

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 10, 2005

Ms. Genevieve Salmonson, Director  
Office of Environmental Quality Control (OEQC)  
235 South Beretania Street, Suite 702  
Honolulu, Hawaii 96813

RECEIVED  
05 MAR 11 AM 02:21  
OFFICE OF ENVIRONMENTAL  
QUALITY CONTROL

Dear Ms. Salmonson:

SUBJECT: FINDING OF NO SIGNIFICANT IMPACT FOR PAIA  
MINI-BYPASS; TMK: 2-5-5:POR. 18, HANA HIGHWAY  
(POR.), BALDWIN AVENUE (POR.); PAIA, MAUI, HAWAII

The County of Maui Department of Public Works and Environmental Management has reviewed the comments received during the 30-day public comment period which began on March 8, 2004. The agency has determined that this project will not have significant environmental effects and has issued a Finding of No Significant Impact (FONSI). Please publish this notice in the March 23, 2005 OEQC Environmental Notice. ✓

We have enclosed a completed OEQC Publication Form, four (4) copies of the final Environmental Assessment, and the project summary on disk.

Please call Milton Arakawa at (808) 270-7845 if you have any questions.

Sincerely,

MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

Enclosures

xc: Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
Cary Yamashita, Engineering Division Chief

s:\milton\gsalmonson-paia mini-bypass final ea oeqc publication

2005-03-23 FONSI  
PAIA MINI-BY-PASS

MAR 23 2005

# Final Environmental Assessment

---

## PAIA MINI-BYPASS

Prepared by:

County of Maui  
Department of Public Works and  
Environmental Management

March, 2005



---

# CONTENTS

Preface	i
I. PROJECT OVERVIEW	1
II. DESCRIPTION OF THE EXISTING ENVIRONMENT	10
A. PHYSICAL ENVIRONMENT	10
1. Surrounding Land Uses	10
2. Climate	11
3. Topography and Soil Characteristics	11
4. Flood and Tsunami Hazard	15
5. Flora and Fauna	15
6. Archaeological Resources	15
7. Air Quality	18
8. Noise	18
9. Scenic and Open Space Resources	18
10. Chemicals and Fertilizers	18
B. SOCIO-ECONOMIC ENVIRONMENT	19
1. Land Use and Community Character	19
2. Population	19
C. PUBLIC SERVICES	20
1. Police and Fire Protection	20

2.	Medical Facilities	20
3.	Schools	20
4.	Solid Waste	20
D.	INFRASTRUCTURE	21
1.	Roadways	21
2.	Water	21
3.	Wastewater	21
4.	Drainage	22
5.	Electricity and Telephone Systems	22
III.	POTENTIAL IMPACTS AND MITIGATION MEASURES	23
A.	IMPACTS TO THE PHYSICAL ENVIRONMENT	23
1.	Surrounding Land Uses	23
2.	Topography and Landform	23
3.	Flood and Tsunami Hazard	24
4.	Flora and Fauna	24
5.	Archaeological Resources	24
6.	Cultural Resources Assessment	25
7.	Air Quality	26
8.	Noise	27
9.	Scenic and Open Space Resources	27
10.	Chemicals and Fertilizers	28
B.	IMPACTS TO THE SOCIO-ECONOMIC ENVIRONMENT	28

1.	Economy	28
2.	Agriculture	28
3.	Police, Fire and Medical	28
4.	Recreational and Educational Resources	28
5.	Solid Waste	29
C.	IMPACTS TO INFRASTRUCTURE	29
1.	Roadways	29
2.	Water	31
3.	Wastewater	32
4.	Drainage and Erosion Control	32
5.	Electrical and Communication Services	33
IV.	RELATIONSHIP TO GOVERNMENTAL PLANS, POLICIES AND CONTROLS	34
A.	STATE LAND USE DISTRICT	34
B.	GENERAL PLAN OF THE COUNTY OF MAUI	34
C.	PAIA-HAIKU COMMUNITY PLAN	36
D.	ZONING	39
E.	COASTAL ZONE MANAGEMENT OBJECTIVES AND POLICIES	39
F.	NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM	47
V.	SUMMARY OF ADVERSE ENVIRONMENTAL EFFECTS WHICH CANNOT BE AVOIDED	48

VI.	ALTERNATIVES TO THE PROPOSED ACTION	49
VII.	IRREVERSIBLE AND IRRETRIEVABLE COMMITMENT OF RESOURCES	57
VIII.	FINDINGS AND CONCLUSIONS	58
IX.	AGENCIES CONSULTED DURING THE PREPARATION OF THE DRAFT ENVIRONMENTAL ASSESSMENT; LETTERS RECEIVED AND RESPONSES TO SUBSTANTIVE COMMENTS	62
X.	LETTERS RECEIVED DURING THE DRAFT ENVIRONMENTAL ASSESSMENT PUBLIC COMMENT PERIOD AND RESPONSES TO PUBLIC COMMENTS	63
	REFERENCES	i

LIST OF APPENDICES

A	Archaeological Assessment Report
B	Cultural Resources Assessment
C	Traffic Letter Report
C-1	Traffic Assessment
D	Drainage Reports
E	Map of Kala/Sunnyside Road

LIST OF FIGURES

1	Regional Location Map	3
2	Signing and Pavement Marking Plans	4
3	Signing and Pavement Marking Plans	5
4	Typical Section Within Hana Highway Right-of-Way	6
5	Plan and Profile	7
6	Plan and Profile	8
7	Typical Section from Hana Highway to Baldwin Avenue	9
8	Soil Association Map	12
9	Soil Classification Map	14
10	ALISH Classification	16
11	Flood Insurance Rate Map Designation	17
12	State Land Use District Boundaries	33
13	Community Plan Land Use Map	35
14	Approximate SMA Limits	38

8:\milton\paia mini bypass final ea

**Preface**

The State of Hawaii Department of Transportation (DOT), A&B Properties, Inc., and the County of Maui Department of Public Works and Environmental Management (DPWEM) are proposing the Paia Mini-Bypass project.

The project involves lands within the Hana Highway right-of-way which is a State highway, lands owned by Alexander & Baldwin, Inc., and the Baldwin Avenue right-of-way which is a County roadway.

Since the project involves State and County lands and funds, an Environmental Assessment (EA) has been prepared as required by Chapter 343, Hawaii Revised Statutes. After consultation with the State DOT and A&B Properties, Inc., it has been resolved that the DPWEM will be the lead agency in processing the EA. This EA documents the proposed action's technical characteristics, environmental impacts and alternatives. Further, the EA advances findings and conclusions relative to the significance of the project and its surrounding environs.

---

## **I. PROJECT OVERVIEW**

The State of Hawaii Department of Transportation (DOT), A&B Properties, Inc. and the County of Maui Department of Public Works and Environmental Management (DPWEM) are proposing the Paia Mini-Bypass Project in Paia, Maui, Hawaii. See Figure 1.

The project would create a one-way, single-lane roadway extending from the vicinity of Baldwin Park to the new Paia Post Office. From Baldwin Park, a shoulder lane is proposed within the Hana Highway right-of-way extending to the vicinity of the existing Paia Municipal Parking Lot, a lineal distance of approximately 2,800 feet or .53 mile. The Hana Highway shoulders will be widened and reinforced so that the vehicular travel lanes would be restriped approximately four (4) feet makai of the existing lanes. The vehicular travel lanes will remain 12 feet in width. A 12 foot wide shoulder travel lane will then be created on the mauka side of the travel lanes along with new four (4) foot wide paved shoulders. All work within this segment of the project is proposed within the existing Hana Highway right-of-way. See Figures 2, 3 and 4.

The shoulder lane would then turn mauka to a single-lane roadway near the existing municipal parking lot, skirting existing developed portions of Paia Town, then connecting with Baldwin Avenue near the new Paia Post Office. A 12 foot wide travel lane with two (2) foot wide shoulders on each side are proposed. At the Baldwin Avenue connection, separate left- and right-turn lanes are proposed. The portion of the project which extends beyond the Hana Highway right-of-way to Baldwin Avenue involves TMK 2-5-5:portion of 18. Transition work within the Baldwin Avenue right-of-way including curb returns, resurfacing and striping are also proposed.

The project originally proposed a connection to Baldwin Avenue makai of the Paia Post Office. However, there were concerns raised during the public comment period that this connection point is too close to the existing post office driveway. To address this concern, the connection has been moved to an adjacent area mauka of the post office. The total lineal distance of the single-lane roadway from Hana Highway to Baldwin Avenue is approximately 2,000 feet. See Figures 5, 6 and 7.

