which may actually contribute a slight rise in ambient canal water temperature in the area immediately surrounding the formwork. However, this rise will be short term (<48 hrs) and will actually be dissipated quickly due to the large volume of the canal water surrounding the forms.

Based on the above description, there are no significant biological, chemical or thermal characteristics of the "discharge activity".

The construction activities do have the potential to 1) create a sediment plume during pile driving/drilling and sheet pile placement; 2) create a visible plume during cast-in-place construction of the concrete pile cap; and 3) create a visible plume during removal of the sheet piles.

Construction impacts can be handled by appropriate Best Management Practices as outlined in the BMP Plan of this application.

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SECTION 401 WATER QUALITY CERTIFICATION APPLICATION

5. Treatment of Discharge Wastes/Effluent

- a. Describe the function and operation of equipment or facilities employed in the treatment of wastes or other effluents which may be discharged:

As a part of the contractor's daily mobilization operations, turbidity containment devices such as silt screens will be put in place, completely encircling that day's impacted construction area and will remain in-place until the operation has been completed at each location.

The silt screens will be deployed around the perimeter of any barges carrying pile driving, dredging or drilling equipment which could create a turbidity plume during its use.

Installation and maintenance of the silt screens will be the responsibility of the contractor. At no time during potential turbidity-producing operations, may the silt screens be removed.

The silt screens will be deployed prior to the start of each potential turbidity-producing activity and will remain in place for the duration of that activity. The silt screens will remain in place overnight as the activity is ceased at the end of each work day. The silt screens will be removed when the activity is completed and the water quality has returned to that day's pre-construction condition.

The equipment barges and silt screens will be placed to allow passage to and from the ocean for water sports and recreational activities. The contractor will coordinate the work schedule with the water activities to ensure safe passage through the work area.

The equipment barges and silt screens will remain in place during non-construction periods. The contractor will provide appropriate signage and safety precautions for passage by water sports and recreational activities.

Should a turbidity plume become apparent outside the perimeter of the silt screens, contractor shall follow the procedures outlined in this project's Best Management Practices Plan.

- b. Specify the degree of treatment of wastes and/or other effluents expected to be obtained:

A BMP Plan and Water Quality Monitoring (WQM) Plan is to be followed by the contractor for turbidity-producing activities of the in-water construction stage of the project

**SECTION 401 WATER QUALITY CERTIFICATION
APPLICATION**

Any construction dewatering work done by the contractor on this project will comply with an approved NPDES General Permit for Construction Dewatering from the Department of Health Clean Water Branch. Approved treatment methods for the dewatered effluent will be specified in the permit, which is expected to be processed shortly.

6. Project Schedule

- a. **Provide the date(s) on which the project activity will begin and end:**

The pedestrian bridge is scheduled to commence construction in late-1998/early-1999, depending on funding availability, and be completed by late-1999.

- b. **Provide the date(s) on which the "discharge(s)" will take place:**

A schedule of the expected discharge activities will depend on the contractor's methods and can not be determined at this time.

7. Monitoring Program

Describe the methods and means that would be used to monitor:

- a. **Receiving water quality:**

Visual inspection of the receiving water body, Ala Wai Drainage Canal, will be conducted prior to the start of construction every day, at least once a day during in-water work, and following ceasing of daily operations to monitor the physical quality of the canal. These observations shall be noted in a daily log by the contractor. If a physical change in the canal is observed resulting from the in-water operations, mitigative measures shall be employed as discussed in the BMP Plan. If the mitigative measures do not control the pollutant, the discharge will discontinued, the Department of Health notified and, as appropriate, alternate remedial measures will be developed.

Water quality monitoring parameters, frequency of sampling, and sampling locations in the State receiving water are described in more detail in this project's WQM Plan.

- b. **Characteristics of the "discharge":**

As described in Item 4d. above, the "discharge", by its definition under DA NWP #25, has no biological, chemical or thermal characteristics and therefore requires no monitoring .

**SECTION 401 WATER QUALITY CERTIFICATION
APPLICATION**

- c. **The operation of equipment or facilities employed in the treatment or control of wastes or other effluents:**

Installation and maintenance of the silt screens will be the responsibility of the contractor. At no time during potential turbidity-producing operations, may the silt screens be removed.

8. Discharge Receiving Waters

- a. **Describe the classification of state water(s) and the associated recreational uses at the "discharge" site(s):**

The Ala Wai Canal is a Class 2 Inland Water Estuary.

Recreational uses throughout the canal range from canoeing, kayaking, boating and fishing/crabbing along the muddy canal banks.

- b. **Provide a signed statement that, "In accordance with the State of Hawaii, Department of Health, Water Quality Standards, there is reasonable assurance that the proposed activity will be conducted in such a manner which will not violate the basic water quality criteria applicable to all waters and the specific water quality criteria applicable to the class of receiving waters where the proposed "discharge" would take place."**

A signed statement has been provided in the previously submitted letter dated March 27, 1998.

See attached "assurance" letter from the State of Hawaii Department of Transportation.

(Note: This will be one of the key elements in the determination to issue a Section 401 WQC. This statement must be signed by the applicant and included as part of the application package.

9. Supporting Documentation

- a. **Submit applicable maps, plans, specifications, copies of associated federal permits or licenses, application, etc., as part of the application package and list here:**

A copy of all the documents listed in Section 9 of the application is provided with the exception of the NPDES Dewatering Permit and DLNR Stream Channel Alteration Permit applications. Both applications are expected to be submitted in (May/June 1998) and will be forwarded to the Clean Water Branch under separate cover.

**SECTION 401 WATER QUALITY CERTIFICATION
APPLICATION**

The following permits and approvals are expected to be obtained for this project.

- 1) U.S. Army Corps of Engineers, Nationwide Permit #25 Structural Discharges;
- 2) State of Hawaii Department of Health Clean Water Branch, Section 401 Water Quality Certification and NPDES General Permit for Construction Dewatering;
- 3) U.S. Coast Guard 14th District Bridges & Causeways Approvals;
- 4) State of Hawaii Department of Land and Natural Resources Commission on Water Resource Management, Stream Channel Alteration Permit;
- 5) City and County of Honolulu Department of Land Utilization, Flood Fringe Certification for a Waikiki Special District Approval;
- 6) Federal Consistency Determination (CZM).

b. Submit copies or citation of an Environmental Assessment and/or Environmental Impact Statement, as it may apply, and list here:

- 1) An Environmental Assessment has been prepared in accordance with Chapter 343 HRS and Title 11 Chapter 200 HAR. A Draft EA was published in the Environmental Bulletin in April 23, 1998 OEQC Environmental Notice.

10. Additional Information

Explain any irregularities or unique features of the project:

NONE

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**SECTION 401 WATER QUALITY CERTIFICATION
APPLICATION PACKAGE REQUIREMENTS - CHECKLIST**

The applicant should use this checklist to verify that the Section 401 Water Quality Certification (WQC) application package is in order. If, in processing the application, the Director determines that additional information is required, you will be directed to provide such information.

Section 401 WQC Requirement	Requirement Has Been Met	
	Yes	No
1. All of the line items in the application have been addressed.	<u> X </u>	<u> </u>
2. Supporting information/documentation has been attached to or enclosed with the Section 401 WQC application.	<u> X </u>	<u> </u>

The following items may be combined in a letter, signed by the applicant, and must be submitted to the Department of Health as part of the Section 401 WQC application package:

3. If applicable, a statement, signed by the applicant, naming and authorizing its agent, who shall act on its behalf to fulfill the Section 401 WQC application processing requirements.	<u> X </u>	<u> </u>
4. A statement of assurance, signed by the applicant, with respect to the Water Quality Standards. (Refer to Item No. 8.b. of the application)	<u> X </u>	<u> </u>
5. A statement of choice for publication of either a public notice of proposed action or a public notice of public hearing.	<u> X </u>	<u> </u>
6. Transmittal of the Section 401 WQC filing fee, if applicable: \$100.00 certified check or money order made payable to the State of Hawaii.	<u> </u>	<u> X (N/A) </u>

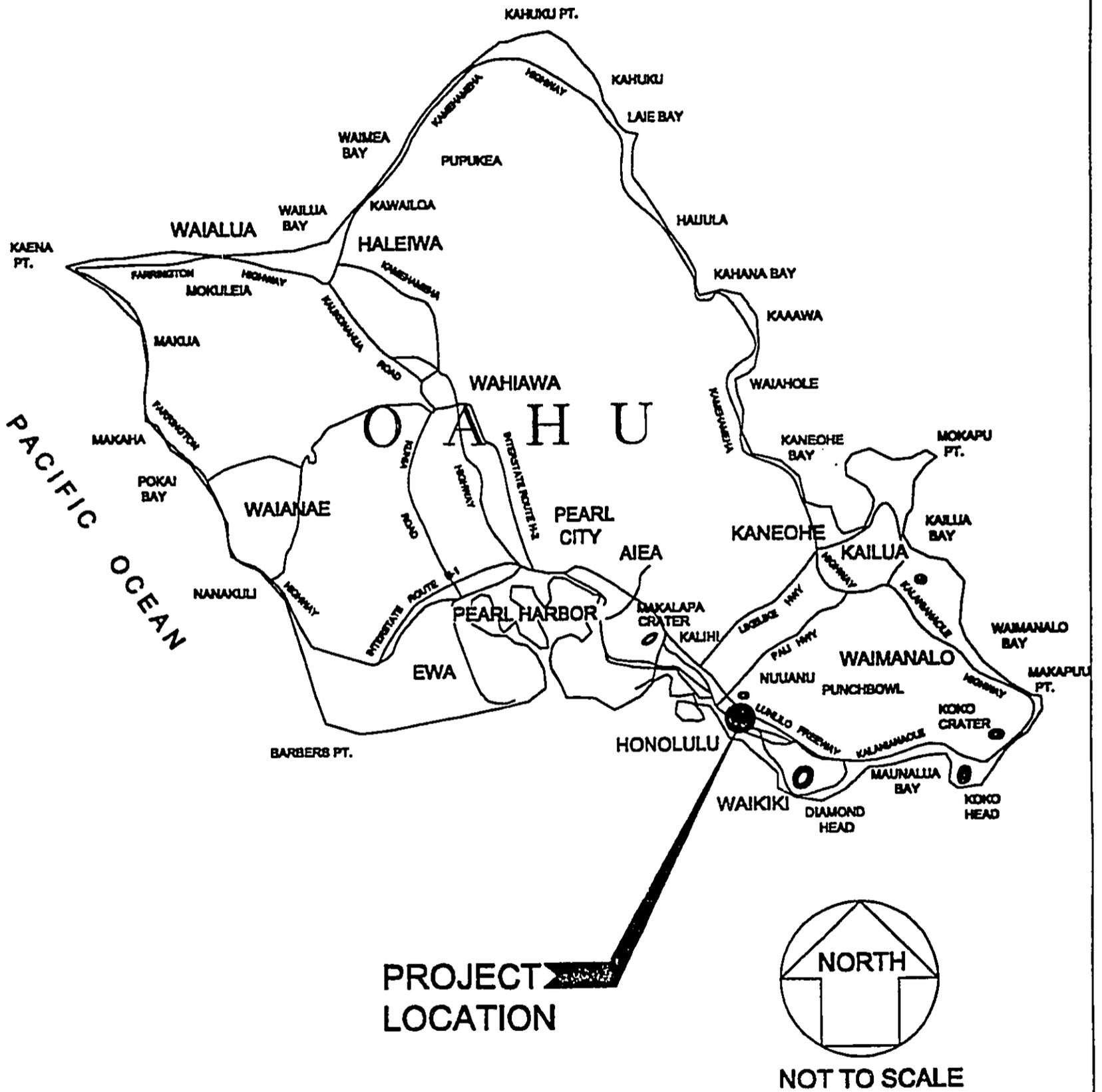
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**SECTION 401 WATER QUALITY CERTIFICATION
APPLICATION PACKAGE REQUIREMENTS - CHECKLIST**

The applicant shall be required to provide the appropriate number of copies of the Section 401 WQC application package.

7. For activities proposed for the Island of Oahu, two (2) copies have been provided or for activities proposed for islands other than Oahu, three (3) copies have been provided. X _____

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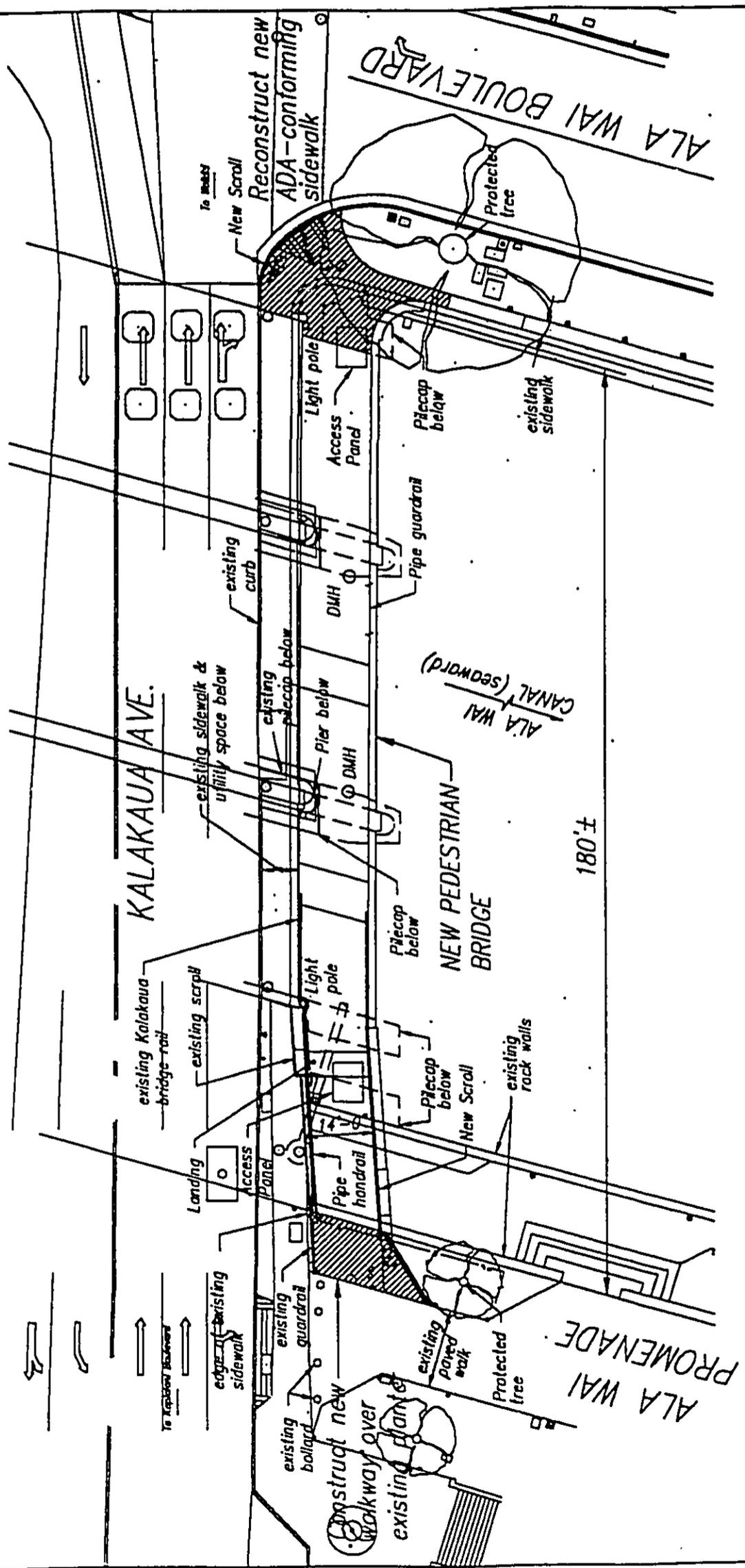
KALAKAUA AVENUE BRIDGE IMPROVEMENTS