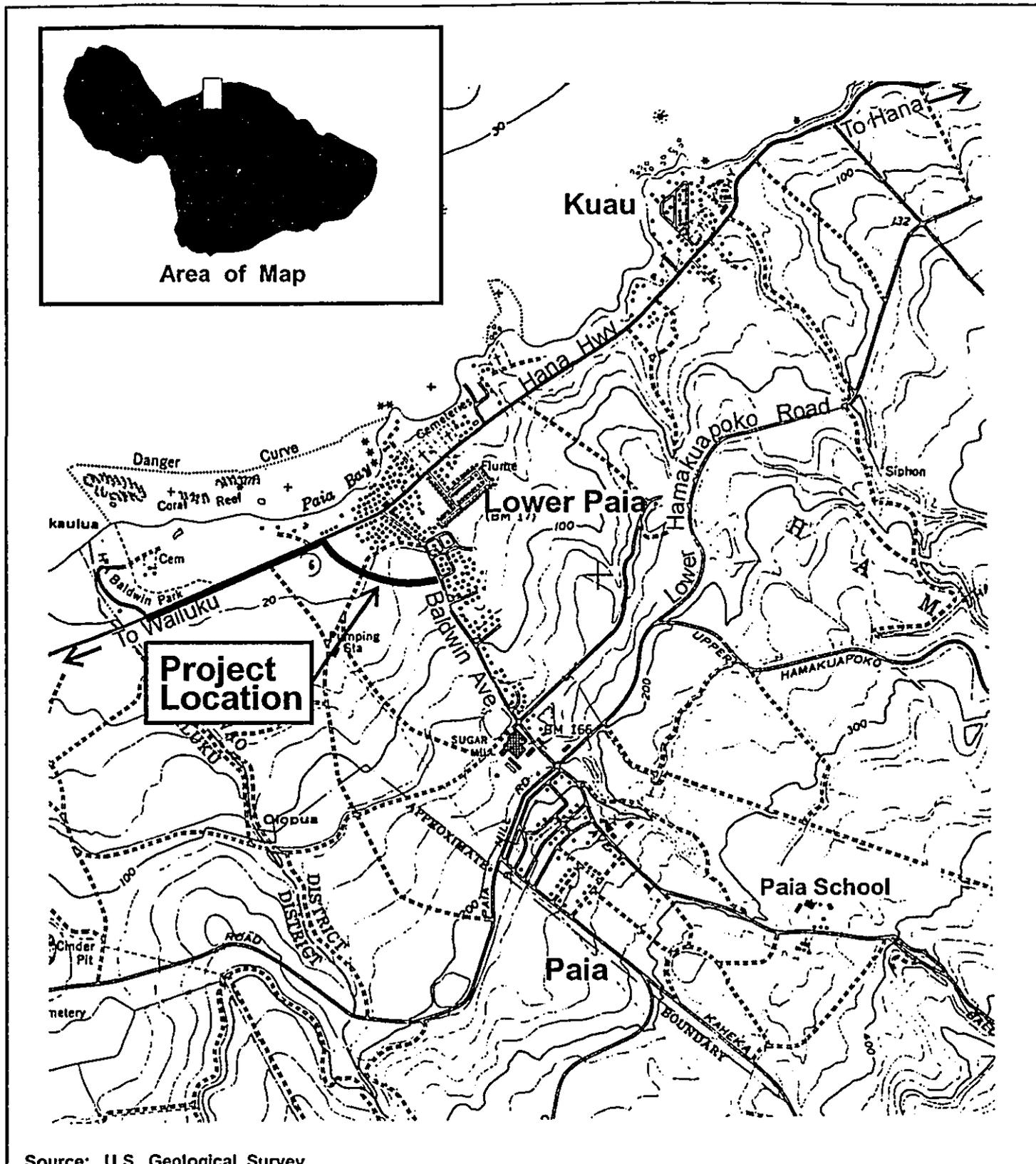
---

The intent of the project is to mitigate some of the existing traffic congestion on Hana Highway in the afternoon peak hour by providing an additional travel lane for vehicles desiring to turn right on Baldwin Avenue.

Hours of operation for the single-lane roadway are 4:00 p.m. to 6:00 p.m., excluding Saturdays, Sundays and holidays. During non-afternoon peak hours, the single-lane roadway will be gated near the municipal parking lot and at its Baldwin Avenue connection.

The State DOT is proposing to construct the portion of the project within the Hana Highway right-of-way which is a State highway. A&B Properties, Inc. will be building the portion of the project from Hana Highway to Baldwin Avenue. The County DPWEM will be constructing improvements within the Baldwin Avenue right-of-way, which is a County roadway. Since the project involves the use of State and County land and funds, an Environmental Assessment (EA) is being prepared pursuant to Chapter 343, Hawaii Revised Statutes (HRS).

The estimated construction cost for the entire project is \$650,000. Assuming all applicable approvals are obtained, construction is anticipated to begin during the second quarter of 2005. Construction is anticipated to be complete approximately three (3) months after construction start.



Source: U.S. Geological Survey

**Figure 1 PAIA MINI-BYPASS  
Regional Location Map**

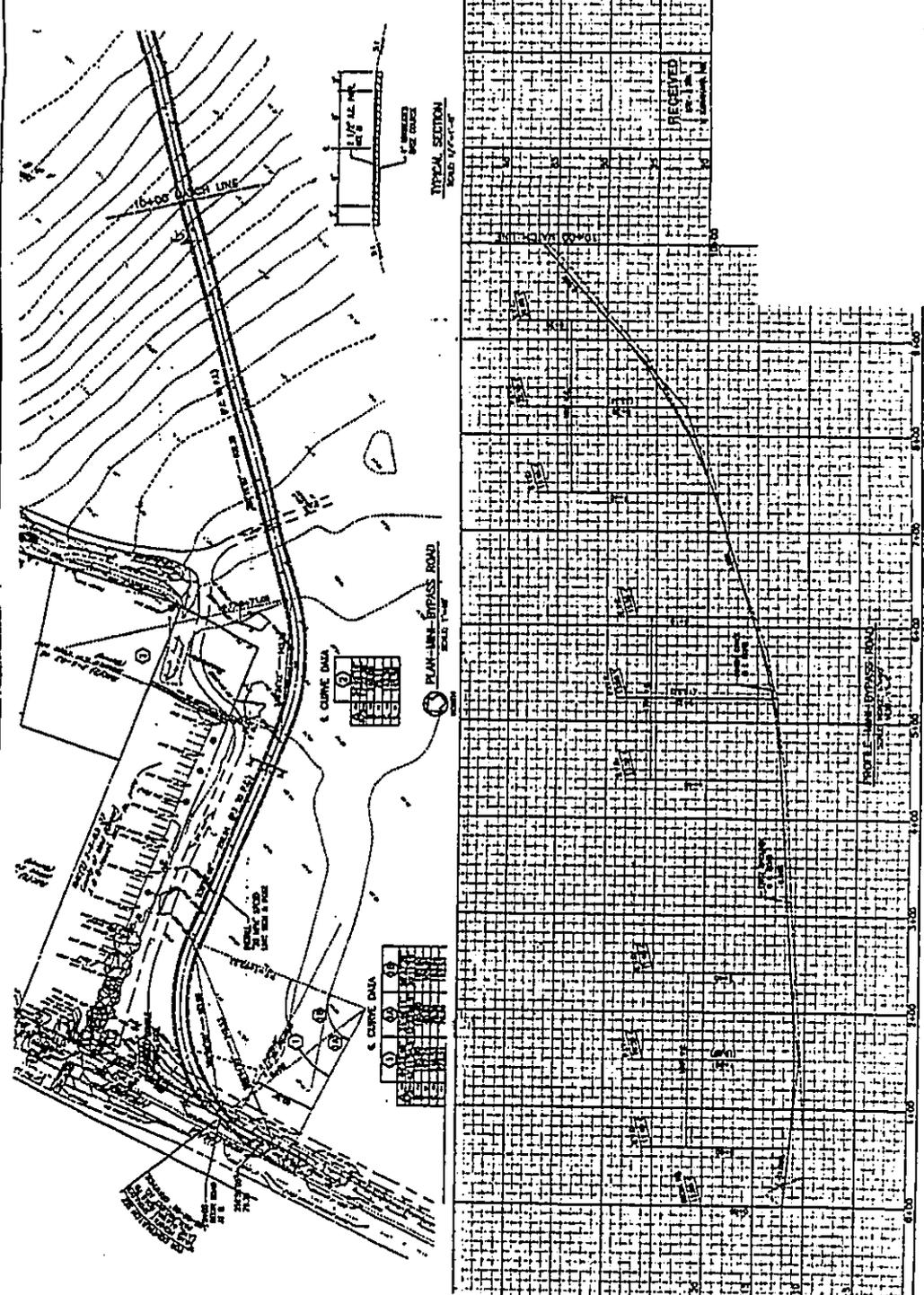


1 in. = 2000 ft.









Source: Sato and Associates, Inc.

**PAIA MINI-BYPASS  
Plan and Profile**

Not to scale

Figure 5

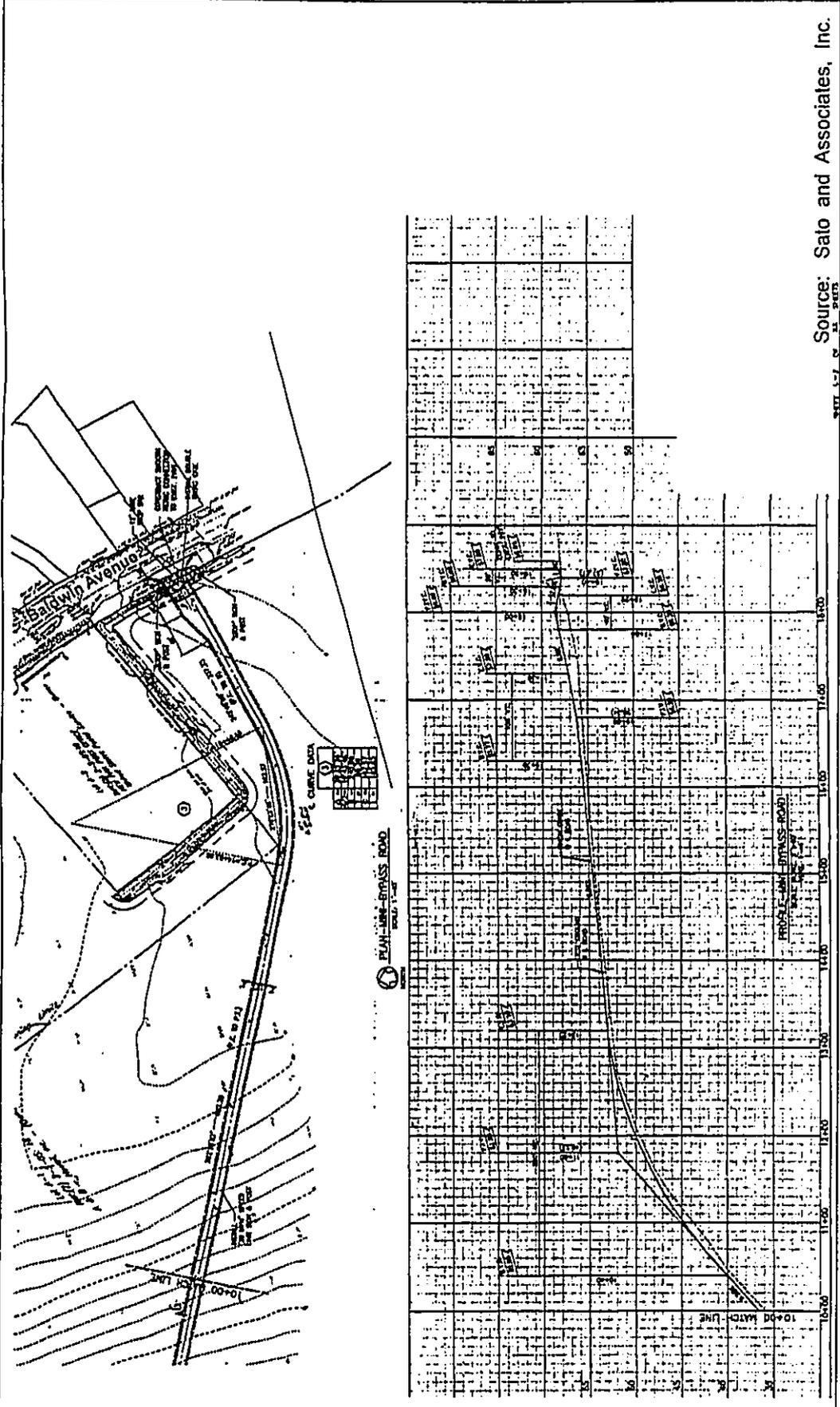
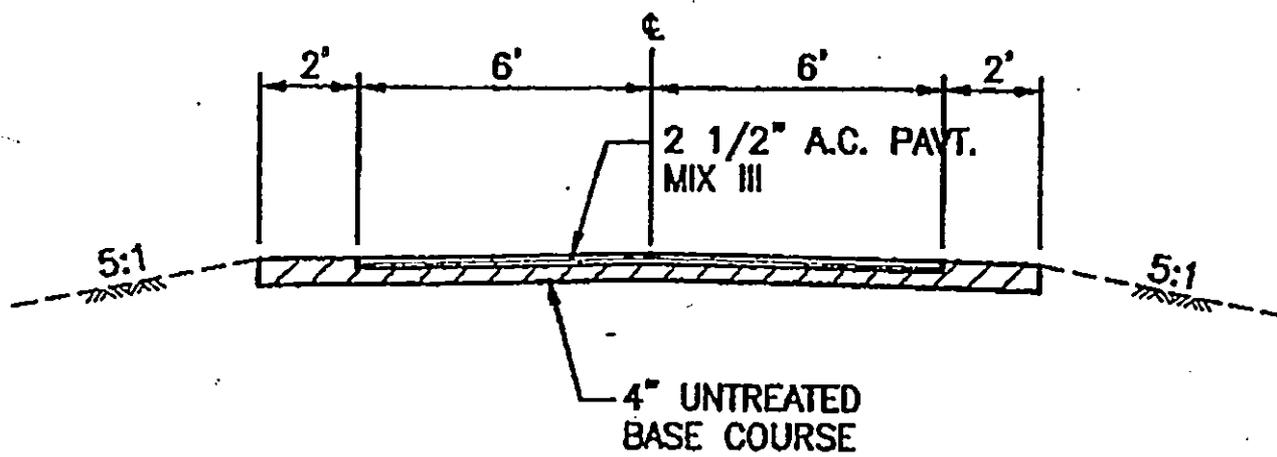


Figure 6

**PAIA MINI-BYPASS  
Plan and Profile**

Not to scale



Source: Sato and Associates, Inc.

**Figure 7 PAIA MINI-BYPASS**  
**Typical Section from Hana Highway**  
**to Baldwin Avenue**

1/4 in. = 1 foot

---

## **II. DESCRIPTION OF THE EXISTING ENVIRONMENT**

### **A. PHYSICAL ENVIRONMENT**

#### **1. Surrounding Land Uses**

Near the westerly end of the project, Henry A. Baldwin Park is located on the makai side of Hana Highway. This is a 17.2 acre County beach park. The private nine (9) hole Maui Country Club golf course and the town of Spreckelsville lie further to the west. Mauka or south of Hana Highway are existing sugar cane fields which extend gently upslope.

On the east side of Baldwin Park, there is the Paia Rin Zai Zen Mission, several residential dwellings not yet completed, and sugar cane fields. On the makai side of Hana Highway, the Paia Youth Center and the 1.5 acre Lower Paia Park abut Puna Road at the western edge of Paia Town.

In general, Paia Town is noted for its plantation era architecture and small town ambiance. The business/commercial establishments of Paia are primarily located along the Hana Highway and Baldwin Avenue, with the vicinity of the Hana Highway-Baldwin Avenue intersection serving as a focal point of business/commercial activity. Existing town businesses include retail shops, restaurants, boutiques and real estate businesses. Interspersed between business/commercial uses are existing residential uses, vacant lots, and public uses. Further south or mauka along Baldwin Avenue, single-family residential becomes the predominant use.

The Paia Mini-Bypass would extend mauka adjacent to the 82 stall Paia Municipal Parking Lot which is located just before the western or Wailuku entrance to Paia Town. The proposed project extends through sugar cane fields near existing developed portions of Paia and would connect to Baldwin Avenue just mauka or south of the Paia Post Office.

---

2. **Climate**

Like most of the State of Hawaii, Maui experiences a relatively uniform year-round climate with mild temperatures, moderate humidity and consistent northeasterly tradewinds. However, local terrain can greatly influence the climate in different areas. The region experiences a relatively even climate with little seasonal and day-night temperature variation. Cool tradewinds from the northeast help keep the warm summer months pleasant.

Temperatures in Paia range from 54 to 94 degrees, with the lowest temperatures typically occurring between December and February, and the highest temperatures in August and September. Situated at the base of Haleakala, Paia is located directly in the path of the northeast tradewinds. The tradewinds usually range from 15 to 25 miles per hour (mph) and increase in strength during the day from March to September. Winds generally become light and variable during the absence of the tradewinds. The diurnal heating and cooling of the island creates onshore sea breezes during the day and offshore land breezes at night.

Paia receives about 25 inches of rainfall annually. Following the wet winter/dry summer pattern typical for most of Hawaii, the Paia-Haiku region usually receives two (2) to three (3) times of its average monthly rainfall in the winter months compared to the summer months.

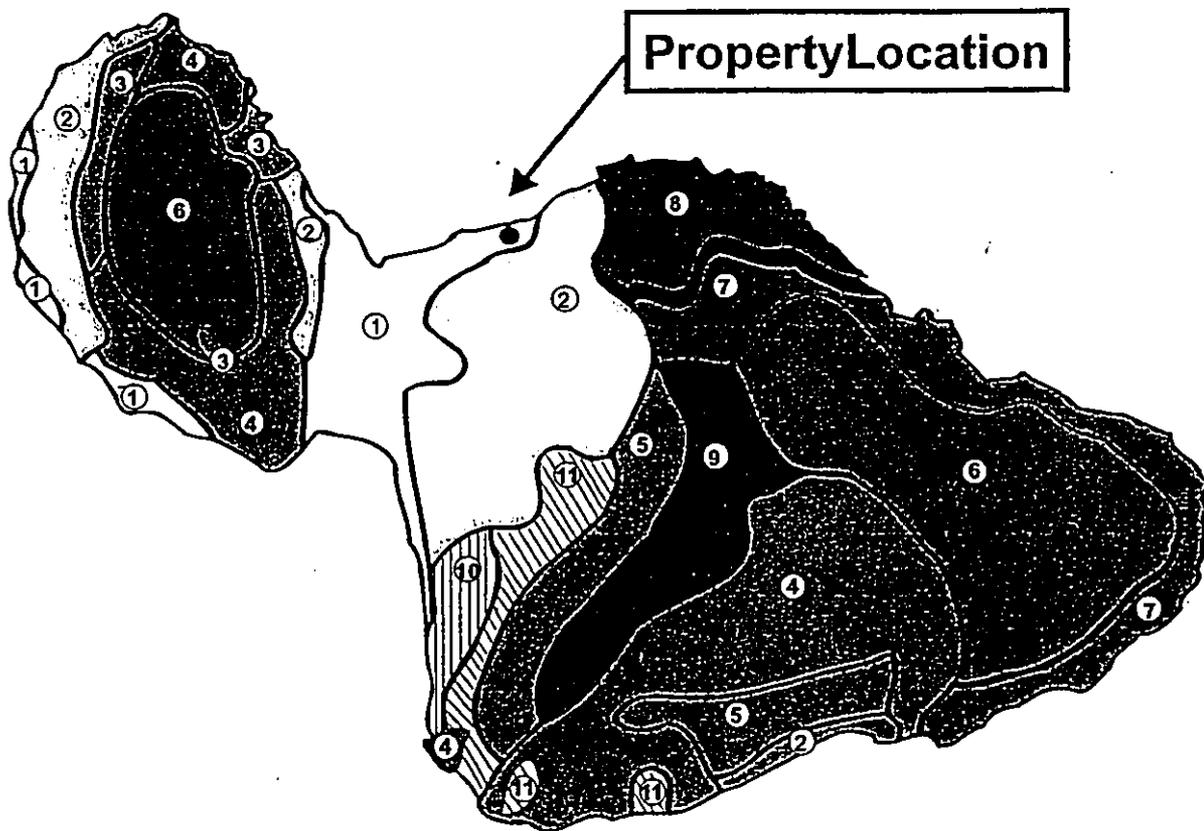
3. **Topography and Soil Characteristics**

The topography of Paia Town slopes gently upward from sea level to an elevation of about 340 feet above mean sea level (amsl) near Paia School. The town of Paia is located on a lower sloping plain, with development generally clustered around Hana Highway and Baldwin Avenue.

The soils underlying the subject area are of the Pulehu-Ewa-Jaucas association. See Figure 8. These deep, nearly level to moderately

### LEGEND

- |  |                                     |
|--|-------------------------------------|
| ① Pulehu-Ewa-Jaucas association                | ⑦ Hana-Makaalae-Kailua association  |
| ② Waiakoa-Keahua-Molokai association           | ⑧ Pauwela-Haiku association         |
| ③ Honolua-Olelo association                    | ⑨ Laumaia-Kaipoi-Olinda association |
| ④ Rock land-Rough mountainous land association | ⑩ Keawakapu-Makena association      |
| ⑤ Puu Pa-Kula-Pane association                 | ⑪ Kamaole-Oanapuka association      |
| ⑥ Hydrandepts-Tropaquods association           |                                     |



Map Source: USDA Soil Conservation Service

**Figure 8 PAIA MINI-BYPASS  
Soil Association Map**



Not to scale

---

sloping, well to excessively drained soils occur on alluvial fans and basins. The subsoil is moderately fine to coarse textured.