FIGURE

ISLAND MAP

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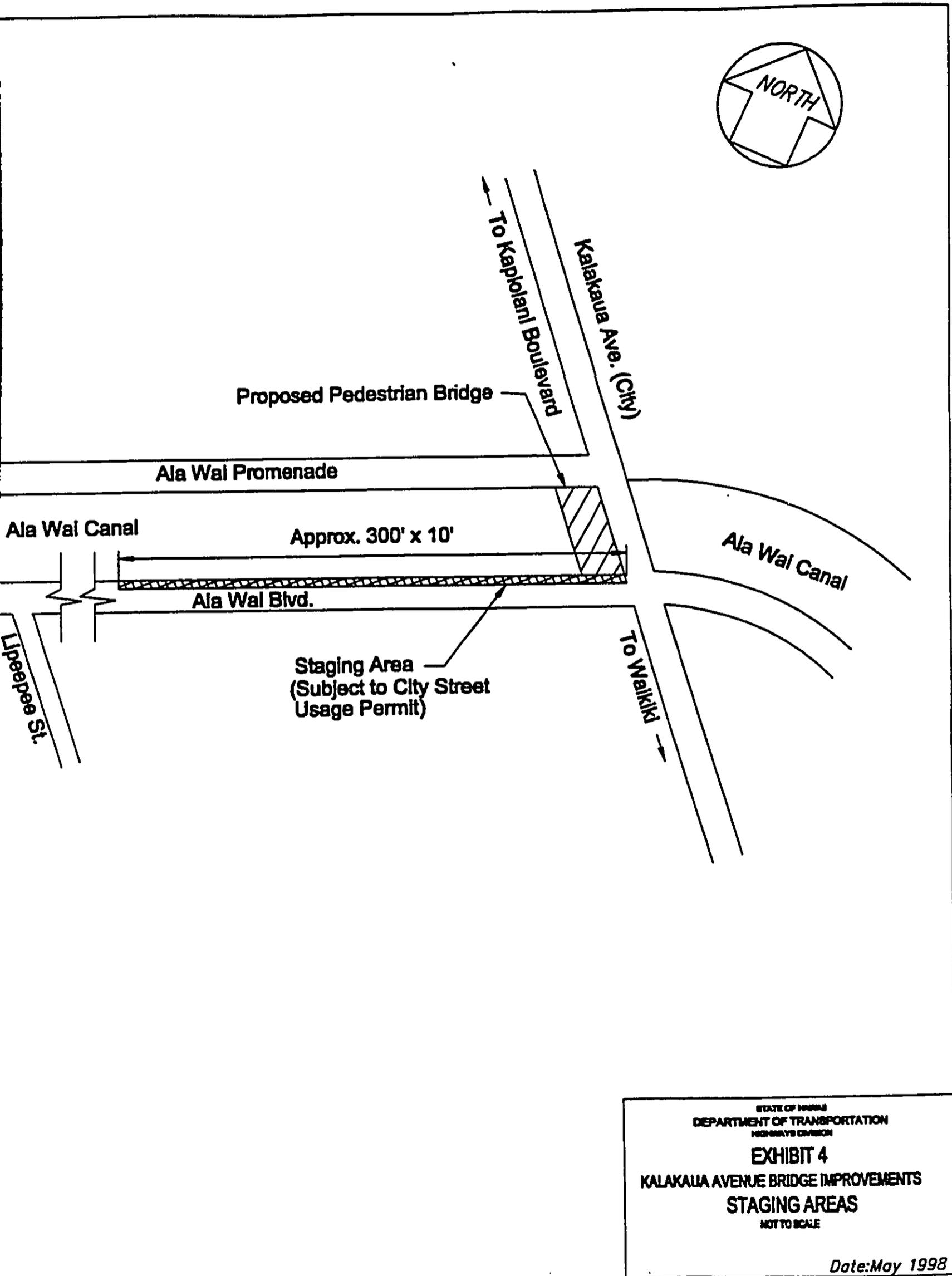


KALAKAUJA AVENUE BRIDGE IMPROVEMENTS

FIGURE 3

PEDESTRIAN BRIDGE PLAN

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BEST MANAGEMENT PRACTICES (BMPs) PLAN KALAKAUA AVENUE BRIDGE IMPROVEMENTS PROJECT

July 2, 1998

INTRODUCTION

In accordance with requirements to provide for Best Management Practices to be applied before, during and after potential pollution-producing activities, the following methods, measures and practices will be applied to the work to be performed for the planned Kalakaua Avenue Bridge Improvements project for the State of Hawaii, Department of Transportation – Highways Division.

PROJECT DESCRIPTION

The proposed Kalakaua Avenue Bridge Improvements project will include a three-span, concrete pedestrian bridge abutting and reflecting in profile, length, size and architectural detail the existing historic bridge. This new construction does not affect the structural integrity of the existing bridge since the new pedestrian bridge will be an independent, free-standing structure separated by a non-structural expansion joint. The separation of pedestrians from vehicular traffic through the use of the existing bridge railing is designed to be an improvement over the existing condition. Grade and slope adjustment improvements to the existing sidewalk at the Ala Wai Boulevard end of the new bridge will be designed to remedy the current non-conforming ADA conditions.

The bridge will be supported by new abutments on each end of the bridge and be pile supported in two location in the Ala Wai Canal.

PRE-CONSTRUCTION SOILS EXPLORATION PROGRAM *(not part of this 401 WQC Application – for information only)*

The soils exploration program for this project consisted of a total of four (4) borings. Two (2) borings were drilled over water at each pier and two (2) holes were drilled on-land at the abutment locations. A Section 401 Water Quality Certification (File No. WQC400 – processing waived) and DA Nationwide Permit #6 (File No. 98000077) were processed as a part of this activity.

The borings for the piers were located in the canal and were drilled using a truck or skid mounted drill rig installed on a small 20' x 30' barge. The method used to drill the borings consisted of rotary wash drilling using temporary steel casing approximately 4 and 6 inches in diameter. Water for the drilling activities consisted of water pumped directly from the Ala Wai Canal. Each of these holes were drilled to depths of approximately 100 feet or so below the mudline.

0000 0024 2556

**Best Management Practices Plan
Kalakaua Avenue Bridge Improvements
July 2, 1998
Page 2**

The borings for the abutments were drilled using a truck or trailer mounted drill rig using rotary wash drilling methods and temporary steel casings. These holes were approximately 4 inches in diameter and roughly 100 feet or so in depth below the existing ground surface.

Silt screens were deployed around the drilling rig platform for the in-water drilling work.

SCHEDULE OF ACTIVITIES

The Kalakaua Avenue Bridge Improvements is scheduled to commence construction in late-1998/early-1999, depending on availability of funding, and be completed by late-1999.

ACTIVITIES BEFORE CONSTRUCTION

The Contractor will ensure that all gasoline and/or diesel equipment, hydraulic equipment, accessory fuel storage tanks, and other sources of petroleum, oils, and lubricants are properly stored in leak proof storage tanks or containers. Secondary containment shall be provided for accessory fuel storage tanks, i.e. drip pans. Tanks, containers or equipment found to be leaking will be replaced or repaired.

DURING IN-WATER WORK

During in-water work, a potential for increased turbidity may result:

1. Potential water quality impacts due to construction activities will be mitigated through deployment of silt screens. A schematic sketch of a typical barge set-up with silt screens is shown by Exhibit BMP-1.

The silt screens will be deployed prior to the start of each turbidity-producing activity and will remain in place for the duration of that activity. The silt screens will remain in place overnight as the activity is ceased at the end of each work day. The silt screens will be removed when the activity is completed.

Best Management Practices Plan
Kalakaua Avenue Bridge Improvements
July 2, 1998
Page 3

2. The equipment barges and silt screens will be placed to allow passage to and from the ocean for water sports and recreational activities. The contractor will coordinate the work schedule with the water activities to ensure safe passage through the work area.

The equipment barges and silt screens will remain in place during non-construction periods. The contractor will provide appropriate signage and safety precautions for passage by water sports and recreational activities.

3. No new source of water will be allowed to discharge into the Ala Wai Canal.

No unpermitted discharges will be allowed into the Ala Wai Canal or existing storm drains during the construction of the pier caps, abutments and bridge structure. The Contractor may deploy various means to control unpermitted discharges during concrete pours and pile cutting (should driven piles be used), including drip pans, collars, troughs or hoppers.

4. Additional silt screens will be installed under the following conditions:

- A turbidity plume is readily observed and readily apparent to the casual observer from adjacent streets or the Kalakaua Bridge; or
- Turbidity generated as a result of any in-water work is greater than representative turbidity values measured prior to the start of the work.

Should additional silts screens be required, the following additional measures will be instituted as necessary:

- The immediate area directly adjacent to the barge shall be restricted to boat traffic during work hours. Other areas beneath the bridge, not affected by the work, may continue to be utilized for through boat traffic; and
- Should water quality monitoring indicate that representative turbidity values measured prior to the start of the in-water work are exceeded with the silt screens in place, an additional silt screen may be installed to further reduce potential water quality impacts.

5. The contractor will notify their work crew that leakage of petroleum, oils and/or lubricants into the Ala Wai Canal is not allowed. Should any major spillage occur, the Department of Health, Clean Water Branch, and the U.S. Coast Guard will be notified immediately in order that appropriate mitigation measures may be employed.

0000 0024 2558

**Best Management Practices Plan
Kalakaua Avenue Bridge Improvements
July 2, 1998
Page 4**

POST IN-WATER ACTIVITIES

After the completion of in-water operations, all equipment, silt screens and the barge mobilized to the site for the bridge work will be removed.

MISCELLANEOUS PLAN PRACTICES

The following standard practices will be followed by the contractor during construction:

The effectiveness and adequacy of the BMPs shall be reviewed and updated as often as needed;

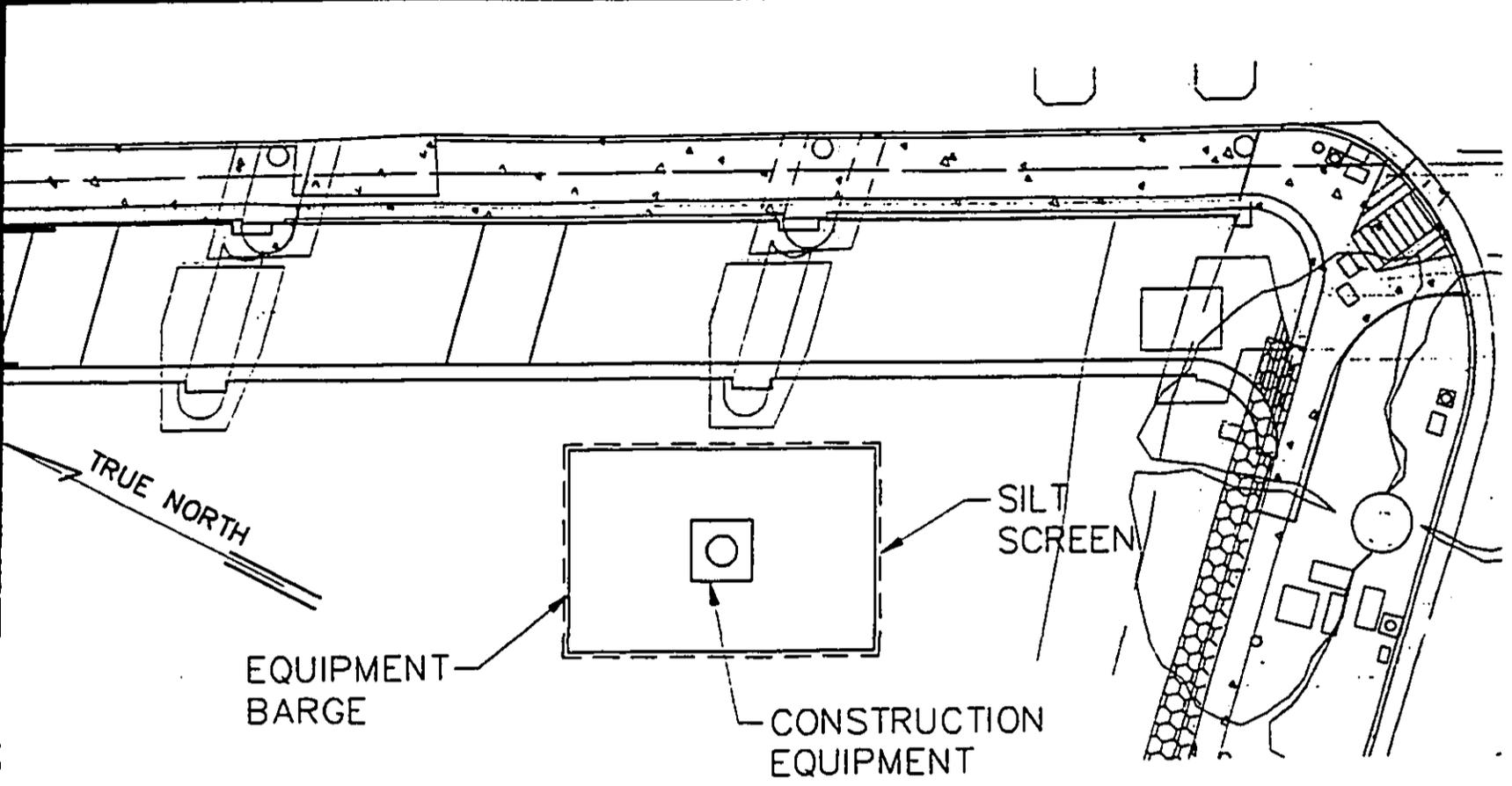
Any changes to this project's BMP or WQM plans, or corrections or modifications to information already on file with the Department of Health, shall be submitted to the Clean Water Branch for review and approval prior to implementation;

The discharge of any construction dewatering effluent into the canal waters shall conform to an NPDES General Permit for Construction Dewatering which will be processed through the Department of Health Clean Water Branch shortly.