The specific soil types underlying the project site are Paia silty clay, 3 to 7 percent slopes (PcB); Paia silty clay, 7 to 15 percent slopes (PcC); Pulehu silt loam, 0 to 3 percent slopes (PpA); Pulehu silt loam, 3 to 7 percent slopes; and Ewa silty clay loam, 0 to 3 percent slopes (EaA). See Figure 9.

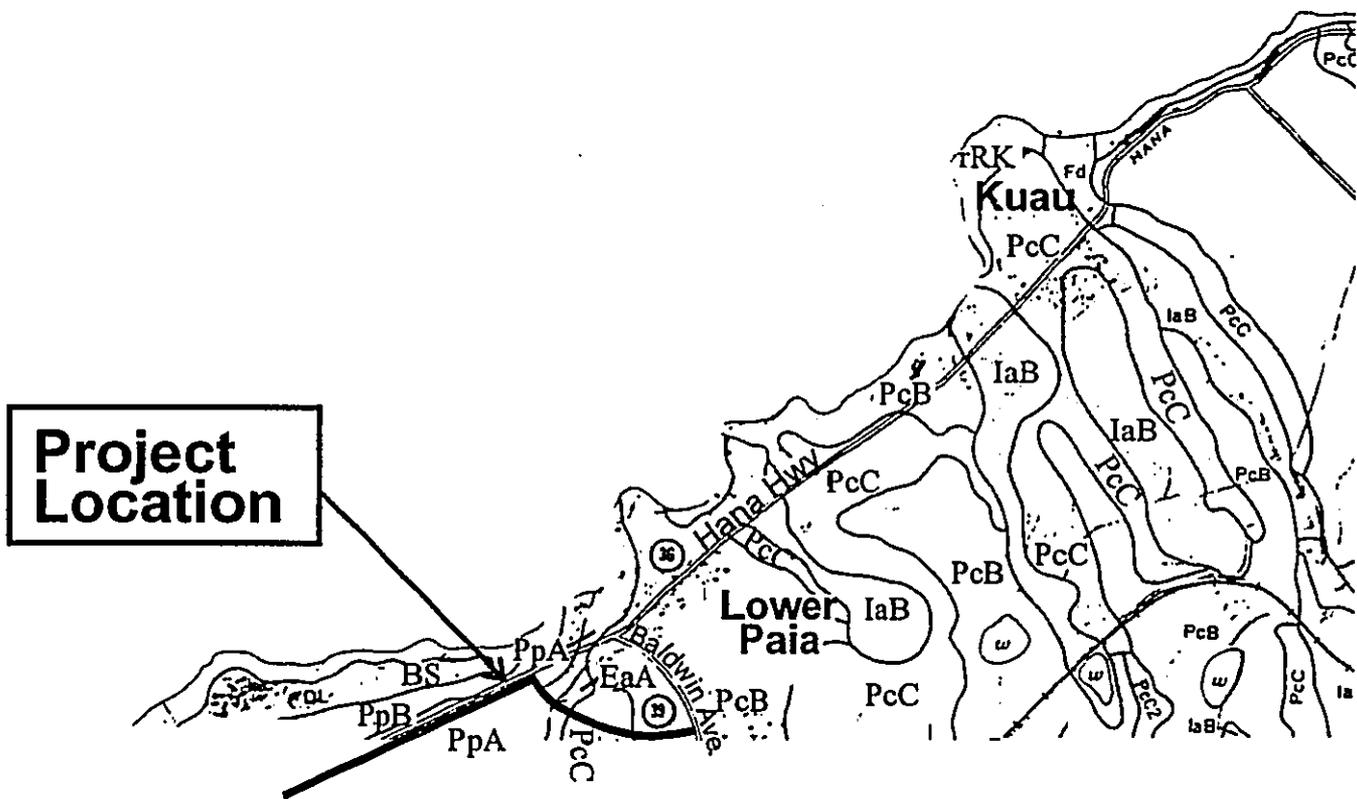
Paia silty clay, 3 to 7 percent slopes (PcB) soils are typically found on nearly level areas, noted for their moderate permeability, slow runoff and a slight erosion hazard. Paia silty clay, 7 to 15 percent slopes (PcC) soils are characterized by slow to medium runoff and a slight to moderate erosion hazard.

Pulehu silt loam, 0 to 3 percent slopes (PpA) soils are noted for moderate permeability, slow runoff and a no more than slight erosion hazard. Pulehu silt loam, 3 to 7 percent slopes (PpB) soils are characterized by slow runoff and a slight erosion hazard.

Ewa silty clay loam, 0 to 3 percent slopes (EaA) soils have very slow runoff and an erosion hazard characterized as no more than slight.

The State Department of Agriculture has established three (3) categories of Agricultural Lands of Importance to the State of Hawaii (ALISH). The ALISH system classifies lands into "Prime", "Unique" and "Other Important Agricultural Land". The remaining lands are "Unclassified". Utilizing modern farming methods, "Prime" agricultural lands have the soil quality, growing season, and moisture supply needed to produce sustained crop yields economically, while "Unique" agricultural lands possess a combination of soil quality, location, growing season, and moisture supply currently used to produce sustained high yields of a specific crop. "Other Important Agricultural Land" includes those which have not been rated as "Prime" or "Unique". As indicated by the ALISH map, the land encompassing

# Pacific Ocean



Source: U.S. Department of Agriculture,  
Soil Conservation Service

Figure 9

## PAIA MINI-BYPASS Soil Classification Map



1 in. = 2000 ft.

---

the proposed subject project falls within the "Prime" category. See Figure 10.

4. **Flood and Tsunami Hazard**

According to Flood Insurance Rate Maps (FIRM) for the region, the subject properties are located in Zone C, areas of minimal flooding, and Zone A4, areas of the 100-year flood with base flood elevations of 17 feet above sea level. See Figure 11. It is noted that Hana Highway in this vicinity is the mauka limit of areas of the 100-year coastal flood with velocity (wave action).

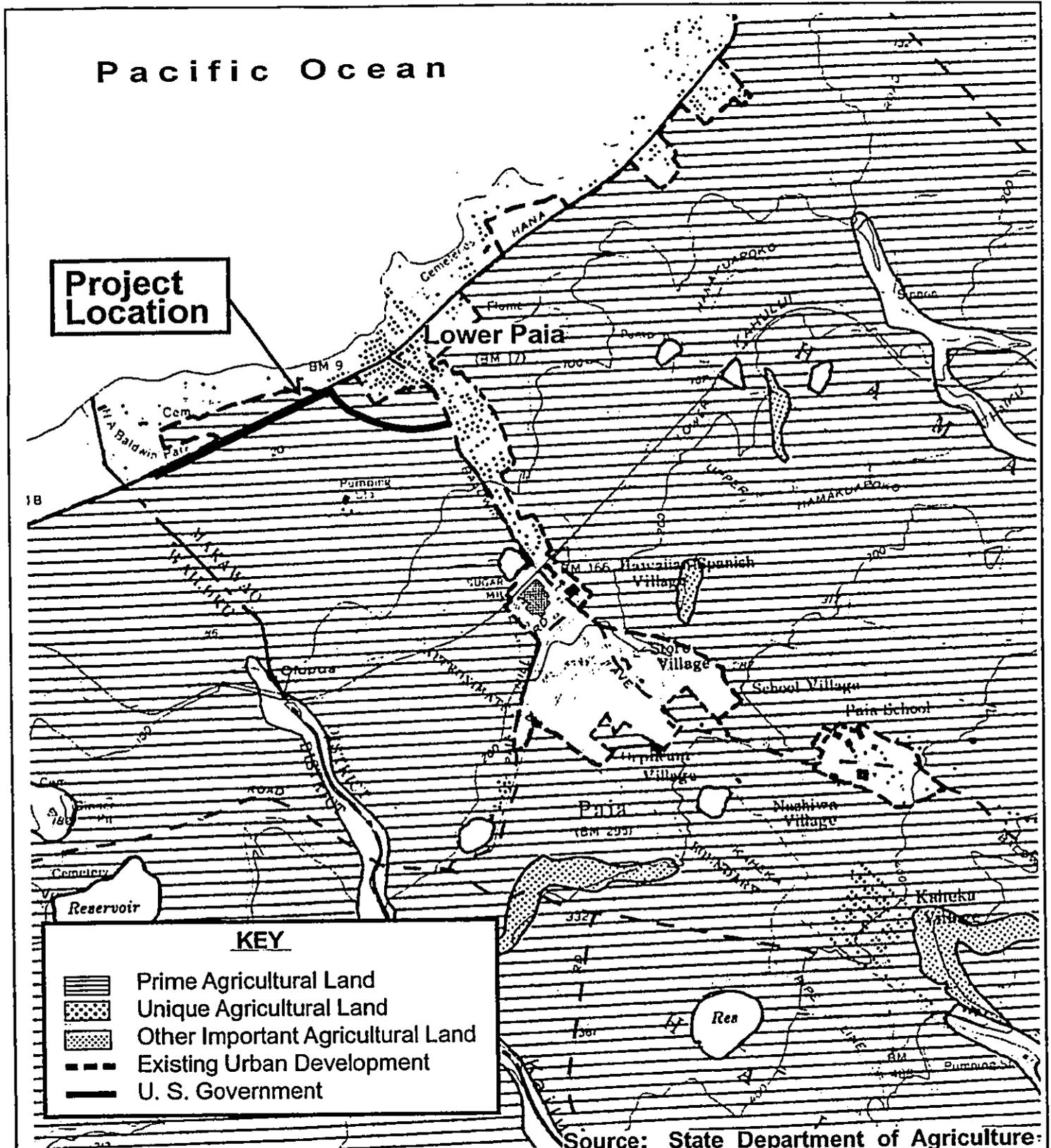
5. **Flora and Fauna**

Portions of the project within the Hana Highway right-of-way are either already paved or grassed. Portions of the project within TMK: 2-5-5:portion 18 have been cultivated in sugar cane. Vegetation commonly found in the vicinity of the project site include buffelgrass, koa haole, kiawe, coconut palm, date palm tree, and other grasses and annual weeds.

Introduced terrestrial fauna in the region include rats, mice, feral cats, and mongoose. Introduced avifauna include the Mynah, Spotted Dove, Barred Dove, Japanese White-eye and House Sparrow.

6. **Archaeological Resources**

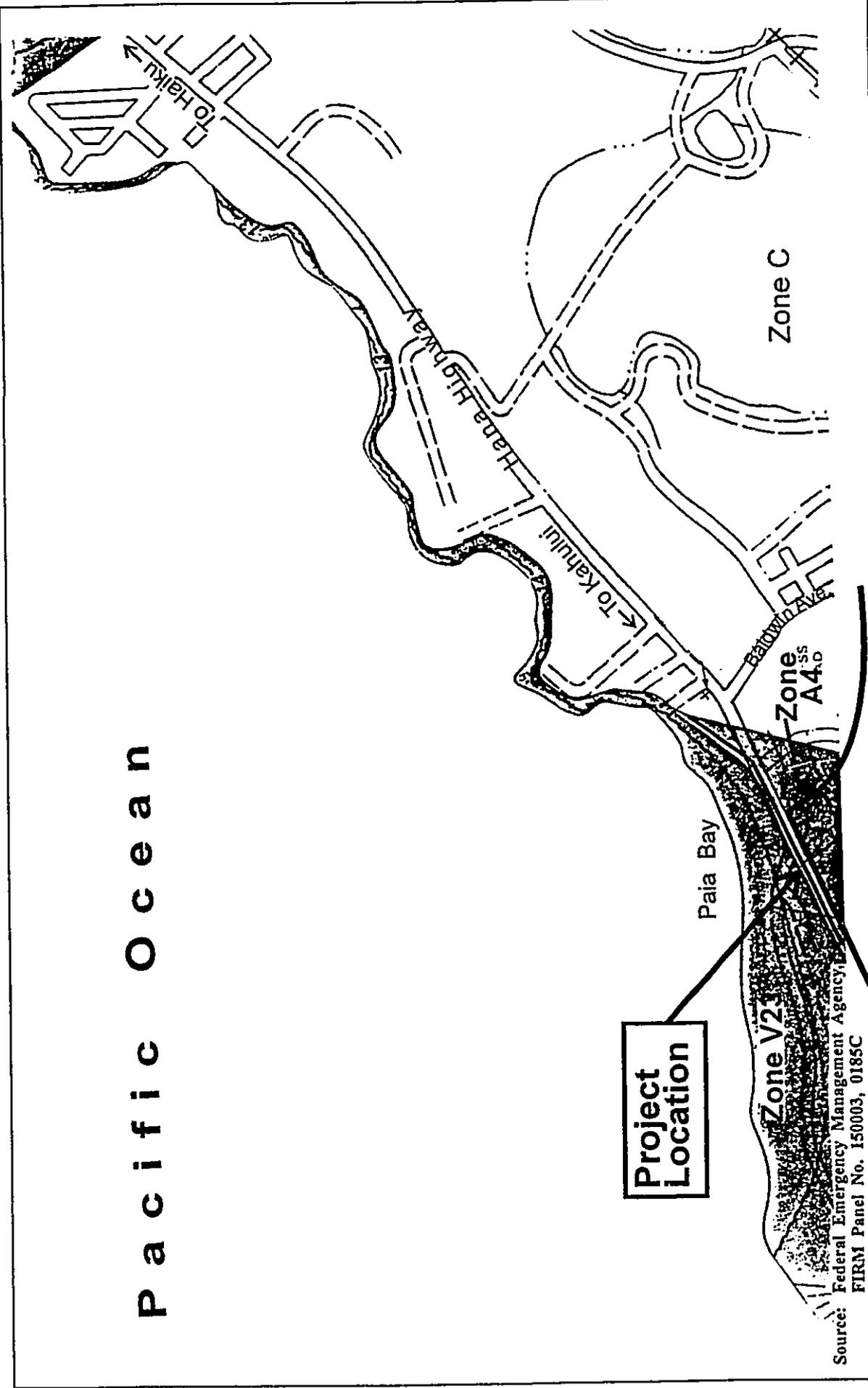
The segment of Hana Highway which is part of the subject project was previously constructed on top of fill material. All excavation which is being proposed to construct the necessary base course and shoulder extension within the Hana Highway right-of-way will occur within fill material from previous highway construction. Portions of the project within land designated as TMK 2-5-5:portion of 18 have an extensive history of sugar cane cultivation which has significantly altered the underlying lands. The portion of the project within Baldwin Avenue involves construction of curb returns which would involve excavation of not more than 12 inches below existing grade. No



**Figure 10 PAIA MINI-BYPASS ALISH Classification**



1 in. = 2000 ft.



Source: Federal Emergency Management Agency,  
FIRM Panel No. 150003, 0185C

**Figure 11 PAIA MINI-BYPASS  
Flood Insurance Rate Map Designation**

---

archaeological or cultural materials are believed to be present within the affected areas of the project.

7. **Air Quality**

There are no point sources of airborne emissions in proximity of the subject project. Air quality in the vicinity of the properties may be affected by a variety of sources, including smoke and dust from sugar cane harvesting and cultivation operations which are adjacent to Paia Town. Although minimal, airborne pollutants are largely attributable to vehicular exhaust from traffic along the region's roadways. However, sources are intermittent and prevailing winds quickly disperse the particulates generated by these temporary sources.

8. **Noise**

Vehicular traffic traveling along Baldwin Avenue and the Hana Highway are the primary sources of noise at the subject properties. Other noise sources are attributable to agricultural operations and natural conditions such as wind and rain.

9. **Scenic and Open Space Resources**

Scenic and open space resources at the subject parcels include views of the Upcountry area and Mount Haleakala to the southeast. Views of the West Maui Mountains are also provided from this vicinity of Paia Town. The subject properties are not part of a scenic corridor.

10. **Chemicals and Fertilizers**

Land designated as TMK 2-5-5: portion of 18 has a land use history of sugar cane cultivation by Hawaiian Commercial & Sugar Company (HC&S). To assist in sugar cane production efforts for Parcel 18, HC&S has utilized common fertilizers, including Urea, Phosphoric acid and potash. HC&S has also used various chemicals to protect crops from invasive weeds and pests. The chemicals applied for both properties have been approved by the Environmental Protection Agency (EPA), and applied in accordance with the pesticide labels.

---

B. SOCIO-ECONOMIC ENVIRONMENT

1. *Land Use and Community Character*

The Paia-Haiku Community Plan region is largely agricultural and rural in character. Although Paia is a primary urban center within the region, it retains a "small town" scale and character. The Paia commercial town core is situated around the intersection of Hana Highway and Baldwin Avenue. Existing residential development is generally concentrated around the commercial core, between Paia Town and Kuau, and along Baldwin Avenue to Skill Village, above the old Paia Mill.

The primary agricultural activity in the Paia region is sugar cane. Sugar cane grown in the area was processed at the HC&S Paia Sugar Mill, until the mill permanently closed in September of 2000. Today, all sugar cane from the Paia region is processed at the HC&S Puunene Sugar Mill. Although Paia's identity and character can be traced back to its agricultural past, the town character has evolved over the last decade, reflecting its proximity to the North Shore. Today, ocean sporting enthusiasts from around the world travel to Paia to experience Maui's world class ocean resources, with many opting to take both temporary and permanent residence in the area.

2. *Population*

The population of the County of Maui has exhibited relatively strong growth over the past decade with the 2000 population estimated to be 128,241, a 27.6 percent increase over the 1990 population of 100,504 (Maui County Data Book, June 2001). Growth in the County is expected to continue, with resident population projections to the year 2010 estimated to be 136,400 (Maui County Data Book, June 2001).

The Paia-Haiku-Pauwela region follows the County-wide pattern of population growth, with the region's 2000 population estimated at 9,077 (Maui County Data Book, June 2001).

---

**C. PUBLIC SERVICES**

**1. Police and Fire Protection**

The County of Maui's Police Department is headquartered at its Wailuku Station on Mahalani Street. There are three (3) patrol divisions on the island of Maui, serving the Wailuku, Lahaina and Hana regions. The Wailuku division services Central Maui, Paia-Haiku, Upcountry and the Kihei-Makena areas.

Fire prevention, suppression and protection services for the Paia-Haiku region is provided by the County Department of Fire Control's Paia Station, located along Hana Highway in Paia Town.

**2. Medical Facilities**

Maui Memorial Medical Center, the only major medical facility on the Island, services the Paia-Haiku region. Acute, general and emergency care services are provided by the 196-bed hospital. Private medical and dental clinics and practices which are located predominantly in the Wailuku-Kahului area also serve residents of Paia-Haiku.

**3. Schools**

The State Department of Education operates one (1) school in the Paia area. Paia Elementary School covers grades K to 5. Public school students from the Paia area would then be serviced by Samuel Kalama Intermediate School in Makawao which covers grades 6 to 8. King Kekaulike High School serves as the region's high school, for grades 9 through 12.

The Doris Todd Memorial Christian School is a private educational facility located in Paia covering grades K to 6.

**4. Solid Waste**

Single-family residential solid waste collection service is provided by the County of Maui on a once-a-week basis. Residential solid waste collected by County crews is disposed at the County's 55-acre Central Maui Landfill, located four (4) miles southeast of the Kahului Airport.

---

In addition to County-collected refuse, the Central Maui Landfill accepts commercial waste from private collection companies.

**D. INFRASTRUCTURE**

**1. Roadways**

The major roadway serving the Paia region is the Hana Highway, which extends from Kahului to Hana. In the Paia area, Hana Highway is a two-way, two-lane arterial. Fronting the project site is Baldwin Avenue, a two-way, two-lane major collector which links the rural towns of Makawao and Paia.