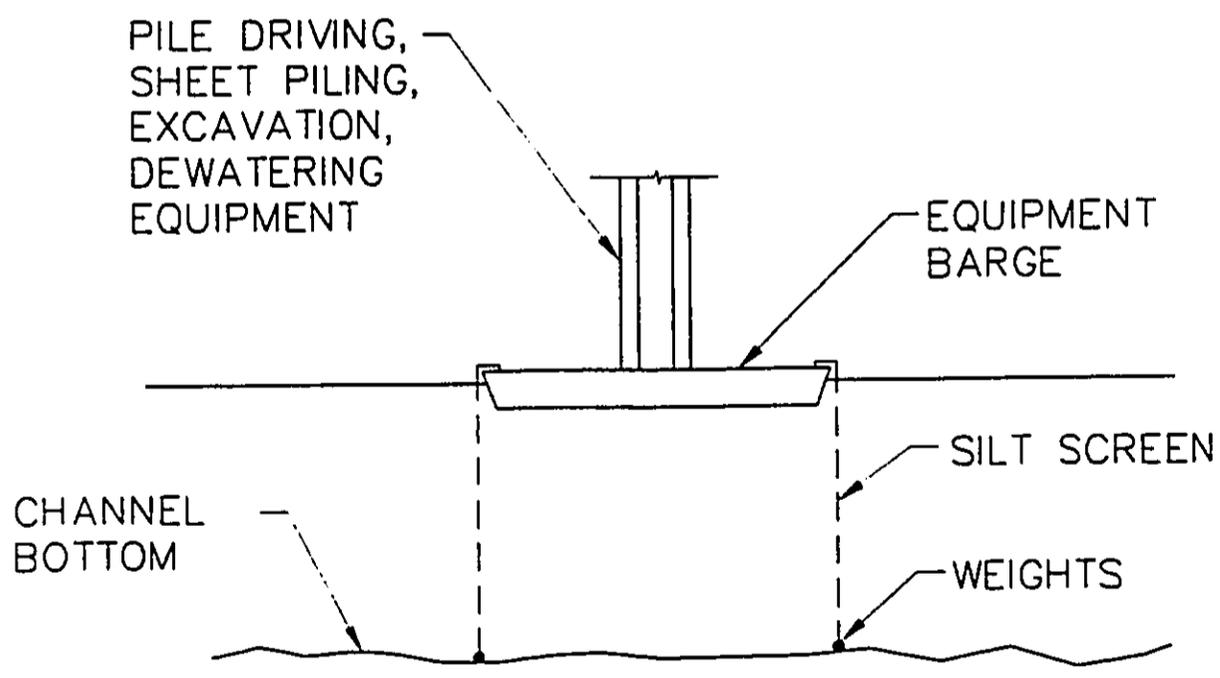
- oOo -

Attachments: **Exhibit BMP-1**

0000 0024 2559



PLAN



SECTION

DATE	REVISION	ENG

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

EXHIBIT BMP-1
TYPICAL CONSTRUCTION
BARGE SET-UP FOR
IN-WATER WORK

Scale: As Shown Date: Feb 1998
SHEET No. OF SHEETS

0000 0024 2560

WATER QUALITY MONITORING PLAN KALAKAUA AVENUE BRIDGE IMPROVEMENTS PROJECT

July 2, 1998

INTRODUCTION

This water quality monitoring plan outlines monitoring procedures to be employed daily during the in-water work planned for the construction of the above subject project. For the purposes of this Water Quality Monitoring Plan (WQMP), "in-water work" shall refer primarily to daily mobilization and demobilization of barges and related equipment for use in the bridge construction; driving and removal of sheet piling surrounding the impact area; construction of a cofferdam; driving or drilling of concrete piles within the sheet pile cofferdam; concrete pile cap work within the cofferdam; pier construction; abutment construction; pedestrian bridge construction; and water pollution control (BMPs) and public safety measures during work hours within the canal.

The cofferdam, constructed of driven interlocking sheet piles, will be located inside the silt screen perimeter ("impact area") and approximately 2'-3' off the proposed pile cap face. The duration of this in-water work is estimated to take between three and seven months to complete. Silt screens will be deployed around the cofferdams every day that work is done within them.

The equipment barges and silt screens will remain in place during non-construction periods. The contractor will provide the appropriate signage and safety precautions for passage by water sports and recreational activities.

Visual inspection of the receiving water body, Ala Wai Drainage Canal, will be conducted at least three times a day to monitor the physical quality of the canal. If a physical change in the canal is observed resulting from the in-water operations, mitigative measures shall be employed as discussed in the BMP plan. If the mitigative measures do not control the pollutant, the discharge will be discontinued, the Department of Health notified and, as appropriate, alternate remedial measures will be developed.

MONITORING PLAN

1. Turbidity will be monitored approximately 75 to 100 feet directly upstream of the proposed impact area, within one meter downstream of the silt screen at each impact area, and approximately 250 feet directly downstream of the impact area. For the purposes of this WQMP, the "impact area" is assumed to be the area surrounded by and including the sheet pile cofferdam.

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Water Quality Monitoring Plan
Kalakaua Avenue Bridge Improvements
July 2, 1998
Page 2

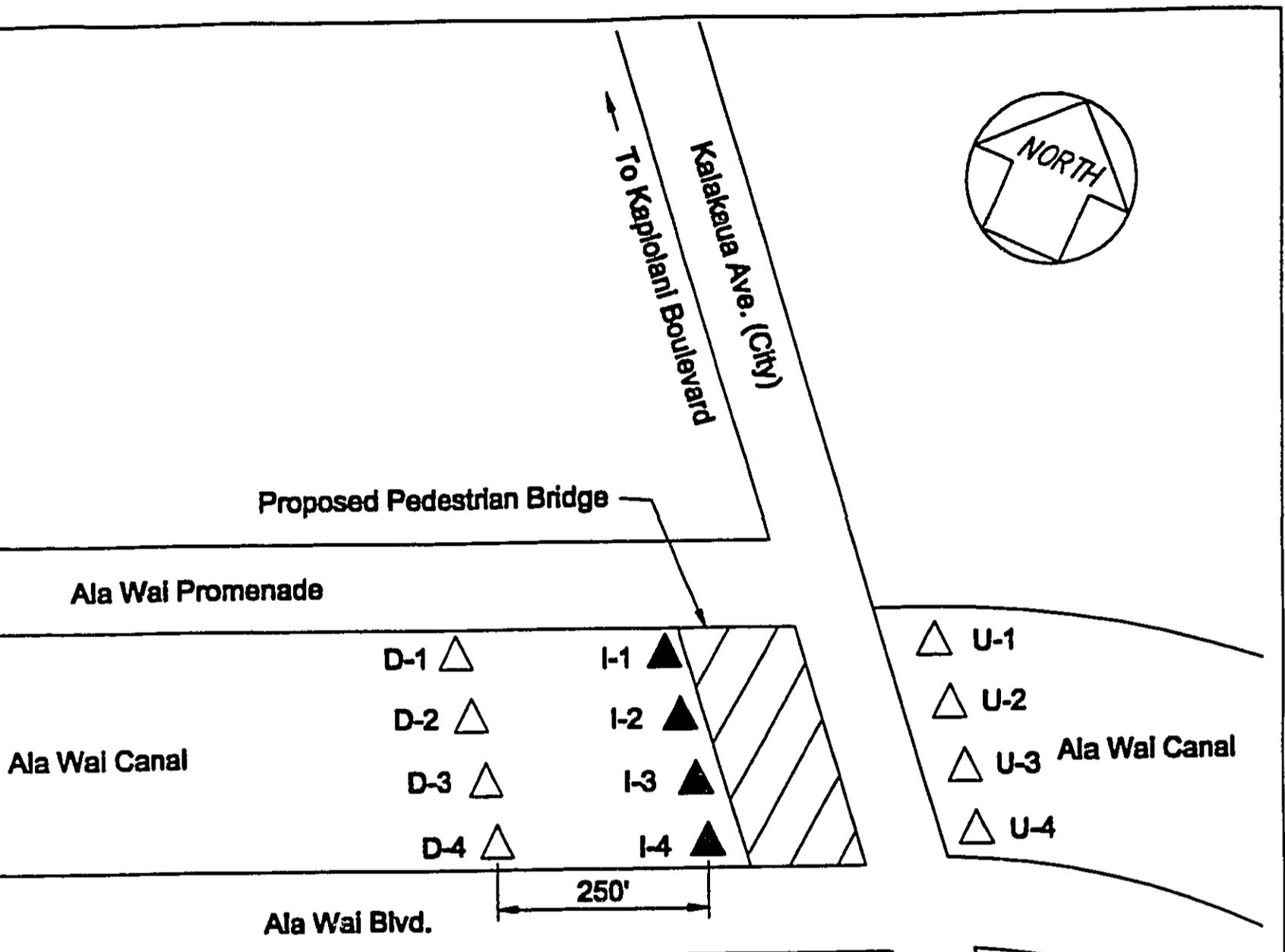
The Contractor shall provide at least 10 samples for the pre-construction monitoring period, commencing at least 2 weeks before construction activities start. One grab sample shall be taken per work day (Monday-Friday) for two weeks at the project "Impact Area" (location anywhere between I-2 or I-3) as depicted in Exhibit WQ-1 of this Water Quality Monitoring Plan.

Post construction sampling will depend upon the significant impact of the construction activity as determined by the sampling results obtained during construction.

2. A sample of the water at each monitoring location will be obtained using a disposable teflon bailer. Each sampling station will consist of a sample at the surface and about mid-depth using the bailer.
3. Turbidity will be measured in the field using a turbidity meter. The meter will be calibrated prior to the start of each day of drilling. A record of daily calibrations and measurements will be maintained.
4. Weather conditions, such as approximate wind strength and direction; visual monitoring of rainfall at the site and in the upstream watersheds (mountains); and incoming or outgoing tides will be noted at the time of the turbidity readings based on site observations and available tide charts, and will be taken into consideration when reviewing the test results.
5. The monitoring will be performed under the direction of field personnel properly trained in the use of the sampling and testing equipment prior to the start of work.
6. Daily turbidity results will be faxed to the State DOH, Clean Water Branch at (808) 586-4352 as soon as it becomes available.

Attachment: **Exhibit WQ-1 – Proposed Water Quality Monitoring Locations**

0000 0024 2562



LEGEND

- ▲ — "IMPACT AREA" MONITORING STATION
- △ — UPSTREAM*/DOWNSTREAM* MONITORING STATION
- * VARIES WITH TIDE AND STREAMFLOW

"D-1" SAMPLE NO.

NOTE:

IMPACT AREA SAMPLING LOCATIONS TO BE LOCATED WITHIN ONE (1) METER OF THE SILT SCREENS

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

EXHIBIT WQ-1
KALAKAUA AVENUE BRIDGE IMPROVEMENTS
PROPOSED WATER QUALITY
MONITORING STATIONS
NOT TO SCALE

Date: May 1998

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APPENDIX B
SUMMARY OF
PUBLIC INFORMATION
MEETINGS & PRESENTATIONS

0000 0024 2564

April 7, 1998

ORGANIZATION	MEETING DATES	COMMENTS
CITY		
1 Honolulu Board of Water Supply	Jan 6, Feb 13	Coordinated with Agency
2 Department of Land Utilization	Jan 9	Coordinated with Agency
3 Department of Public Works	Jan 6, Feb 13	Coordinated with Agency
4 Department of Wastewater Management	Jan 6, Feb 13	Coordinated with Agency
5 Department of Transportation Services	Jan 6, Feb 13	Coordinated with Agency
6 Exceptional Tree Committee	Feb 18	No objection to project
7 Transition Plan Consultant for City	Jan 24	Coordinated with Agency and Consultant
8 Ala Wai Canal Dredging Consultant	Mar 5	Coordinated with Consultant
STATE		
9 Dept of Health - Comm on Persons with Disab	Jan 7, 15, 30	Coordinated with Agency
10 Dept of Health - Clean Water Branch	Jan 22	Coordinated with Agency
11 DLNR - State Historic Preservation Division	1997: Oct 17, Nov 20, Dec 23. 1998: Feb 4	Coordinated with Agency - No adverse effect ruling
FEDERAL		
12 US Army Corps of Engineers	Jan 5	Coordinated with Agency
NEIGHBORHOOD BOARDS AND MEETINGS		
13 NB No. 8 - McCully/Moiliili	Mar 5	No objection to project. One Bd member and Public Michelle Matson objected to project. Concerns were compliance with Historic Preservation regulations.
14 NB No. 9 - Waikiki	Mar 17, Apr 21	
15 NB No. 11 - Ala Moana/Kakaako	Mar 24	
16 Public Informational Meeting	Mar 12	One person objected to project - Michelle Matson
17 Joint Advisory Council	Feb 4	Sam Bren & Don Steiner liked design but want street widening done at same time
18 Community Briefing	Mar 26	Similar remarks repeated again by individuals noted in 13, 25 plus one other. One resident wants bridge to carry vehicles
ATHLETIC ORGANIZATIONS		
19 Na Opio Canoe Racing Association	Mar 2	No objections. Keep at least one arch open at all times
20 Oahu Canoe Racing Association	Mar 2	No objections. Keep at least one arch open at all times
21 Outrigger Canoe Club	Mar 2	No objections. Keep at least one arch open at all times
22 Na Ohana Ona Hui Wa'a Canoe Assoc	Mar 2	No objections. Keep at least one arch open at all times

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COMMENTS

MEETING DATES

COMMUNITY ORGANIZATIONS	MEETING DATES	COMMENTS
23 Waikiki Improvement Association	Feb 11	Favored project
24 The Outdoor Circle/via Exceptional Tree Committee	Feb 18	Wants exceptional trees protected during construction. Concerns: dust, water qual, noise.
25 Kapiolani Park Preservation Society	Mar 2	Objection to the bridge - wrong location. KPPS stated that they own certain rights to the Ala Wai Canal
CONDO ASSOCIATIONS AND BUSINESSES		
26 1717 Ala Wai Condo	Feb 23	No objection from Board. Resident Sam Bren objects to the noise of pile driving.
27 Waikiki Landmark	Mar 10	No objection from Board.
28 Eaton Square Condo	Feb 26	Met but Board did not allow presentation of the project.
29 Marina Towers	Mar 10, 12	Board moved meeting date, did not meet. Met Bldg Migr who had no objections.
30 Hard Rock Café	Feb 12	Favored project
31 Hilton Hawaiian Village	Feb 23	Favored project
32 1833 Kalakaua Office Bldg	Feb 27	Favored project
ELECTED OFFICIALS		
33 Senator Les Ihara (10th, Kapahulu - Waialae)	Feb 2, Mar 26	Met with Aide Elaine Lee, Senator
34 Senator Carol Fukunaga (12th, Makiki - Ala Moana)	Mar 26	Met Senator
35 Representative Galen Fox (21st, Waikiki - Ala Wai)	Feb 2	Met Representative
36 Councilman Duke Bainum	Feb 2	Met with Aide Linda Iwasaki
37 Councilman Andy Mirikitani	Mar 5, Mar 12	Discussed with Aides Hank and Cindy McMillan

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DOCUMENT CAPTURED AS RECEIVED

COMMENTS

MEETING DATES

COMMUNITY ORGANIZATIONS	MEETING DATES	COMMENTS
23 Waikiki Improvement Association	Feb 11	Favored project
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34 Senator Carol Fukunaga (12th, Makiki - Ala Moana)	Mar 26	Met Senator
35 Representative Galen Fox (21st, Waikiki - Ala Wai)	Feb 2	Met Representative
36 Councilman Duke Bainum	Feb 2	Met with Aide Linda Iwasaki
37 Councilman Andy Mirikitani	Mar 5, Mar 12	Discussed with Aides Hank and Cindy McMillan

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APPENDIX C
DRAFT ENVIRONMENTAL ASSESSMENT
COMMENTS & RESPONSES

0000 0024 2568

DRAFT EA COMMENTS & RESPONSES

CITY & COUNTY OF HONOLULU AGENCIES

0000 0024 2569

DEPARTMENT OF PARKS AND RECREATION
CITY AND COUNTY OF HONOLULU

HONOLULU BOTANICAL GARDENS
50 NORTH VINEYARD BOULEVARD • HONOLULU, HAWAII 96817
PHONE: (808) 522-7060 • FAX: (808) 522-7050



WILLIAM D. SALFOUR JR.
DIRECTOR OF PARKS AND RECREATION

May 4, 1998

Norman Nagamine, S.E.
Nagamine Engineers Inc.
1001 Bishop Street
Pauahi Tower, Suite 725
Honolulu, Hawaii 96813

DIRECTOR'S OFFICE
MAY 5 10 21 AM '98

Dear Norman:

Re: Kalakaua Avenue Bridge Improvements/Ficus benjamina Exceptional Tree

This letter is in reference to our telephone conversation of April 30, 1998 relative to the concerns of the Mayor's Arborist Advisory Committee with the wording appearing in the semimonthly bulletin of The Office of Environmental Quality Control, specifically "The 'exceptional' Banyan trees along the Ala Wai Promenade may be altered or removed to make way for construction activities".

In our view, our meetings with you on February 25, 1998 and April 8, 1998 produced unanimous agreement that only one Banyan Tree was affected by construction and negative impacts on that tree, clearly figured in your drawings, would be restricted to minimal root and canopy pruning. A list of mitigating requirements, mutually agreed to, are to be placed on appropriate drawings.

We request that you send a letter to the Mayor's Arborist Advisory Committee reaffirming our understanding and stating that trees will not be removed. In addition, please state in your letter that the language quoted above will be deleted in all final documents pertinent to this project.

Sincerely,

Paul R. Weissich/lt

PAUL R. WEISSICH, Chairman -
Arborist Advisory Committee

PRW:lt

- cc: Governor Ben Cayetano, State of Hawaii
- ✓ Kazu Hayashida, Director, State Department of Transportation
- Mayor Jeremy Harris, County of Honolulu
- The Outdoor Circle
- Irvin Higashi, Walters Kimura & Motoda, Landscape Architects

0000 0024 2570

AMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINA'I
GLENN M. OKIMOTO

IN REPLY REFER TO:

HWY-DS
2.9455

JUN 10 1998

Mr. Paul R. Weissich
Arborist Advisory Committee
City and County of Honolulu
Department of Parks and Recreation
50 North Vineyard Boulevard
Honolulu, Hawaii 96817

Dear Mr. Weissich:

Subject: Draft Environmental Assessment
Kalakaua Avenue Bridge Improvements

Thank you for your May 4, 1998, letter to Nagamine Engineers Inc. commenting on the Draft Environmental Assessment for the proposed Kalakaua Avenue Bridge Improvements.

On May 28, 1998, we met with the Arborist Advisory Committee and The Outdoor Circle at the project site to clarify that we are not proposing removal of any banyans trees along the Ala Wai Promenade or the Ala Wai Boulevard. To provide overhead clearance for construction machinery and to install underground electrical conduits, we propose selective trimming of the canopies of three trees nearest the work area and pruning the roots of one of them.

This meeting reached an understanding of appropriate trimming, pruning, and protection of the trees. We will prepare plans detailing the impacts and mitigative measures for the affected trees and will submit the plans to your office for approval.

The Final Environmental Assessment will present the general impacts and treatments for the trees and will confirm that we are not planning removal of any of the banyan trees. Should you require more information about this matter, please contact Edmund Yoshida at 587-2122.

Very truly yours,

A handwritten signature in cursive script, appearing to read "Kazu Hayashida".

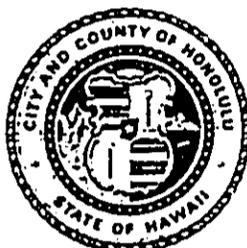
KAZU HAYASHIDA
Director of Transportation

c: Office of Environmental Quality Control
The Outdoor Circle

0000 0024 2571

OFFICE OF THE MANAGING DIRECTOR
CITY AND COUNTY OF HONOLULU

WAIKIKI DEVELOPMENT
650 SOUTH KING STREET, 8TH FLOOR
HONOLULU, HAWAII 96813 • PHONE: (808) 527-5751 • FAX: (808) 523-4950



JEREMY HARRIS
MAYOR

ROBERT J. FISHMAN
MANAGING DIRECTOR

~~REDACTED~~
R. Doug Aton

~~REDACTED~~
EXECUTIVE DIRECTOR

April 21, 1998

Mr. Norman Nagamine, President
Nagamine Engineers Inc.
1001 Bishop Street
Pauahi Tower Suite 725
Honolulu, Hawaii 96813

Dear Mr. Nagamine:

Draft Environmental Assessment

Thank you for giving my office the opportunity to review the Draft Environmental Assessment for the proposed Kalakaua Pedestrian Bridge Project. Part of the function of this office is to coordinate, facilitate and build consensus on City projects in Waikiki. It is inevitable that public and private projects that have an impact on the Ala Wai Canal, directly or indirectly, often surface with the canoeing community. Hence, in my meetings with the canoeing community, the biggest concern is with blockage of one or more of the 3 bridge openings beneath the Kalakaua Bridge.

Your estimated commence date for construction (September 1998) is during the peak training period for both the Molokai Na Wahine O Ke Kai race for Women and the Bankoh Molokai Ho'e for men held in September and October of each year. The canoe traffic traversing both ways on the Ala Wai beneath this bridge is constant and heavy especially during the evening hours between 4:00 p.m. and 7:00 p.m.

The canoeing community's greatest fear is that safety is severely jeopardized with the closing of any of the three openings. As it stands now the mauka most opening is rarely utilized by the paddlers as the mauka most side of the canal is its shallowest point and offers paddlers only "dead" water or water that has the most resistance to free flow of a canoe. Hence, the middle and makai openings are the most heavily trafficked by canoes.

As you have stated in section 5.6.1 Water Activities (Page 29), it is imperative during this time to assure that the Contractor maintain communications between these affected groups to ensure a minimum disruption of both recreational activities and the construction schedule.

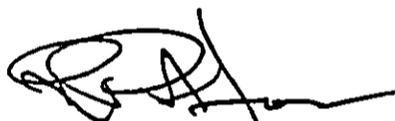
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Mr. Norman Nagamine, President
Nagamine Engineers Inc.
April 21, 1998
Page 2

I also agree with your mitigative measures to include temporary signage to warn canal users of the construction closings and their duration. Appropriate signage at both the McCully and Ala Moana Bridge during the construction would put the paddlers on alert and would avert the potential for collisions and injuries.

Should you have any further questions or concerns, please call me at 527-5751.
Thank you for the opportunity to review this document.

Sincerely,



R. DOUG ATON
Executive Director

RDA:ak

0000 0024 2573

BENJAMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

July 2, 1998

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINAII
GLENN M. OKIMOTO

IN REPLY REFER TO:
HWY-DS
2.9931

Mr. R. Doug Aton
Executive Director
Office of Waikiki Development
City and County of Honolulu
530 South King Street, 8th Floor
Honolulu, Hawaii 96813

Dear Mr. Aton:

Subject: Draft Environmental Assessment
Kalakaua Avenue Bridge Improvements

Thank you for your April 21, 1998, letter to Nagamine Engineers Inc. commenting on the Draft Environmental Assessment for the proposed Kalakaua Avenue Bridge Improvements.

We have met with the canoe associations and other users of the Ala Wai Canal to discuss the project's impacts upon water sports and recreational activities on the canal. These groups have been very helpful in informing us of their training and competition schedules.

Locating construction equipment and temporary work platforms on the canal surface adjacent to the Kalakaua Avenue Bridge has been also discussed. We have reached an understanding with the canal users that while the equipment and platforms may block two of the three arch openings beneath the bridge, one arch opening will remain passable by water activities during construction. To ensure safe passage through the work area, we will direct the contractor to maintain communications with the canal users, install appropriate warning signs, and coordinate the work schedule with the water activities.

Please direct questions concerning the above to Edmund Yoshida at 587-2122.

Very truly yours,

A handwritten signature in cursive script, appearing to read "Kazu Hayashida".

KAZU HAYASHIDA
Director of Transportation

0000 0024 2574

DEPARTMENT OF PARKS AND RECREATION
CITY AND COUNTY OF HONOLULU

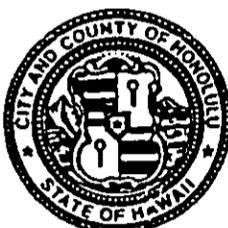
650 SOUTH KING STREET, 10TH FLOOR • HONOLULU, HAWAII 96813
PHONE: (808) 523-4182 • FAX: (808) 523-4054

RECEIVED

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JUN 8 8 36 AM '98

DEPT. OF TRANSPORTATION
HIGHWAYS DIVISION



'98 JUN -8 P1:03

WILLIAM D. BALFOUR, JR.
DIRECTOR

MICHAEL T. AMII
DEPUTY DIRECTOR

DESIGN BEARER
HIGHWAYS DIVISION
DEPT. OF TRANSPORTATION

TERESA HARRIS
MAYOR

June 1, 1998

DIRECTOR'S OFFICE
DEPT. OF
TRANSPORTATION
JUN 5 1 32 PM '98

Mr. Kazu Hayashida
Director
Department of Transportation
State of Hawaii
869 Punchbowl Street
Honolulu, Hawaii 96813

Dear Mr. Hayashida:

Thank you for the opportunity to comment on the draft environmental assessment for the Kalakaua Avenue bridge improvements.

We would like to make the following comments and recommendations:

1. During the construction for the bridge, provide at least one span that is free and clear of construction, debris, and catchment systems for canal recreational users.
2. If one span cannot be left free and clear due to safety concerns, please give affected parties at least six weeks' notice of closure.
3. Post appropriate signs or markers on the bridge and in the canal indicating the channel is "closed."
4. Schedule and perform the construction to provide the least amount of inconvenience to the general public and canal recreational users.
5. Should the contractor require use of or entrance to City and County park properties, the contractor shall seek permission from the Director of the Department of Parks and Recreation.

0000 0024 2575

Mr. Kazu Hayashida

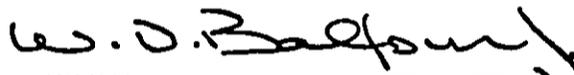
Page 2

June 1, 1998

6. For specific review of the design of the bridge and landscape improvements, please have the consultant coordinate with our Landscape Section. You may contact Mr. David Kumasaka, Landscape Architect, at 523-4884 for more information.
7. We encourage the contractor to maintain continuous communication with the local community and other affected organizations and agencies.

Please have your staff contact Mr. Donald Griffin, Chief of our Advance Planning Branch, at 527-6324 if you have any questions.

Sincerely,


WILLIAM D. BALFOUR, JR.
Director

WDB:ei

cc: Mr. Gary Gill, Office of Environmental Quality Control

0000 0024 2576

BENJAMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

July 21, 1998

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINAII
GLENN M. OKIMOTO

IN REPLY REFER TO:

HWY-DS
2.9928

Mr. William D. Balfour, Jr.
Director
Department of Parks and Recreation
City and County of Honolulu
650 South King Street, 10th Floor
Honolulu, Hawaii 96813

Dear Mr. Balfour:

Subject: Draft Environmental Assessment
Kalakaua Avenue Bridge Improvements

Thank you for your June 1, 1998, letter commenting on the Draft Environmental Assessment for the proposed Kalakaua Avenue Bridge Improvements. The following items respond to your comments and are numbered similarly as in your letter:

1. When construction platforms and equipment are placed within the Ala Wai Canal, we will direct the project contractor to leave one of the three bridge spans unobstructed for passage by canal recreational users, except when construction activities, such as overhead lifting, cause safety hazards.
2. There will be times when passage through the construction area will not be allowed due to safety hazards. To address this matter, we will direct the project contractor to coordinate the work schedule with the recreational users and to provide advance notice of those times when passage through the area will not be possible.
3. Appropriate signage and markers will be posted for the project.
4. The construction schedule will be coordinated to mitigate impact to the general public and canal recreational users.
5. The contractor will be required to obtain City approval for use of or entrance to City park properties.

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Mr. William D. Balfour, Jr.
Page 2

HWY-DS 2.9928

6. The project construction plans will be submitted for review to your Landscape Section, as you recommend.
7. We will direct the contractor to maintain communication during construction with the local community.

Please direct questions concerning the above to Edmund Yoshida at 587-2122.