**2. Water**

The Paia area is serviced by the Department of Water Supply's domestic water system. The Waihee Wells, which were developed by the Central Maui Source Joint Venture, are the specific water sources for two (2) tanks servicing Paia, including a 100,000 gallon tank located at an elevation of 267 feet amsl and a 300,000 gallon tank located at an elevation of 456 feet amsl.

Within the Hana Highway right-of-way, there are eight (8) inch and 12 inch waterlines and fire hydrants. Fronting the project site along Baldwin Avenue are two (2) existing waterlines, including a six (6) inch line on the east side of the roadway and an eight (8) inch line on the west side of the roadway and fire hydrants.

**3. Wastewater**

An eight (8) inch sewerline is located within the Baldwin Avenue right-of-way in the vicinity of the project's connection to Baldwin Avenue. Within Hana Highway, there is an 18 inch sewerline within Paia Town. The sewerline then follows Puna Road connecting to a County wastewater pump station. The wastewater is transported to the Wailuku-Kahului Wastewater Reclamation Facility via a series of coastal sewerlines, force mains and pump stations.

The design capacity of the Wailuku-Kahului Wastewater Reclamation Facility is 7.9 million gallons per day. Excluding groundwater and

---

storm water infiltration, current wastewater flow volume treated at the facility is approximately 6.6 mgd.

4. **Drainage**

Along the mauka portions of the project site, runoff is currently retained within the existing sugar cane fields by earth berms which are located along Baldwin Avenue, around the Paia Main Post Office lot, existing developed areas of Paia Town and Hana Highway. Runoff which does not infiltrate into the ground flows toward a low area near the County's public parking lot on Hana Highway. Within the Hana Highway right-of-way, surface runoff is conveyed in a westerly direction along the grassed shoulders to an existing box culvert in the proximity of the Baldwin Park entrance. This runoff eventually outlets to the ocean.

5. **Electricity and Telephone Systems**

Electrical and telephone services in the Paia-Haiku region are provided by Maui Electric Company and Verizon Hawaii, respectively.

---

### **III. POTENTIAL IMPACTS AND MITIGATION MEASURES**

#### **A. IMPACTS TO THE PHYSICAL ENVIRONMENT**

##### **1. Surrounding Land Uses**

The proposed project would be located to the west of Paia Town along Hana Highway. Then the one (1) lane road proceeds mauka, adjacent to the western boundaries of the town connecting to Baldwin Avenue just mauka of the Paia Post Office.

Baldwin Park is located near the western end of the project along Hana Highway. Further toward Paia Town, lands adjacent to the highway and lands to the west of the one (1) lane roadway extending around the town are in sugar cane cultivation. The general land use pattern within Paia entails commercial and retail uses along Hana Highway and Baldwin Avenue, with residential uses to the interior areas and adjacent to the shoreline.

Typical commercial establishments are one (1) story in height with occasional two (2) story buildings adding interest and variety to the streetscape. Building frontages are typically located at or near the front property line. Within the Hana Highway right-of-way in Paia Town, there is angle parking along the storefronts on the mauka side with parallel parking on the makai side. Along Baldwin Avenue, parallel parking is concentrated near its intersection with Hana Highway. Street parking diminishes with greater distance from the commercial core.

The proposed project is consistent with adjacent and surrounding land uses and is not anticipated to create any adverse impacts.

##### **2. Topography and Landform**

The proposed project is not anticipated to result in significant earthmoving activities. To the extent practicable, finished contours will follow existing grades to minimize earthwork costs and maintain drainage patterns.

---

3. **Flood and Tsunami Hazard**

The proposed improvements will not significantly alter existing grades or drainage patterns, and will not adversely impact the flood zone, coastal high hazard areas or adjoining properties. In addition, there are no habitable areas proposed as part of the project. There are no known streams, water bodies or wetlands within the project site.

4. **Flora and Fauna**

There are no identified significant habitats of rare, endangered, or threatened species of flora or fauna located within the vicinity of the project site. During construction, Best Management Practices (BMPs) will be utilized to mitigate potential adverse impacts to surrounding environs. The proposed project is not anticipated to result in adverse impacts to the flora and fauna in the area.

5. **Archaeological Resources**

The existing Hana Highway in the vicinity of the project has been constructed on fill material. Excavation within the Hana Highway right-of-way will occur entirely within the fill area. Lands underlying Parcel 18 have been previously utilized in the cultivation of sugar cane which has resulted in significant ground alterations. Work within Baldwin Avenue will take place in areas of the proposed curb returns which would be located next to the existing edge of pavement. During the construction and excavation of the adjacent Paia Post Office, no significant cultural materials were encountered.

Based on comments from the State Historic Preservation Division, an archaeological assessment report was done by Xamanek Researches for the portion of the project area consisting of the single-lane roadway extending from Hana Highway through former sugar-cane lands to Baldwin Avenue. This area has been impacted by previous field clearing, planting and harvesting activities.

The pedestrian portion of the archaeological inventory/assessment did not locate any surface structural remains. Subsurface

---

investigation consisting of 11 backhoe trenches were utilized to sample the study area. Four (4) of the 11 backhoe trenches revealed relatively shallow marine sand deposits in the makai portions of the study area. However, no precontact or post contact cultural layers were found during the survey.

The report concludes that while there were no significant material culture remains found, the presence of intact marine sand deposits between Hana Highway and a point approximately 450 feet mauka of the highway is of interest. The study notes that human burials have been located in the general area, often associated with sand deposits. Thus, precautionary monitoring is recommended during construction activities in the makai portions of the project area extending from Hana Highway to a point approximately 450 feet mauka of the highway. See Appendix A.

In accordance with these recommendations, the applicant will implement these precautionary measures during construction.

In the event that any archaeological remains or cultural materials are encountered during construction activities, all work in the vicinity of the find will be halted and the State Historic Preservation Division will be contacted to establish appropriate mitigative measures in accordance with Chapter 6E, Hawaii Revised Statutes.

**6. Cultural Resources Assessment**

A cultural impact assessment was done by CKM Cultural Resources in July, 2002 for A&B Properties, Inc. for the Paia Post Office and an adjacent commercial and parking area comprising an area of approximately five (5) acres. Although the post office has been constructed, the commercial and parking area has been deferred.

The assessment indicated that the lands in the vicinity of the study area were used during pre-contact times for farming only, noting that the area's dry plains and slightly arid temperatures made for ideal conditions for the growing of 'uala (*Ipomea batatas*) or sweet potato.

---

The long history of sugar cane cultivation in the region which probably started in the mid-1800's was also noted. The report concluded that post contact use of the lands in sugar cane production has most likely eliminated the possibility of the study area containing significant surface cultural materials. The report did not identify any traditional access ways or significant cultural practices in the vicinity of the study area. See Appendix B.

While it is recognized that the subject cultural assessment concerned lands adjacent to the subject project, the conclusions regarding the effects of sugar cane cultivation also apply. Moreover, it is noted that work within the Hana Highway right-of-way will affect only areas of fill. Minor excavation is anticipated within the Baldwin Avenue right-of-way. While the subsurface area in this vicinity may have been disturbed by past sugar cane cultivation and/or past roadway construction, maps or data to confirm such a finding are not available. However, it is unlikely that items of historical or cultural significance will be affected by the subject project.

7. **Air Quality**

The proposed project is not anticipated to significantly impact air quality conditions at the property or surrounding environs. Future construction activities (relating to site work) may temporarily impact the ambient air quality in the vicinity. Potential impacts will be mitigated through use of appropriate Best Management Practices, including dust barriers, silt fences, waterwagons and/or sprinklers to control dust, and watering graded areas.

On a long-term basis, it is noted that use of the Paia Mini-Bypass is limited to 4:00 p.m. to 6:00 p.m., excluding Saturdays, Sundays and holidays. The proposed action is not anticipated to adversely impact air quality standards in the project vicinity. Moreover, on a regional level, the project should generally reduce afternoon commuting times which would reduce engine idling time and associated emissions.

---

8. **Noise**

Ambient noise conditions may be temporarily affected by construction activities. Heavy construction machinery, such as backhoes, dump trucks, front end loaders, paving equipment, and material transport vehicles, are anticipated to be the dominant noise generating sources during the construction period.

Proper equipment and vehicle maintenance are anticipated to reduce noise levels. Equipment mufflers and other noise attenuating equipment may also be employed as required. All construction activities will be limited to daylight working hours.

Once completed, it is noted that use of the project will be limited to afternoon peak traffic hours, from 4:00 p.m. to 6:00 p.m., excluding weekends and holidays. Thus, any noise impacts resulting from the project will be confined to those time periods and not anticipated to be adverse.

9. **Scenic and Open Space Resources**

The portion of the project within the Hana Highway right-of-way will involve approximately four (4) feet of additional paved shoulder on the makai side and approximately six (6) feet of paved shoulder on the mauka side. The existing shoulders are currently grassed. The one (1) lane roadway behind Paia Town would encompass approximately 32,000 square feet or .73 acre which would preclude the land from being utilized for sugar cane cultivation. While there would be an increase in paved surface, the project should not adversely impact the scenic and open space qualities of Paia Town.

10. **Chemicals and Fertilizers**

Parcel 18 has a prior history of commercial sugar cane cultivation by HC&S with associated fertilizer and pesticide use, in accordance with approved standards of application. The proposed project will involve grassing of roadway shoulders which will involve minimal application of chemicals and fertilizers. However, periodic shoulder grass mowing would be required.

---

**B. IMPACTS TO THE SOCIO-ECONOMIC ENVIRONMENT**

**1. Economy**

The proposed project should represent an incremental improvement in easing traffic congestion which backs up in the afternoon peak hours on Hana Highway. The land use and circulation pattern within Paia Town lends itself to slow-moving traffic with its angled parking on the Wailuku approach on Hana Highway, numerous driveways and pedestrian orientation. The Paia Mini-Bypass is intended to provide an alternative travel route for vehicles desiring to turn right on Baldwin Avenue. Through traffic would still utilize Hana Highway. The intent would be to minimize the travel time through Paia Town while not taking away customers who wish to patronize Paia's businesses. Thus, the proposed project should not result in any adverse economic impact to businesses within Paia Town.