Very truly yours,



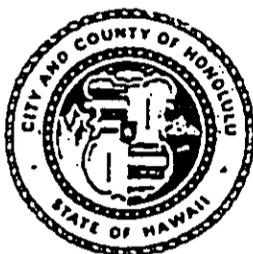
KAZU HAYASHIDA
Director of Transportation

c: Office of Environmental Quality Control

0000 0024 2578

DEPARTMENT OF PUBLIC WORKS
CITY AND COUNTY OF HONOLULU

650 SOUTH KING STREET, 11TH FLOOR • HONOLULU, HAWAII 96813
PHONE: (808) 523-4341 • FAX: (808) 527-5857



JEREMY HARRIS
MAYOR

JONATHAN K. SHIMADA, PhD
DIRECTOR AND CHIEF ENGINEER
ROLAND D. LIBBY, JR.
DEPUTY DIRECTOR
ENV 98-107

May 15, 1998

Mr. Kazu Hayashida, Director
Department of Transportation
State of Hawaii
869 Punchbowl Street
Honolulu, Hawaii 96813

Dear Mr. Hayashida:

Subject: Draft Environmental Assessment (DEA)
Kalakaua Avenue Bridge Improvements
TMK: 2-3-34

We have reviewed the subject DEA and have the following comments:

1. Page 1, Project Summary: We believe that the existing Kalakaua Avenue Bridge is still a State right-of-way which the City is currently maintaining. Please verify.
2. We wish to re-emphasize that proper design measures should take place to ensure compliance of the Americans With Disabilities Act Accessibility Guidelines.

Should you have any questions, please contact Mr. Alex Ho, Environmental Engineer, at 523-4150.

Very truly yours,

A handwritten signature in cursive script, appearing to read "J. Shimada".

JONATHAN K. SHIMADA, PhD
Director and Chief Engineer

cc: OEQC
Nagamine Engineers, Inc.

0000 0024 2579

DEPARTMENT OF WASTEWATER MANAGEMENT
CITY AND COUNTY OF HONOLULU

650 SOUTH KING STREET, 3RD FLOOR • HONOLULU, HAWAII 96813
PHONE: (808) 527-6663 • FAX: (808) 527-6675



KENNETH E. SPRAGUE P.E., Ph.D.
DIRECTOR

CHERYL K. OKUMA-SEPE, CSO
DEPUTY DIRECTOR

In reply refer to:
WCC 98-92

April 29, 1998

Mr. Norman Nagamine, President
Nagamine Engineers, Inc.
Pauahi Tower, Suite 725
1001 Bishop Street
Honolulu, Hawaii 96813

Dear Mr. Nagamine:

Subject: **Draft Environmental Assessment
Kalakaua Pedestrian Bridge
TMK: 2-3-34**

Please verify the statement made on page 33, item 5.9.1, "Existing subsurface and overhead utilities are not expected to be impacted by this project." Will the three existing 10-inch sewer lines be impacted, such as being encased or concrete jacketed?

We reserve further comment on this project upon receipt and review of the sixty percent construction plans from you. Our review of these plans will be based on the alignment and clearance of the proposed sewer line corridor.

If you have any questions, please contact Ms. Tessa Ching of the Service Control Branch at 523-4956.

Sincerely,

A handwritten signature in black ink, appearing to read "Cheryl K. Okuma-Sepe", is written over the typed name of the Director.

KENNETH E. SPRAGUE
Director

0000 0024 2580

0000 0024 2581

BENJAMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

July 8, 1998

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINAI
GLENN M. OKIMOTO

IN REPLY REFER TO:

HWY-DS
2.9932

Dr. Kenneth E. Sprague
Director
Department of Wastewater Management
City and County of Honolulu
650 South King Street, 3rd Floor
Honolulu, Hawaii 96813

Dear Dr. Sprague:

Subject: Draft Environmental Assessment
Kalakaua Avenue Bridge Improvements

Thank you for your April 29, 1998, letter to Nagamine Engineers, Inc. commenting on the Draft Environmental Assessment for the proposed Kalakaua Avenue Bridge Improvements.

Item 5.9.1 on Page 33 cites that this project does not include work for existing subsurface or overhead utilities. The primary work for this project is the construction of the pedestrian bridge structure and does not include improvements to the existing utilities.

The three existing 10-inch sewer lines will not be encased or concrete jacketed by this project. We have met with Ms. Tessa Ching and Mr. Kumar Bhagavan of your staff to discuss this project. These meetings have not identified any requirement for encasement or concrete jacketing of the existing sewer lines.

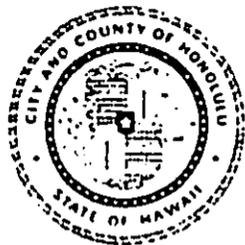
Please direct questions concerning the above to Edmund Yoshida at 587-2122.

Very truly yours,

A handwritten signature in cursive script that reads "Kazu Hayashida".

KAZU HAYASHIDA
Director of Transportation

0000 0024 2582



CITY COUNCIL

CITY AND COUNTY OF HONOLULU
HONOLULU, HAWAII 96813-3065 / TELEPHONE 547-7000

DUKE BAINUM
COUNCILMEMBER
DISTRICT IV
(808)547-7004
(808)523-4220 (FAX)
Email: bainum@co.honolulu.hi.us
<http://www.pixi.com/~bainum>

May 19, 1998

Mr. Norman Nagamine, P.E.
President
Nagamine Engineers, Inc.
1001 Bishop Street, Pauahi Tower, #725
Honolulu, Hawaii 96813

Dear Mr. Nagamine:

Subject: Draft Environmental Assessment
Kalakaua Pedestrian Bridge, Waikiki, Oahu, TMK: 2-3-34

Thank you for allowing me to review and comment on the Draft Environmental Assessment for the proposed Kalakaua Pedestrian Bridge project. The following matters are of concern:

- Pile driving noise: serious consideration is requested to be given to using the best pile driving technology that is available to lessen the impact from noise pollution;
- Location of construction equipment: to lessen the impact on the area and prevent street congestion, would it be possible to locate most of the equipment on the water? Of course we must take care to leave one segment open to access and egress the ocean for water activities; and
- Notification of DEA: the following area condominium associations have been listed in the DEA, but it appears they have not been notified. Shouldn't they be requested to comment? They are the Waipuna, Villa on Eaton Square, Kalia, Wailana and Discovery Bay.

Should you have any questions, please contact Ms. Linda Iwasaki at 527-5591.

Sincerely,

A handwritten signature in cursive script that reads "Duke Bainum".

DUKE BAINUM
Councilmember, District IV

DB:li

0000 0024 2583

BENJAMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

JUL 7 1998

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINAAI
GLENN M. OKIMOTO

IN REPLY REFER TO:

HWY-DS
2.9972

The Honorable Duke Bainum
Councilmember
City Council
530 South King Street, Room 202
Honolulu, Hawaii 96813

Dear Councilmember Bainum:

Subject: Draft Environmental Assessment
Kalakaua Avenue Bridge Improvements

Thank you for your May 19, 1998, letter to Nagamine Engineers Inc. commenting on the Draft Environmental Assessment for the proposed Kalakaua Avenue Bridge Improvements.

We will inform contractors of our noise and traffic concerns in a pre-bid meeting and during the construction of the project. We will direct the project contractor to obtain a noise permit and abide by the permit conditions. When construction equipment is located on the Ala Wai Canal, we will direct the project contractor to coordinate the work schedule with the water activities to allow safe passage to and from the ocean.

On June 16, 1998, we forwarded copies of the Draft Environmental Assessments to the Waipuna, Villa on Eaton Square, Kalia, Wailana, and Discovery Bay condominium associations, as you commented. We requested submittal of their written comments by June 30, 1998.

Should you have questions concerning the above, please contact Edmund Yoshida at 587-2122.

Very truly yours,

A handwritten signature in cursive script, appearing to read "Kazu Hayashida".

KAZU HAYASHIDA
Director of Transportation

0000 0024 2584



ANDY MURDITTANI
COUNCILMEMBER
(808) 547-7005

CITY COUNCIL

CITY AND COUNTY OF HONOLULU
HONOLULU, HAWAII 96813 TELEPHONE 547-0000

May 26, 1998 '98 MAY 30 P3:35

Mr. Kazu Hayashida, Director
Department of Transportation Services
869 Punchbowl Street
Honolulu, Hawaii 96813

DESIGN DIVISION
HIGHWAY DIVISION
DEPT. OF TRANSPORTATION SERVICES

Re: Proposed Kalakaua Pedestrian Bridge, TMK: 2-3-34

Dear Mr. Hayashida:

As the City Councilmember representing two of the three communities surrounding the site of the State Department of Transportation's proposed pedestrian bridge and sidewalk improvements abutting the existing Kalakaua Avenue Bridge over the Ala Wai Canal, my office has received complaints concerning the present and future proposed use of the pedestrian bridge.

Residents in the McCully-Moilili community object to a number of statements made in the Draft Environmental Assessment (EA). Overall, the EA refers to demographics of the Waikiki area which is classified as a "resort and commercial center with a high density urban environment" and the EA concludes with the finding that the project will have "No Significant Impact". However, little or no reference is made to the demographics of the McCully-Moilili and Ala Moana communities, which are also in close proximity to the proposed pedestrian bridge. These communities contain areas with a high density of condominiums, but unlike Waikiki the condominiums in McCully-Moilili and Ala Moana include a high density of families with school age children and a high density of senior citizens. Additionally, other areas of the McCully-Moilili community are characterized as older neighborhoods consisting of established single family residential homes, again far different than Waikiki and not mentioned in the report.

Residents living in the McCully-Moilili have voiced numerous concerns for preserving the residential nature of their communities, especially since the completion of the State Convention Center development. Residents are concerned that the State Convention Center development will result in expanding the negative impacts occurring in Waikiki with it's high density urban environment and classification as a "resort and commercial center," across the Ala Wai Canal and into their residential communities.

Public apprehension and resident complaints which have been raised since the State's decision to place the new State Convention Center at the old Aloha Motors site, have resulted in several major City planning initiatives. These initiatives, including two which

RECEIVED
MAY 23 1998
DEPT. OF TRANSPORTATION SERVICES

DIRECTOR'S OFFICE
DEPT. OF TRANSPORTATION
MAY 27 10 25 AM '98

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Hayashida letter
May 26, 1998
Page 2

were initiated by office, were not mentioned or referenced in the Kalakaua pedestrian bridge EA, even though these initiatives are ongoing and certain to contain subject matter relevant to the Kalakaua pedestrian bridge and its impact on the surrounding communities. These three initiatives are as follows:

First, pursuant to a measure which I authored to study the various community impacts and enormous speculation and development pressures which are changing the character and integrity of the communities surrounding the State Convention Center development, a Convention Center Task Force has been convened to undertake a Community-Based Regional Master Plan Study for the areas surrounding the State Convention Center and the Ala Wai Canal. This measure was adopted and funded by the City Council and the Convention Center Task Force has met once in December and once in April. It is due to conclude and a report issued by the end of this year.

Second, another initiative which I introduced, requires the establishment of a comprehensive bikeway system and master plan for urban Honolulu extending from Diamond Head, through Waikiki, the University area, downtown, the airport, and the Pearl Harbor military base areas. As required by law, public workshops have been held in each area to assist in creating the Urban Honolulu Bikeway System Master Plan. Under this initiative bicyclists have been made aware of the proposed Kalakaua pedestrian bridge and bicyclists have held meetings specifically to address this issue.

Third, a Primary Urban Center Development Plan Study is being undertaken by the City's Planning Department documenting the intrusion of hotel and other commercial-related activities associated with Waikiki tourism, together with their community impacts into the surrounding residential communities. This planning effort is also ongoing and any findings from the public workshops held to address issues involving the Primary Urban Center, including the subject matter area, have not been issued.

Public input gathered throughout all of the above planning initiatives has centered around resident concerns that the State's Convention Center development will result in expanding major redevelopment of the Ala Wai Canal and potential commercialization of the mauka side of the Ala Wai, including the development of peripheral parking in McCully-Moilili to further support Waikiki. Already, as residents have complained, public safety in the nearby communities has been compromised because of the diversion of police protection, traffic enforcement and other services required to address the needs of the up to 20,000 additional conventioners who will soon be utilizing the facility and its surrounding environs. Residents are equally concerned that the proposed Kalakaua pedestrian bridge will serve to increase crime spreading from Waikiki across the Ala Wai Canal and into the surrounding residential communities and negatively impacting the quality of life in these family neighborhoods.

0000 0024 2586

Hayashida letter
May 26, 1998
Page 3

This shifting of Waikiki's high intensity tourist related crime, including prostitution, drug dealing and thefts, has recently been exemplified by the purse snatchings in Waikiki spreading to Ala Moana and against senior citizens in McCully-Moilili.

Additionally, many complaints have been received from concerned cyclists, many of which are residents in the area and participants in City's Master Bikeway Plan Study, which I initiated. Cyclists are quick to point out that a major design flaw exists in the planning of the Kalakaua pedestrian bridge and the EA only serves to perpetuate this error. As such, the Kalakaua pedestrian bridge is not in compliance with the original purpose to increase "pedestrian safety over the bridge through better separation of the pedestrian/bicycle and vehicular traffic flow." Contrary, the EA is completely silent in addressing concerns raised by cyclists. Cyclists have questioned the proposed 14' width of the pedestrian bridge stating that it is inadequate for both cyclists and pedestrians and ambient users. Additionally, the cyclists have complained that failure to include a bikeway in the planning of the pedestrian bridge is pointless and charge that as a major 'gateway' to Waikiki this bridge creates an ambient first impression that cyclists appreciate more than motorists. The cyclists contend that "massive confusion" will result in failing to adequately plan for an increasing number of bicyclists moving in and out of Waikiki, and that the pedestrian bridge should add to rather than detract from plans to connect the newly constructed Ala Wai Promenade to the bike path system in Waikiki and the Date Street areas.

With respect to these cyclists' concerns, imbedded in the EA is the inappropriate reasoning and response that because the proposed Kalakaua Pedestrian Bridge does not qualify as a "roadway" in accordance with the State's definition of a "bike lane" that "any bicycle facility enhancements" are not being included in the "scope of the work for this project." Further, as additional justification for failing to address the cyclists' concerns the EA determines that the City and not the State or the developer will need to "decide this biking issue," because once the pedestrian bridge is constructed ownership will be turned over to the City.

Unfortunately, the problems being left for the City to decide are far from insubstantial. As one Cyclist complained, the State is creating another "Lanikai Bike Bridge kind of structure" where cyclists will either have to cross from one side of the road and back again or conflict entirely with plans to increase safety. Other cyclists have complained that the State's failure to accommodate both bikes and pedestrians coming from the State's Convention Center will make it impossible for the City to correct the deficiencies created by the State's "flawed planning process" and as a result the Kalakaua pedestrian bridge will never be "bike sensitive" as the original purpose of the EA implies.

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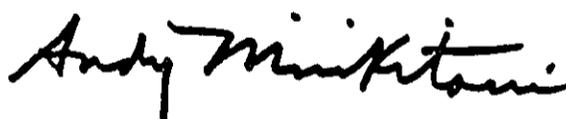
Hayashida letter
May 26, 1998
Page 4

Lastly, we concur with all of the recommendations made by the Outdoor Circle meant to assure the long term health of the "exceptional" Banyan trees located near the proposed construction site. These recommendations are as follows: A certified arborist must be on site whenever work on the trees is being done. Additional assurances must be given by the developer to assure that construction equipment will not be parked under the trees' canopies. Lastly, watering and misting procedures need to be established to keep the trees strong during the stress of construction.

Consequently, in view of the EA's failure to adequately address the three major City planning initiatives which I have mentioned above and the numerous concerns raised by residents concerning the preservation of the residential character of the McCully-Moilili and Ala Moana neighborhoods, and the EA's design failure as expressed by cyclists and the State's failure to accommodate these cyclists concerns in accordance with either the State's or the City's bikeway planning initiatives, the EA referenced above, is totally inadequate.

Thank you for the opportunity to review this document.

Very truly yours,



ANDY MIRIKITANI
Councilmember, District V

AKM:ahr

0000 0024 2588

BENJAMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

JUL 8 1998

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINAAI
GLENN M. OKIMOTO

IN REPLY REFER TO:

HWY-DS
2.9663

The Honorable Andy Mirikitani
Councilmember
City Council
530 South King Street, 2nd Floor
Honolulu, Hawaii 96813

Dear Councilmember Mirikitani:

Subject: Draft Environmental Assessment
Kalakaua Avenue Bridge Improvements

Thank you for your May 26, 1998, letter commenting on the Draft Environmental Assessment for the proposed Kalakaua Avenue Bridge Improvements.

We will amend the Environmental Assessment (EA) to include references to the McCully-Moiliili and Ala Moana communities, in addition to Waikiki, as your letter mentions. The EA will provide general descriptions and demographics of these nearby communities.

We will also further amend the EA to provide information relating to the three major planning initiatives mentioned in your letter. To research those initiatives, we met with City staff from the Planning Department and the Department of Transportation Services.

For the first initiative, regarding the various community impacts related to the Convention Center, City Council Resolution No. 96-267 implemented the formation of a "task force to examine the future impacts of the Convention Center and the redevelopment and renovation of projects on the surrounding communities to determine how to best preserve their residential character." In meeting with the Planning Department, we are informed that a task force has been formed and that a Convention Center Regional Master Plan is being prepared by the consultant firm Belt Collins Hawaii. For purposes of that effort, we will forward our EA to the Planning Department and to Belt Collins Hawaii for reference and inclusion into their work.

For the second initiative, concerning the Urban Honolulu Bikeway System Master Plan, we met with the City Department of Transportation Services to determine the status of the initiative. We are informed that the City's bikeway master plan is being prepared by the consultant firm Helbert, Hastert and Fee. To assist in that effort, we will forward our EA and construction plans to that consultant firm for reference and inclusion into their work.

0000 0024 2589

The Honorable Andy Mirikitani
Page 2

HWY-DS 2.9663

JUL 8 1998

With respect to bicycle uses, Kalakaua Avenue is not presently designated as a bicycle facility. The proposed pedestrian bridge does not preclude the implementation of a bicycle facility along Kalakaua Avenue, should the City decide such designation.

Travel upon the pedestrian bridge will be subject to City ordinances similar to other sidewalks in the Waikiki Special District and will be limited to pedestrian traffic including wheelchairs and any others allowed under the ordinances. References within the EA that the proposed bridge will combine pedestrian and bicycle uses will be removed.

For the third initiative, regarding the Primary Urban Center Development Plan, we are informed that the Planning Department and two consultant firms, Plan Pacific and Helbert, Hastert and Fee, are conducting workshops to develop a Honolulu 2020 vision and policy. The Planning Department has briefed us on the progress of these workshops and we will make reference to this initiative in our EA.

We appreciate your in-depth commentary and critique of the proposed project. In proceeding with this project, we will communicate with City agencies and their consultants with respect to the three planning initiatives you have cited. We will also continue communications with the public through contact with the neighborhood boards and affected community organizations.

Should you have questions concerning the above, please contact Edmund Yoshida at 587-2122.

Very truly yours,



KAZU HAYASHIDA
Director of Transportation

c: Office of Environmental Quality Control
Cheryl D. Soon, Dept of Transportation Svcs

0000 0024 2590

BOARD OF WATER SUPPLY

CITY AND COUNTY OF HONOLULU
SOUTH BERETANIA STREET
HONOLULU, HAWAII 96843
PHONE (808) 527-6180
FAX (808) 533-2714

MAY 28 11 54 AM '98
DEPT. OF TRANSPORTATION
HIGHWAYS DIVISION
MAY 29 10 08 AM '98

JEREMY HARRIS, Mayor

WALTER O. WATSON, JR., Chairman
EDDIE FLORES, JR.
KAZU HAYASHIDA
JAN M. L. Y. AMII
FORREST C. MURPHY
JONATHAN K. SHIMADA, PhD
BARBARA KIM STANTON

BROOKS H. M. YUEN, Acting
Manager and Chief Engineer

Mr. Kazu Hayashida, Director
Department of Transportation
State of Hawaii
869 Punchbowl Street
Honolulu, Hawaii 96813

Dear Mr. Hayashida:

Subject: Draft Environmental Assessment for the Kalakaua Avenue Bridge Improvements,
Waikiki, Oahu, TMK: 2-3-34

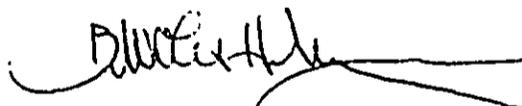
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98 MAY 29 A6:48
DEPT. OF TRANSPORTATION
HIGHWAYS DIVISION

Thank you for the opportunity to review and comment on the Draft Environmental Assessment (EA) for the proposed pedestrian bridge project.

We have no objections to the proposed project. The draft EA and construction plans that were submitted for our review have adequately addressed our concerns.

If you have any questions, please contact Barry Usagawa at 527-5235.

Very truly yours,



BROOKS H. M. YUEN
Acting Manager and Chief Engineer

cc: Office of Environmental Quality Control
Nagamine Engineers, Inc.

DIRECTOR'S OFFICE
DEPT. OF TRANSPORTATION
MAY 29 10 49 AM '98

0000 0024 2591

DEPARTMENT OF LAND UTILIZATION
CITY AND COUNTY OF HONOLULU

650 SOUTH KING STREET, 7TH FLOOR • HONOLULU, HAWAII 96813
PHONE: (808) 523-4414 • FAX: (808) 527-6743

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DEPT. OF TRANSPORTATION
HIGHWAY DIVISION



JAN NAOE SULLIVAN
DIRECTOR

LORETTA K.C. CHEE
DEPUTY DIRECTOR

98-02821 (ST)

'98 EA Comments Zone 2

May 26, 1998

EMY HARRIS
MAYOR

DEPT. OF TRANSPORTATION
HIGHWAY DIVISION

98 MAY 29 A6:50

RECEIVED

Mr. Kazu Hayashida, Director
Department of Transportation
State of Hawaii
869 Punchbowl Street
Honolulu, Hawaii 96813

Dear Mr. Hayashida:

Draft Environmental Assessment (DEA):
Kalakaua Bridge Improvements, Waikiki, Oahu
Tax Map Key: 2-3-34

We have reviewed DEA for the above-referenced project transmitted by your letter dated April 15, 1998, and have no additional comments to offer beyond those made previously during the early assessment consultation.

We have no other comments to offer at this time.

Thank you for the opportunity to comment on this matter. Should you have any questions, please contact Steve Tagawa of our staff at 523-4817.

Very truly yours,

Loretta Chee
JAN NAOE SULLIVAN
Director of Land Utilization

JNS:am

cc: Gary Gill, Office of Environmental
Quality Control

g:\ppd\DEApedbr.sht

DIRECTOR'S OFFICE
DEPT. OF
TRANSPORTATION
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PLANNING DEPARTMENT
CITY AND COUNTY OF HONOLULU

650 SOUTH KING STREET, 8TH FLOOR • HONOLULU, HAWAII 96813-3017
PHONE: (808) 523-4533 • FAX: (808) 523-4950

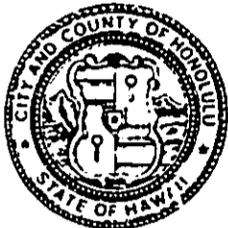
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DIRECTOR'S OFFICE
DEPT. OF
TRANSPORTATION

MAY 22 10 32 AM '98

JEREMY HARRIS
MAYOR

DEPT.



PATRICK T. ONISHI
CHIEF PLANNING OFFICER

DONAL HANAIKE
DEPUTY CHIEF PLANNING OFFICER

TH 4/98-0846

May 18, 1998

Honorable Kazu Hayashida, Director
Department of Transportation
State of Hawaii
869 Punchbowl Street
Honolulu, Hawaii 96813

RECEIVED
MAY 22 1 23 PM '98
DEPT. OF TRANSPORTATION
HIGHWAYS DIVISION

Dear Mr. Hayashida:

Draft Environmental Assessment (DEA) for the
Kalakaua Avenue Bridge Improvements
Waikiki, Oahu, Hawaii, TMK: 2-3-34

We have reviewed the subject DEA and have no further comments to add at this time.

Thank you for the opportunity to comment on this matter. Should you have any questions, please contact Tim Hata of our staff at 527-6070.

Yours very truly,

PATRICK T. ONISHI
Chief Planning Officer

PTO:ft

c: OEQC

0000 0024 2593

DRAFT EA COMMENTS & RESPONSES

STATE OF HAWAII AGENCIES

0000 0024 2594



**DEPARTMENT OF BUSINESS,
ECONOMIC DEVELOPMENT & TOURISM**

OFFICE OF PLANNING

235 South Beretania Street, 6th Flr., Honolulu, Hawaii 96813
Mailing Address: P.O. Bpx 2359, Honolulu, Hawaii 96804

Ref. No. P-7410

April 29, 1998

BENJAMIN J. CAYETANO
GOVERNOR
SEIJI F. NAYA
DIRECTOR
BRADLEY J. MOSSMAN
DEPUTY DIRECTOR
RICK EGGED
DIRECTOR, OFFICE OF PLANNING

Tel.: (808) 587-2846
Fax: (808) 587-2824

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MAY 12 3 28 PM '98
DEPT. OF TRANSPORTATION
HIGHWAYS DIVISION

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HIGHER
DEPT. OF TRANSPORTATION

MEMORANDUM

TO: Kazu Hayashida, Director
Department of Transportation

FROM: Rick Egged *[Signature]*
Director, Office of Planning

SUBJECT: Draft Environmental Assessment, Kalakaua Avenue Bridge Improvements,
TMK 2-3-34, Waikiki, Oahu, Hawaii

We appreciate your department's consideration of our Coastal Nonpoint Pollution Control Program Management Plan in the formulation of mitigation measures to minimize coastal water quality degradation. In addition, as provided in the Office of Environmental Quality Control's administrative rules, an assessment of the project's consistency with the Coastal Zone Management (CZM) objectives and policies should be included in the environmental assessment.

If you have questions, please contact Steve Olive of our Coastal Zone Management Program at 587-2877.

cc: Gary Gill, OEQC

DIRECTOR'S OFFICE
DEPT. OF
TRANSPORTATION
MAY 12 10 33 AM '98

0000 0024 2595

ENRIQUE J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINAAI
GLENN M. OKIMOTO

IN REPLY REFER TO:

HWY-DS
2.9456

May 28, 1998

TO: RICK EGGED
DIRECTOR, OFFICE OF PLANNING

FROM: KAZU HAYASHIDA *K.H.*
DIRECTOR OF TRANSPORTATION

SUBJECT: DRAFT ENVIRONMENTAL ASSESSMENT
KALAKAUA AVENUE BRIDGE IMPROVEMENTS
PROJECT NO. HWY-O-13-98

Thank you for your April 29, 1998, memorandum commenting on the Draft Environmental Assessment for the proposed Kalakaua Avenue Bridge Improvements.

In response to your comments, we will include a copy of our March 27, 1998, Coastal Zone Management Federal Consistency Determination submittal within an appendix of the Final Environmental Assessment.

Should you have any questions, please call Edmund Yoshida at 587-2122.

c: Office of Environmental Quality Control

0000 0024 2596

BENJAMIN J. CAYETANO
GOVERNOR OF HAWAII



STATE OF HAWAII
DEPARTMENT OF HEALTH
P.O. BOX 3378
HONOLULU, HAWAII 96801

LAWRENCE MIKE
DIRECTOR OF HEALTH

In reply, please refer to

June 23, 1998

98-090/epo

RECEIVED
JUN 27 1998

Mr. Norman Nagamine, President
Nagamine Engineers, Inc.
Pauahi Tower, Suite 725
1001 Bishop Street
Honolulu, Hawaii 96813

NAGAMINE ENGINEERS INC.

Dear Mr. Nagamine:

Subject: Draft Environmental Assessment
Kalakaua Avenue Bridge Improvements
Waikiki, Oahu
TMK: 2-3-34

Thank you for allowing us to review and comment on the subject project. We have the following comments to offer:

Noise Concerns

1. Activities associated with the construction phase of the project must also comply with the Department of Health's Administrative Rules, Chapter 11-46, "Community Noise Control."
 - a. The contractor must obtain a noise permit if the noise levels from the construction activities are expected to exceed the allowable levels of the rules as stated in Section 11-46-6(a).
 - b. Construction equipment and on-site vehicles requiring an exhaust of gas or air must be equipped with mufflers as stated in Section 11-46-6(b)(1)(A).
 - c. The contractor must comply with the requirements pertaining to construction activities as specified in the rules and the conditions issued with the permit as stated in Section 11-46-7(d)(4).

0000 0024 2597

Mr. Norman Nagamine
June 23, 1998
Page 2

98-090/epo

Should there be any questions on this matter, please call
Mr. Jerry Haruno, Environmental Health Program Manager of the
Noise, Radiation and Indoor Air Quality Branch at 586-4701.

Sincerely,



BRUCE S. ANDERSON, Ph.D.
Deputy Director for
Environmental Health

c: NR&IAQB
DOT
OEQC

0000 0024 2598

JAMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINAII
GLENN M. OKIMOTO

IN REPLY REFER TO:

HWY-DS
2.0140

July 21, 1998

TO: BRUCE S. ANDERSON, DEPUTY DIRECTOR
DEPARTMENT OF HEALTH

FROM: KAZU HAYASHIDA, DIRECTOR *← K*
DEPARTMENT OF TRANSPORTATION

SUBJECT: DRAFT ENVIRONMENTAL ASSESSMENT
KALAKAUA AVENUE BRIDGE IMPROVEMENTS
PROJECT NO. HWY-O-13-98

Thank you for your June 23, 1998, letter to Nagamine Engineers, Inc. commenting on the Draft Environmental Assessment for the proposed Kalakaua Avenue Bridge Improvements.