On a short-term basis, the project will provide construction employment.

**2. Agriculture**

A total of 32,000 square feet or .73 acre of State-designated agricultural lands are affected by the subject project.

Although the action will result in a reduction of the overall agricultural lands on the island, the loss of agricultural lands is relatively small, and not anticipated to constitute an adverse impact to the sugar cane productivity of HC&S.

**3. Police, Fire and Medical**

The development of the Paia Mini-Bypass should not impact the service capabilities of police, fire, and emergency medical operations. The project will not extend the existing service area limits for emergency services.

**4. Recreational and Educational Resources**

The proposed project is not expected to generate a need for recreational facilities. In addition, there are no anticipated impacts to

---

existing educational facilities or resources. The project should have no negative impacts upon recreational or educational parameters.

5. **Solid Waste**

The most recent sugar cane crop has been harvested as of June, 2003. A Solid Waste Management Plan will be developed in coordination with the DPWEM Solid Waste Division for the disposal of clearing and grubbing material from the project site during construction.

C. **IMPACTS TO INFRASTRUCTURE**

1. **Roadways**

A traffic letter report was commissioned by A&B Properties, Inc. in May, 2002 pertaining to an interim Paia By-Pass Road. The report notes that eastbound traffic generally begins to congest at about 3:00 p.m. and deteriorates as the traffic demand increases towards the evening peak hour of traffic. Traffic operation on Hana Highway breaks down through Paia because of side street friction caused by diagonal parking, left turning vehicles and pedestrians. Vehicular traffic at times may come to a complete stop when a motorist is backing out of a diagonal parking stall. In addition, left turning vehicles and jay walking pedestrians also cause traffic to come to a momentary stop. This also leads to inefficient operation of the Baldwin Avenue traffic signal system, as there are times when the Hana Highway green indication is on and there are no eastbound vehicles approaching the intersection. This sends traffic shock waves on Hana Highway back towards Spreckelsville and queues back to near Nonohe Place.

Data collected at that time showed that 2,255 eastbound vehicles approached the Hana Highway/Baldwin Avenue intersection between 3:00 p.m. and 6:00 p.m. Approximately 30 percent of 664 vehicles turned right to proceed mauka on Baldwin Avenue. The intent of the proposal at that time was to provide a one (1) lane bypass road from Hana Highway proceeding mauka near the County of Maui Municipal

---

Parking Lot around existing developed portions of Paia Town and connecting to Baldwin Avenue as a stop controlled intersection.

However, the report noted that this bypass alternative considered at the time will not significantly reduce the length of the moving queue because its access from Hana Highway is located too close to the area of Paia Town that is causing the traffic to congest; namely, the built up area and the area along the diagonal parking. Thus, the report recommended that a parallel access road mauka of Hana Highway be built extending a mile or more out of Paia Town. The report notes that this would result in significant savings of travel time for motorists destined for the Paia Post Office or mauka portions of Paia. Further, the report notes that there should be no opportunity for motorists using this parallel road to re-enter Hana Highway closer to Paia Town. See Appendix C.

It is noted that the current proposed project includes a shoulder lane which is to start near Alawai Road at Baldwin Park approximately 0.53 mile outside of Paia Town. While the length of the storage lane would not be as long as the recommended one mile distance, it represents a significant storage capacity compared to existing conditions. Use of the shoulder lane is proposed to be restricted to the afternoon peak hours by the use of signage. Enforcement on the use of the shoulder lane would be by the Police Department.

An additional traffic assessment was done by Wilson Okamoto & Associates in response to concerns regarding possible impacts of the proposed project upon traffic operations on Baldwin Avenue. The assessment evaluated anticipated traffic operations through Paia Town as a result of the proposed Paia Mini-Bypass. See Appendix C-1.

With the mini-bypass, the assessment noted that there will be a reduction in travel demand at the intersection of Hana Highway and Baldwin Avenue. Thus, traffic signal timing may need to be adjusted to efficiently service the anticipated traffic volumes entering the

---

intersection. With the reduction of traffic signal cycle length, the intersection can function more efficiently. The analysis showed that all of the traffic movements at the intersection of Hana Highway and Baldwin Avenue are expected to operate at LOS "C" or better with the proposed bypass and traffic signal timing modifications.

Queue lengths on the northbound Baldwin Avenue approach of the intersection would be greatly reduced with the traffic signal timing modifications. The queue length reduction may also provide easier ingress and egress to and from parking areas off of Baldwin Avenue, as well as improve existing on-street parking maneuvers.

The proposed bypass connection on Baldwin Avenue is expected to be located near the existing Paia Post Office driveway. However, the critical demand at the intersection is the eastbound right-turn movement from the mini-bypass road to southbound Baldwin Avenue and, as such, would not significantly impact the post office operations. Furthermore, since the mini-bypass road would be located on the opposite side of the post office customer driveway, there should be adequate separation between turning maneuvers. Therefore, from a traffic capacity standpoint, the location of the mini-bypass road connection at Baldwin Avenue should not have a significant impact to the post office driveway operations, provided that sufficient sight distances are provided at the mini-bypass road and Baldwin Avenue intersection.

The assessment concludes that with the modifications to the existing traffic signal timing, the proposed mini-bypass is expected to significantly improve traffic operations along Hana Highway, through Paia Town, and at the intersection of Hana Highway and Baldwin Avenue.

2. **Water**

Water from the County system will be utilized for dust- and erosion-control and irrigation purposes during the construction phase. Where

---

feasible, non-potable water will be utilized for these purposes. In the long term, there will be no hookup to the County water system. Construction plans will be circulated to the Department of Water Supply for review. Water valve covers will be lifted to match the finish grade of the roadway.

3. **Wastewater**

There will be no additional wastewater generation resulting from the project and no impact upon the County wastewater system.

4. **Drainage and Erosion Control**

The proposed development will not alter the natural drainage patterns of the area. Drainage calculations for the portion of the project from the County parking lot to Baldwin Avenue show an increase of 1.17 cfs based on a 50-year, one (1) hour storm. This would have a negligible impact upon the existing holding basin near the County parking lot. For the work within the Hana Highway right-of-way, there would be an increase of 1.6 cfs based on a 50-year, one (1) hour storm. The existing box culvert at the Baldwin Park entrance should have adequate capacity to convey this increase in runoff. See Appendix D.

Although the project will have negligible impacts upon existing drainage, it is noted that this does not preclude the future possibility of working with applicable government agencies such as the Natural Resources Conservation Service on upstream diversion or soil conservation measures in order to minimize runoff, flooding and erosional impacts on a regional basis.

Erosion-control measures are proposed to be implemented. These include limiting the area of clearing and grubbing, sprinkling for dust control, installation of silt screening or fencing, minimizing the construction period, and constructing or installing permanent erosion-control measures as soon as possible. The contractors shall also follow guidelines for Best Management Practices.

---

5. **Electrical and Communication Services**

The project will not affect existing utility poles within the Hana Highway right-of-way. Electrical and communication services will not be affected by the project.

---

#### **IV. RELATIONSHIP TO GOVERNMENTAL PLANS, POLICIES AND CONTROLS**

##### **A. STATE LAND USE DISTRICT**

Pursuant to Chapter 205, Hawaii Revised Statutes, all lands in the State have been placed into one (1) of four (4) land use districts by the State Land Use Commission. These land use districts have been designated "Urban", "Rural", "Agricultural" and "Conservation". The project site, including the Hana Highway and Baldwin Avenue rights-of-way within the vicinity of the project, is designated "Agricultural". See Figure 12. The proposed action to construct roadway improvements within the Hana Highway and Baldwin Avenue rights-of-way, as well as the new one (1) way travel lane are not contrary to "Agricultural" district provisions.

##### **B. GENERAL PLAN OF THE COUNTY OF MAUI**

The General Plan of the County of Maui provides long-term goals, objectives and policies directed toward the betterment of living conditions in the County. Addressed are social, environmental and economic issues which influence both the quantity and quality of growth in Maui County. The following General Plan objectives and policies are addressed by the proposed project.

##### **TRANSPORTATION**

###### **Objective:**

To support an advanced and environmentally sensitive transportation system which will enable people and goods to move safely, efficiently and economically.

###### **Policy:**

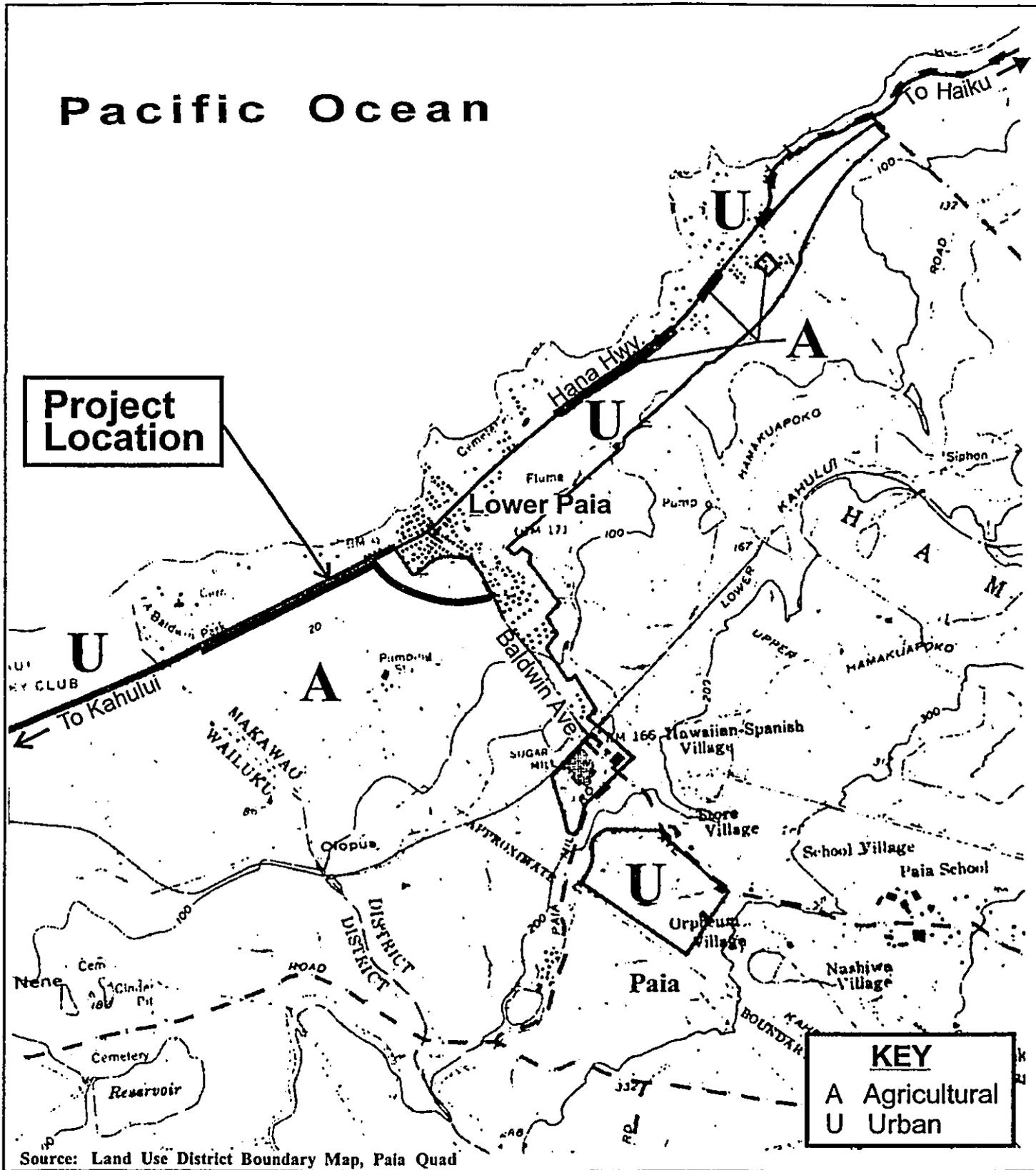
Encourage the development of more efficient water and air transportation.