We have consulted with the Department of Health's Noise, Radiation and Indoor Air Quality Branch regarding the requirements of Title 11, Hawaii Administrative Rules, Chapter 46, entitled "Community Noise Control." Since we anticipate that the noise due to the proposed project and construction activities will exceed the allowable noise levels of the above rules, we are applying for a noise permit in advance of actual construction. Our advance application for a noise permit and its conditional issuance by the DOH will assist us to better define, in our construction plans and specifications and to potential contractors bidding on the project, the requirements and restrictions applicable to noise. We have discussed this advance processing of the noise permit with the staff of the Noise, Radiation and Indoor Air Quality Branch and have received their concurrence in this method.

After the construction bidding is completed and we have determined a project contractor, we will direct that contractor to comply with the noise permit requirements. We will also direct the contractor to provide supplemental information to the DOH, such as the contractor's contact person and phone number for community coordination, the specific list of equipment to be utilized, the plan for attenuation of noise emissions, and any other information conditioned or required by the DOH for the noise permit.

Please direct questions regarding the above to Edmund Yoshida at 587-2122.

0000 0024 2599



STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES

P.O. BOX 621
HONOLULU, HAWAII 96809

May 15, 1998

AQUACULTURE DEVELOPMENT
PROGRAM
AQUATIC RESOURCES
BOATING AND OCEAN RECREATION
CONSERVATION AND
RESOURCES ENFORCEMENT
CONVEYANCES
FORESTRY AND WILDLIFE
HISTORIC PRESERVATION
LAND DIVISION
STATE PARKS
WATER RESOURCE MANAGEMENT

LD-NAV

Ref.: KALAKAUA.RCM

Mr. Norman Nagamine, President
Nagamine Engineers, Inc.
Consulting Structural Engineers
1001 Bishop Street, Suite 725
Honolulu, Hawaii 96813

Dear Mr. Norman Nagamine:

SUBJECT: Review : Draft Environmental Assessment
Project : Kalakaua Avenue Bridge Improvements
Proposal: Construct a 180-foot long pedestrian bridge
Location: Waikiki, Island of Oahu, Hawaii
TMK : 1st/ 2-3-34

Thank you for the opportunity to review and comment on the subject matter.

Our Commission on Water Resource Management has no corrections or additions at this time. The Department of Public Works acknowledges that a stream channel alteration permit (HRS Chapter 174C-71) is required for the proposed project (page 5 of the DEA).

Attached herewith is our Division of Aquatic Resources' comments on the subject matter. Should you have any questions, please feel free to contact Nicholas A. Vaccaro of our Land Division Support Services Branch at 587-0438.

HAWAII: Earth's best!

Aloha,

A handwritten signature in cursive script, reading "Michael D. Wilson".

MICHAEL D. WILSON

c: Oahu Land Board Member
Oahu District Land Agent

0000 0024 2600

May 8, 1998

TO: William Devick, Acting Administrator *MD*
FROM: Jo-Anne N. Kushima, Aquatic Biologist
SUBJECT: Draft Environmental Assessment
Kalakaua Avenue Bridge Improvements
Ref.: KALAKAUA.COM

Comment

Requested by: Dean Y. Uchida, Administrator

Date of Request: 04-28-98

Date Received: 04-29-98

Summary of Proposed Project:

Title: Kalakaua Avenue Bridge Improvements

Project by: Akinaka & Associates, Ltd. and
Nagamine Engineers, Inc.

Location: Waikiki, Island of Oahu, Hawaii
TMK: 1st/2-3-34

Brief Description:

The State Department of Transportation, Highways Division is proposing to construct a 14 foot wide, 180-foot long pedestrian bridge and sidewalk improvements. The purpose of the pedestrian bridge is to 1) increase pedestrian safety over the bridge by separating the pedestrian/bicycle and the vehicle traffic flow, 2) reconstruct the existing sidewalk at the Waikiki end of the bridge to conform to the American Disabilities Act (ADA), and 3) improve the bridge, which is designated as one of the five "gateways" to Waikiki, by coordinating open space, landscaping and architectural features so that it promotes a sense of arrival to a special place.

The new bridge will abutt the existing Kalakaua Bridge between Kapiolani and Ala Wai Boulevards, spanning the Ala Wai Canal. The Ala Wai Canal has a long standing poor water quality rating which is the result of urbanization of the contributing watersheds of Makiki, Manoa, St. Louis Heights, Palolo, Moiliili, Kapahulu and parts of Diamond Head. The canal connects to the ocean and is subject to the diurnal tidal influence, but very little flushing takes place in the upper reaches of the canal.

The applicant states that based on a thorough evaluation of the affected environment, project impacts, mitigative measures and alternatives considered in this project's Draft EA document, a Finding of No Significant Impact (FONSI) is anticipated and an environmental impact statement (EIS) will not be required.

0000 0024 2601

The applicant also reports that marine resources in the vicinity of the bridge include tilapia, mullet, barracuda and various bottom dwelling invertebrates and crustaceans typical of an urbanized marine setting. The continuous sediment loading and poor water quality in the area of the proposed project are unsuitable environments for coral colonies and the pile driving or pier construction work are not expected to impact any coral colony habitats. The applicant goes on to cite the U.S. Fish and Wildlife Service as stating that there are no rare, threatened or endangered species of flora or fauna on the proposed project site.

Comments:

The applicant is relying on mitigation measures to address concerns centered around construction impacts will be identified in the permitting and approval conditions that they expect to be imposed on the project before groundbreaking occurs.

The applicant reports that marine resources in the vicinity of the present bridge include tilapia, mullet, barracuda, and various bottom dwelling invertebrates and crustaceans. DAR requests the applicant to explain what method was used to obtain the inventory of fish in the vicinity of the present bridge. Additionally, the applicant is requested to provide a detailed list of the invertebrates and crustaceans that was observed in the area.

Tilapia, mullet and barracuda are common species, and not on the endangered species list, however, as the agency responsible for marine resources DAR requests the applicant to provide details of construction methods and the mitigating measures to be employed to minimize adverse effects to the environment and marine resources in the final environmental assessment.

0000 0024 2602

BENJAMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

July 21, 1998

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINAII
GLENN M. OKIMOTO

IN REPLY REFER TO:

HWY-DS
2.9929

TO: MICHAEL D. WILSON, DIRECTOR
DEPARTMENT OF LAND AND NATURAL RESOURCES

FROM: KAZU HAYASHIDA, DIRECTOR *K.H.*
DEPARTMENT OF TRANSPORTATION

SUBJECT: DRAFT ENVIRONMENTAL ASSESSMENT
KALAKAUA AVENUE BRIDGE IMPROVEMENTS
PROJECT NO. HWY-O-13-98

Thank you for your May 15, 1998, letter to Nagamine Engineers, Inc. commenting on the Draft Environmental Assessment for the proposed Kalakaua Avenue Bridge Improvements.

Regarding the Division of Aquatic Resources' comments, references to the types of fish, invertebrates and crustaceans were obtained from the "Ecological Studies of the Biota of the Ala Wai Canal" authored by Jacquelin N. Miller of the University of Hawaii Department of Oceanography and published in the Hawaii Institute of Marine Biology Technical Report Number 32, dated March 1975.

For this project, twelve site visits and visual observations of marine life within the Ala Wai Canal near the existing Kalakaua Avenue Bridge were made between November 1997 and June 1998. Tilapia was observed to be the most prevalent fish population on every occasion. Mullet, juvenile barracuda, surgeonfish, manini, and sailfin tang were seen singularly or in small groups on most occasions. No attempts were made to inventory the crustaceans and invertebrates in the area.

Should you have questions concerning the above, please call Edmund Yoshida at 587-2122.

0000 0024 2603

BENJAMIN J. CAYETANO
GOVERNOR



GARY GILL
DIRECTOR

STATE OF HAWAII
OFFICE OF ENVIRONMENTAL QUALITY CONTROL

235 SOUTH BERETANIA STREET
SUITE 702
HONOLULU, HAWAII 96813
TELEPHONE (808) 586-4186
FACSIMILE (808) 586-4186

May 26, 1998

Mr. Kazu Hayashida, Director
State Department of Transportation
869 Punchbowl Street
Honolulu, Hawaii 96813

Dear Mr. Hayashida:

Subject: Draft Environmental Assessment for Kalakaua Avenue Bridge
Improvements, Oahu

Thank you for the opportunity to review the above project. We have
the following questions and comments.

1. Please clearly state whether any of the "exceptional" banyan trees in the vicinity of the site will be affected by the project. If so, describe the extent of the impact and disclose the mitigation measures to reduce adverse effects.
2. Pile driving and drilling activities will create noise impacts. There are many noise sensitive land uses (such as housing units) in the vicinity of the project. Please describe the anticipated noise levels and its duration. Discuss methods to reduce the noise impacts.
3. The project would require construction staging areas and launching facilities for water-bound equipment. Please disclose the areas that may be affected and describe if any recreational activity may be curtailed as a result.
4. It is now illegal to ride a bike on Waikiki sidewalks. Please clearly state whether bicyclists will be allowed on the new "pedestrian" bridge. Show plans for the proposed bike route across the Ala Wai Canal at this location and describe how cyclists would continue down Kalakaua toward Diamond Head after crossing the bridge. Any bike paths or routes heading mauka over the bridge should also be described.

0000 0024 2604

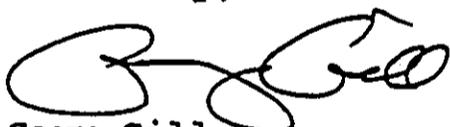
Mr. Hayashida

Page 2

5. The State Department of Land and Natural Resources is planning to dredge the Ala Wai Canal. Please coordinate all aspects this project (planning, design, construction, etc.) with DLNR.
6. The existing makai rail wall of the bridge will be demolished and replaced by a metal railing. Any alteration of the historic structure and mitigation measures to preserve its features should be clearly described in the document.
7. The existing utility poles on the bridge will be removed. How will these utilites then cross the bridge? Will they be placed under the roadway surface? Making the transition from overhead to subsurface and back to overhead would be costly and could be unsightly. Please describe in full the plans for utilites crossing the bridge.
8. Please discuss the findings and reasons for supporting the FONSI determination based on all 13 significant criteria listed in §11-200-12 of the EIS rules. Please see the enclosed example.

Should you have any questions, please call Jeyan Thirugnanam at 586-4185. Mahalo.

Sincerely,



Gary Gill
Director

c: Nagamine Engineers

0000 0024 2605

BENJAMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

July 13, 1998

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINAAI
GLENN M. OKIMOTO

IN REPLY REFER TO:

HWY-DS
2.9664

Mr. Gary Gill
Director
Office of Environmental Quality Control
235 South Beretania Street, Suite 702
Honolulu, Hawaii 96813

Dear Mr. Gill:

Subject: Draft Environmental Assessment
Kalakaua Avenue Bridge Improvements

Thank you for your May 26, 1998, letter commenting on the Draft Environmental Assessment for the proposed Kalakaua Avenue Bridge Improvements. The following items respond to your questions and comments and are numbered similarly as in your letter:

1. At least two of the "exceptional" banyan trees along the Ala Wai Boulevard and one along the Ala Wai Promenade will be affected by the proposed construction. To provide overhead clearance for construction machinery and to install underground electrical conduits for new pedestrian lighting, the canopies of these three trees nearest the work area will be selectively trimmed and the roots of one of them will be pruned. We consulted with the City's Arborist Advisory Committee and the Outdoor Circle to develop appropriate mitigative measures and collectively reached the following understanding:
 - Selective trimming of canopies and root pruning are allowable and shall be performed under the guidance of a certified arborist. Plans detailing the trimming and pruning shall be submitted to the Arborist Advisory Committee for approval.
 - To protect against inadvertent damage due to construction activities, barricades shall be installed surrounding the affected tree root systems and steel plates with mulch underlay shall be installed over affected tree root areas not surrounded by the barricades.
 - The construction contractors shall be notified during any pre-bid and pre-construction meetings of the requirements and obligations to protect the trees.

Should other trees besides the three identified above be affected, we will again consult with the Arborist Advisory Committee to develop appropriate treatment measures.

0000 0024 2606

Mr. Gary Gill
Page 2

HWY-DS 2.9664

2. We estimate that the noise due to pile driving and drilling will range between 110 and 130 decibels, will be intermittent during working hours depending on the progress of the work, and will have a duration of approximately two months. The noise due to construction activities other than pile driving and drilling is estimated to range between 70 and 110 decibels and will be prevalent during working hours for the approximate ten months duration of the project.

The contractor will be required to obtain a noise permit from the State Department of Health and to abide by the permit conditions. The contractor will be directed to communicate with the nearby residents to inform them of the work schedule and to implement prudent measures to minimize noise levels of the construction activities.

3. Anticipated construction staging areas include the parking lane on Ala Wai Boulevard, from the Kalakaua Avenue intersection to the Lipeepe Street intersection, and the Ala Wai Boulevard cul-de-sac near the Ala Moana Boulevard Bridge. Possible launching sites for water-bound equipment include the Ala Wai Boulevard cul-de-sac, the Ala Wai Boulevard parking lane, and the Ala Wai Yacht Harbor. Other staging areas or launching facilities may be selected by the contractor depending on construction methods.

There should be minimal curtailing of water recreational activities as a result of the project construction. We have consulted with water sports and recreational activities groups to discuss impacts to their activities. These groups informed us that passage through the construction area to the ocean and other parts of the Ala Wai Canal for training and competition is necessary for canoe paddlers, kayakers, and other water activities. If such passage is allowed, then the water activities should be minimally curtailed. The contractor will be directed to allow passage through the construction area by the water activities, except during times when overhead lifting poses safety hazards. For these times of hazardous overhead lifting, the contractor will be directed to coordinate the work schedule and communicate in advance with the groups to minimize impacts to the water activities.

4. We acknowledge that City ordinance does not allow bicycle riding on sidewalks within the Waikiki Special District. The proposed pedestrian bridge is located within the Waikiki Special District. The following further responds concerning bicycles:
 - Bicycle riding will not be allowed upon the pedestrian bridge, in conformance with the City ordinance.
 - We are not proposing nor preparing plans for a bike route across the Ala Wai Canal.
 - The existing traffic lanes are available to cyclists for riding across the Kalakaua Avenue Bridge in either direction.
5. We are aware of the proposed State DLNR project to dredge the Ala Wai Canal and will coordinate our efforts with that project.

0000 0024 2607

Mr. Gary Gill
Page 3

HWY-DS 2.9664

6. Your comment that the existing makai rail wall will be demolished and replaced by a metal railing is not accurate. The majority of that existing 160 feet long wall will be preserved. The approximate 20 feet long curved portion of that wall, at the Waikiki end of the bridge, will be removed to provide passage onto the pedestrian bridge. A description of this alteration to the historic structure will be provided in the Environmental Assessment.
7. The two existing wooden utility poles on the bridge will not be removed under this project, and the utilities attached to those poles will remain overhead crossing the bridge.

This project does not include improvements to the existing utilities crossing the Kalakaua Avenue Bridge. However, the design of the pedestrian bridge does include an eight-foot wide utilities chase, interior to the pedestrian bridge structure, to accommodate the future installation or relocation of utilities crossing the Ala Wai Canal.

We have discussed the utilities crossing the existing Kalakaua Avenue Bridge with the City and County of Honolulu. The focus of our discussions has been whether this project should remove the City utilities from within the existing bridge and relocate them into the pedestrian bridge utilities chase. These particular utilities are the 12-inch water main located beneath the existing makai sidewalk and the three 10-inch sewer mains beneath the mauka sidewalk.

Significant issues concerning these utilities are the reconfiguration of traffic lanes crossing the bridge and the impact to the historic character of the bridge structure. Upon removing these utilities from beneath the concrete sidewalks and relocating them into the proposed utilities chase, the existing mauka and makai sidewalks can be narrowed to result in an additional 10 feet of pavement width usable for traffic lanes. This resultant additional pavement width may be used to either widen the existing lanes or to add another lane. However, the narrowing of the concrete sidewalks will impact the historic character of the existing structure and will require the consent of the DLNR Historic Preservation Division and the Advisory Council on Historic Preservation.

The DLNR Historic Preservation Division has been consulted regarding the sidewalk narrowing. In general, the DLNR has stated that the impact to the historic structure is not warranted at this time because the lane widening or lane addition upon the bridge does not connect to any new lane widening or addition within the adjacent sections of Kalakaua Avenue. Further evaluation of this impact would be appropriate at such future time that the City constructs lane improvements along the connecting sections of Kalakaua Avenue.

In summary, improvement plans for utilities crossing the bridge, inclusive of electric and telephone utilities, are not included within this project. If future utilities work is proposed, then that work should be coordinated through the City with respect to City plans for lane improvements on Kalakaua Avenue and should also be evaluated for impacts to the historic character of the Kalakaua Avenue Bridge.

0000 0024 2608

Mr. Gary Gill
Page 4

HWY-DS 2.9664

8. We acknowledge your comment regarding the 13 significance criteria cited in the EIS rules. The EA will be amended to present more detailed discussion of our findings and reasons for the FONSI determination. Our findings and reasons will be presented in a manner similar to the example you have provided.

Your review and comments for the Draft EA have been very helpful to us. We appreciate your assistance in this effort and will maintain communication with your office as we proceed toward submittal of the Final EA.

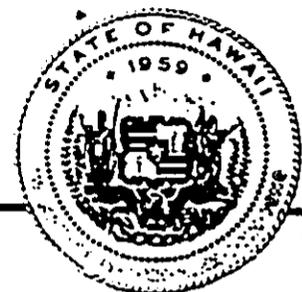
Should you wish to discuss any of the above items, please contact Edmund Yoshida at 587-2122.

Very truly yours,



KAZU HAYASHIDA
Director of Transportation

0000 0024 2609



Convention Center Authority

DIRECTOR'S OFFICE
DEPT. OF
TRANSPORTATION

1833 KALAKAUA AVENUE, SUITE 800 HONOLULU, HAWAII 96813
TELEPHONE: (808) 973-9790 FAX: (808) 973-9795

MAY 21 10 19 AM '98

DEPT. OF TRANSPORTATION
HIGHWAYS DIVISION

RECEIVED
MAY 21 1 52 PM '98
DEPT. OF TRANSPORTATION
HIGHWAYS DIVISION

May 18, 1998

Mr. Kazu Hayashida
Director
Department of Transportation
869 Punchbowl Street
Honolulu, Hawaii 96813

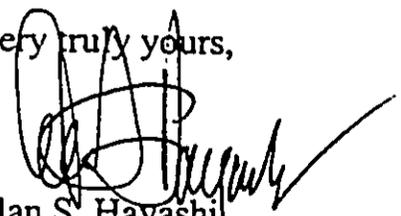
Dear Kazu:

Subject: Kalakaua Avenue Bridge Improvements -- Plans by Nagamine Engineers, Inc.

Thank you for the opportunity to comment on the plan for the widening of the Kalakaua Bridge. This project will enable the pedestrian traffic going to and from the Hawai'i Convention Center to better and more efficiently access their destinations. The existing Kalakaua Avenue Makai sidewalk is 6 feet wide and does not have a barrier between pedestrian and vehicular traffic. The widening will allow for a sidewalk of 10 feet and a physical barrier between pedestrian and vehicular traffic. The widening will also allow for future expansion of vehicular traffic capability on the bridge should the City DTS desire to do so in the future.

The CCA strongly supports the current State initiative to widen the Kalakaua Bridge for pedestrian traffic. We would be glad to submit additional supporting comments, should you feel it appropriate and necessary.

Very truly yours,


Alan S. Hayashi
Executive Director

ASH/lg:3302.KALBRIDG4

c: Brian Minaai, DOT
Gary Gill, OEQC

0000 0024 26 10

DRAFT EA COMMENTS & RESPONSES

FEDERAL GOVERNMENT AGENCIES

0000 0024 26 11



DEPARTMENT OF THE ARMY
U. S. ARMY ENGINEER DISTRICT, HONOLULU
FORT SHAFTER, HAWAII 96858-5440

REPLY TO
ATTENTION OF

April 24, 1998

Civil Works Branch

Mr. Norman Nagamine, President
Nagamine Engineers Incorporated
1001 Bishop Street, Suite 725
Honolulu, Hawaii 96813

Dear Mr. Nagamine:

Thank you for the opportunity to review and comment on the Draft Environmental Assessment for the Kalakaua Bridge Project, Waikiki, Oahu (Tax Map Key 2-3-34). We do not have any additional comments to offer beyond those previously provided in our letter dated December 1, 1995.

Sincerely,

A handwritten signature in cursive script, appearing to read "Paul Mizue".

Paul Mizue, P.E.
Chief, Civil Works Branch

0000 0024 26 12

DRAFT EA COMMENTS & RESPONSES

UTILITY COMPANIES

0000 0024 26 13

Hawaiian Electric Company, Inc. • PO Box 2750 • Honolulu, HI 96840-0001

HWY-DS/EX

MAY 29 10 30 AM '98
DEPT. OF TRANSPORTATION
HIGHWAYS DIV.

DIRECTOR'S OFFICE
DEPT. OF
TRANSPORTATION
MAY 27 10 30 AM '98



Scott W.H. Seu, P.E.
Manager
Environmental Department

May 26, 1998

Mr. Kazu Hayashida, Director
Department of Transportation
869 Punchbowl Street
Honolulu, HI 96813

DEPT. OF TRANSPORTATION
HIGHWAYS DIV.

'98 MAY 30 P 3:35

RECEIVED

Dear: Mr. Hayashida

Subject: Kalakaua Avenue Bridge Improvements

Thank you for the opportunity to comment on your April 1998 Draft EA for the Kalakaua Avenue Bridge Improvements, as proposed by the Department of Transportation, State of Hawaii. We have reviewed the subject document and have no comments at this time.

HECO shall reserve further comments pertaining to the protection of existing powerlines bordering the project area until construction plans are finalized. Again, thank you for the opportunity to comment on this draft environmental assessment.

Sincerely,

cc:
Mr. Gary Gill, OEQC

WINNER OF THE EDISON AWARD
FOR DISTINGUISHED INDUSTRY LEADERSHIP



0000 0024 26 14

GTE Hawaiian Tel

GTE Hawaiian Telephone Company Incorporated
P.O. Box 2200 • Honolulu, HI 96841 • 808 546-4511

Beyond the call

May 26, 1998

Mr. Norman Nagamine
Nagamine Engineers Inc.
Pauahi Tower, Suite 725
1001 Bishop Street
Honolulu, Hawaii 96813

Dear Mr. Nagamine:

This is in response to your letter dated April 15, 1998 regarding the Draft Environmental Assessment for the Kalakaua Pedestrian Bridge project.

I am disappointed that the current project does not provide provisions to allow GTE Hawaiian Tel the opportunity to include the installation of our conduit system which was not included in the City's plan to replace the sidewalk on the existing bridge. During the planning of the City's Sidewalk Replacement project we were informed by Dr. Jonathon Shimada that State of Hawaii had appropriated monies to design and construct a widening of the bridge and our project should be included in it to preclude the possibility of having to relocate our newly installed conduit system. Your project as it is currently proposed does not afford us the opportunity to include our conduit requirements in its scope. While it may be too late to include our facilities in your project may we have the opportunity to review your design to insure that its alignment allows us to construct our future conduit system tying our existing vaults, presently located on both sides of the bridge, without having to affect your newly constructed pedestrian bridge.

If you have any questions, please call me at 840-5822.

Sincerely,


Calvin C. W. Choy
Senior Engineer - Support Structures

0000 0024 26 15

BENJAMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

JUL 8 1998

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
BRIAN K. MINAII
GLENN M. OKIMOTO

IN REPLY REFER TO:

HWY-DS
2.9930

Mr. Calvin C.W. Choy
Senior Engineer - Support Structures
GTE Hawaiian Telephone Company Inc.
P.O. Box 2200
Honolulu, Hawaii 96841

Dear Mr. Choy:

Subject: Draft Environmental Assessment
Kalakaua Avenue Bridge Improvements

Thank you for your May 26, 1998, letter to Nagamine Engineers Inc. concerning the Draft Environmental Assessment for the proposed Kalakaua Avenue Bridge Improvements.

Installation of the GTE Hawaiian Telephone Company's conduit system was not intended to be a part of the proposed pedestrian bridge project. In communicating with the City and County of Honolulu, including Dr. Jonathan Shimada, we stated that this project is the construction of the pedestrian bridge structure and does not include improvements to existing utilities.

Since Kalakaua Avenue is a City right-of-way and telephone utilities work may impact other existing underground utilities within Kalakaua Avenue, we request that you coordinate your work through the City. We will forward our plans to you for your reference, however, please refer matters concerning work within Kalakaua Avenue to the City and County of Honolulu.

Please direct questions regarding the above to Edmund Yoshida at 587-2122.

Very truly yours,

A handwritten signature in cursive script, appearing to read "Kazu Hayashida".

KAZU HAYASHIDA
Director of Transportation

0000 0024 26 16

DRAFT EA COMMENTS & RESPONSES

COMMUNITY ORGANIZATIONS

0000 0024 26 17



THE OUTDOOR CIRCLE

1314 South King St., Suite 306 • Honolulu, HI 96814
Phone: 808-593-0300 Fax: 808-593-0525

Established 1912
Non-profit Organization

ANCHES
TU
Kaneohe
Lani-Kailua
North Shore
Waialae Kahala
WAI
Hilo
Ka'u
Kona
Puna
Waimea
AI
OKAI
DEN CIRCLE
ani-Kai

May 4, 1998

Mr. Kazu Hayashida, Director
Department of Transportation
869 Punchbowl Street
Honolulu, HI 96813

RE: Draft Environmental Assessment, Kalakaua Avenue Bridge Improvements
Waikiki, Oahu, Hawai'i, TMK 2-3-34

Dear Mr. Hayashida:

Thank you for the opportunity to comment on the above referenced Draft Environmental Impact Assessment. Having planted the double row of Exceptional banyan trees along the Ala Wai Canal in 1936, The Outdoor Circle is especially concerned with their well being.

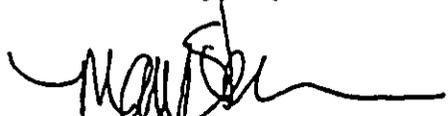
Before the issuance of the DEA, assurances were made to the Mayor's Arborist Advisory Committee by Mr. Nagamine, the consulting engineer, that these trees would not be removed nor relocated during construction. We were told that only one Exceptional banyan tree might be impacted by the work, and on two separate occasions we had lengthy discussions with him on how to mitigate the potential negative impacts on the tree.

Therefore, we were extremely surprised when the Draft Environmental Assessment stated, "should the State require that one of these trees to be altered or removed, the State will consult with the City's Exceptional Tree Committee..." Never before had this been discussed and we would strongly protest the removal of any Exceptional trees. The Final Environmental Assessment should correct this statement to say that, "no Exceptional trees will be removed as a result of this project."

In addition, our response to the early assessment requested that a commitment be made and that details be provided on the protection of all of these trees during construction. This has not been done. For the long-term health of these trees, the Final Environmental Assessment must do so.

Thank you for the opportunity to comment. We look forward to seeing these revisions in the Final Environmental Assessment.

Sincerely, <


Mary Steiner
CEO

cc: Nagamine Engineers Inc., Norman Nagamine
Office of Environmental Quality Control, Gary Gill

0000 0024 26 18

BENJAMIN J. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
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IN REPLY REFER TO:
HWY-DS
2.9454

JUN 10 1998

Ms. Mary Steiner
The Outdoor Circle
1314 South King Street, Suite 306
Honolulu, Hawaii 96814

Dear Ms. Steiner:

Subject: Draft Environmental Assessment
Kalakaua Avenue Bridge Improvements

Thank you for your May 4, 1998, letter commenting on the Draft Environmental Assessment for the proposed Kalakaua Avenue Bridge Improvements. Thank you also for meeting with our staff on May 28, 1998, at the project site to further discuss the exceptional banyan trees.

In meeting with you and the Arborist Advisory Committee on May 28, we clarified that we are not proposing removal of any trees. To provide overhead clearance for construction machinery and to install underground electrical conduits, we propose selective trimming of the canopies of three trees nearest the work area and pruning the roots of one of them. With your help, we reached the following understanding of appropriate treatments for the trees:

- Selective trimming of canopies and root pruning are allowable and shall be performed under the guidance of a certified arborist. Plans detailing the trimming and pruning shall be submitted to the Arborist Advisory Committee for approval.
- To protect against inadvertent damage due to construction activities, barricades shall be installed surrounding the affected tree root systems and steel plates with mulch underlay shall be installed over affected tree root areas not surrounded by the barricades.
- The construction contractors shall be notified during any pre-bid and pre-construction meetings of the requirements and obligations to protect the trees.

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The Final Environmental Assessment will present the general impacts and treatments for the trees and will confirm that we are not planning removal of any of the banyan trees. Should you require more information about this matter, please contact Edmund Yoshida at 587-2122.

Very truly yours,



KAZU HAYASHIDA
Director of Transportation

c: Office of Environmental Quality Control
Arborist Advisory Committee

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