###### **Objective:**

To develop a program for anticipating and enlarging the local street and highway systems in a timely response to planned growth.

###### **Policy:**

Ensure that transportation facilities are anticipated and programmed for construction in order to support planned growth.



**Figure 12 PAIA MINI-BYPASS  
 State Land Use District Boundaries**



1 in. = 2000 ft.

---

**C. PAIA-HAIKU COMMUNITY PLAN**

The subject project is located in the Paia-Haiku Community Plan region which is one (1) of nine (9) Community Plan regions established in the County of Maui. Planning for each region is guided by the respective Community Plans, which are designed to implement the Maui County General Plan. Each Community Plan contains recommendations and standards which guide the sequencing, patterns, and characteristics of future development in the region.

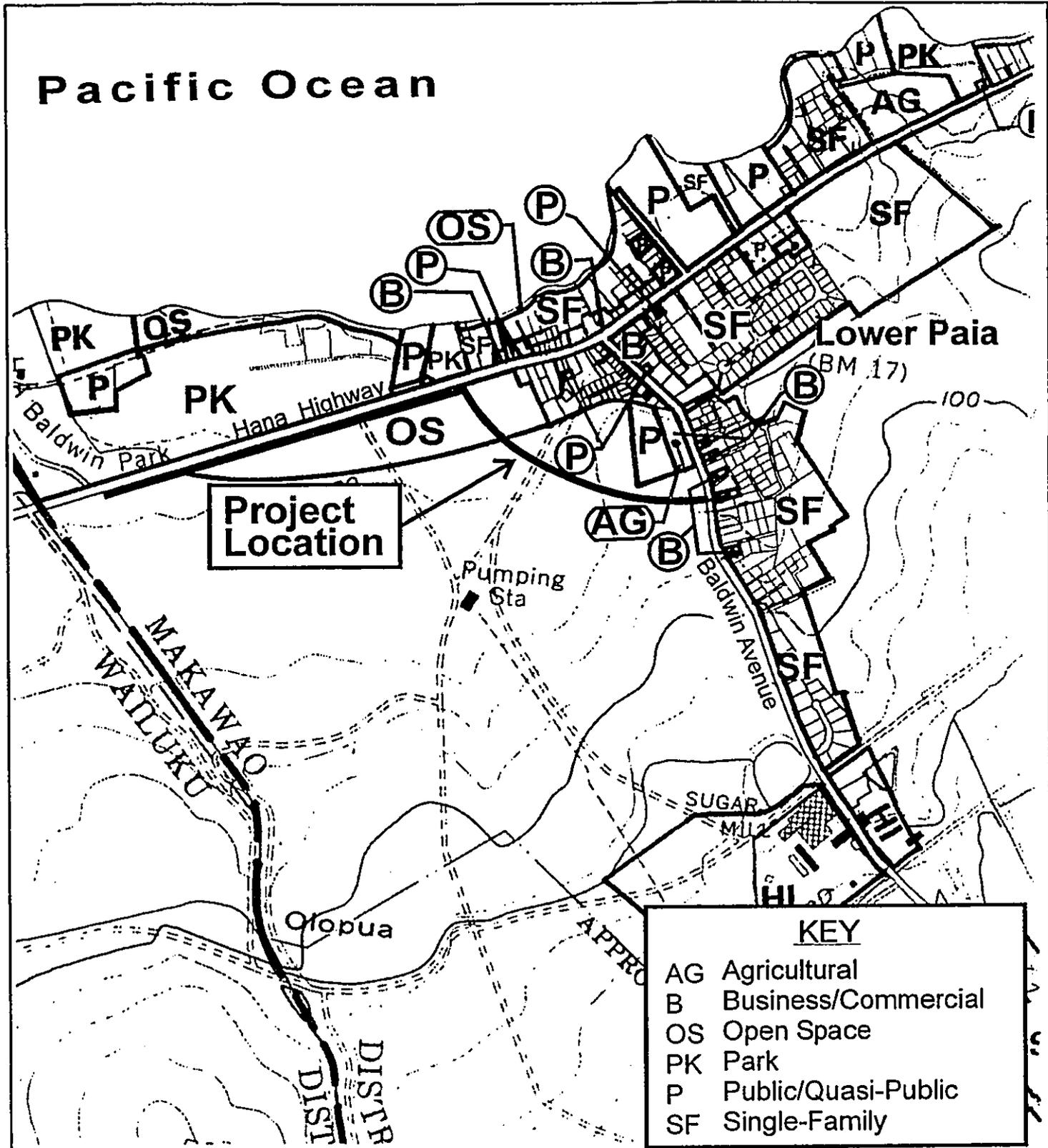
Land use guidelines are set forth in the Paia-Haiku Community Plan Land Use Map. Along the Hana Highway right-of-way, there are "Agricultural" and "Open Space" designations on the mauka side of the road. On the makai side are lands designated as "Park". Near the County parking lot, lands are designated "Open Space" with lands further mauka being designated "Agricultural". Lands adjacent to the Baldwin Avenue terminus are designated "Agricultural". On the east side of Baldwin Avenue are lands designated "Single Family Residential" with several pockets of "Business" designated lands. See Figure 13.

The Paia-Haiku Community Plan recognizes traffic as a key problem to be addressed. The plan is quoted as follows:

Traffic. During peak hours, traffic is heavily congested in Paia Town, notably around the Hana Highway and Baldwin Avenue intersection. This condition is aggravated by the lack of adequate off-street parking in the town and the lack of an alternate roadway to divert traffic along Hana Highway and Baldwin Avenue . . .

Under "Interregional Issues", the Paia-Haiku Community Plan notes the following:

Future alternate roadway. There is an immediate need for an alternate roadway to alleviate congestion in Paia Town and the new proposed residential community at Paia School. This



**Figure 13 PAIA MINI-BYPASS  
Community Plan Land Use Map**



1 in. = 1000 ft.

---

would be subject to consultation with the State Department of Transportation and County Department of Public Works and Waste Management.

The following Community Plan goal, objective, policy and implementing action are addressed by the subject project.

**TRANSPORTATION**

**Goal:**

Transportation systems that facilitate the safe and efficient movement of people, produce and goods within and outside the region.

**Objective and Policy:**

Strongly encourage the coordinated efforts of all appropriate County, State and Federal agencies to plan, fund and construct an alternate route around Paia Town.

**Implementing Action:**

Plan, fund and construct an alternative roadway mauka of Paia Town.

It is noted that the Paia-Haiku Community Plan also proposes a general route for the Paia Bypass Road. The general route would extend from Hana Highway along Sunnyside Road near Kahului Airport, cross Baldwin Avenue between Paia Mill and Paia School, then extend toward Haiku connecting to Hana Highway near Maliko Gulch.

The bypass route shown in the community plan still has numerous planning, design, funding and permitting hurdles remaining before this can become a reality. It is noted that there have been past efforts to implement a Paia Bypass albeit a different alignment. Obtaining community consensus on its implementation was not successful due to concerns relating to possible adverse effects on the town itself.

The proposed mini-bypass is intended to deal with a worsening traffic congestion problem on an interim basis by diverting mauka-bound afternoon peak hour traffic. Through traffic would still be routed along Hana Highway, but traffic congestion should be reduced. It should be emphasized that the mini-bypass is not intended to negate the need for, nor is it intended to delay

---

the implementation of the Paia Bypass. The permanent bypass is a needed facility.

**D. ZONING**

Land encompassed by the subject project from Baldwin Park to Baldwin Avenue is zoned Agricultural. Existing businesses across Baldwin Avenue are zoned Country Town Business (B-CT) with most of the adjacent land being zoned R-3 Residential District.

**E. COASTAL ZONE MANAGEMENT OBJECTIVES AND POLICIES**

The Hawaii Coastal Zone Management Program (HCZMP), as formalized in Chapter 205A, Hawaii Revised Statutes (HRS), establishes objectives and policies for the preservation, protection and restoration of natural resources of Hawaii's coastal zone. As set forth in Chapter 205A, HRS, this section addresses the proposed action's relationship to applicable coastal zone management considerations. It is noted that makai portions of the subject project fall within the County's Special Management Area (SMA). See Figure 14. An application for an SMA Use Permit has been filed with the Maui Planning Department.

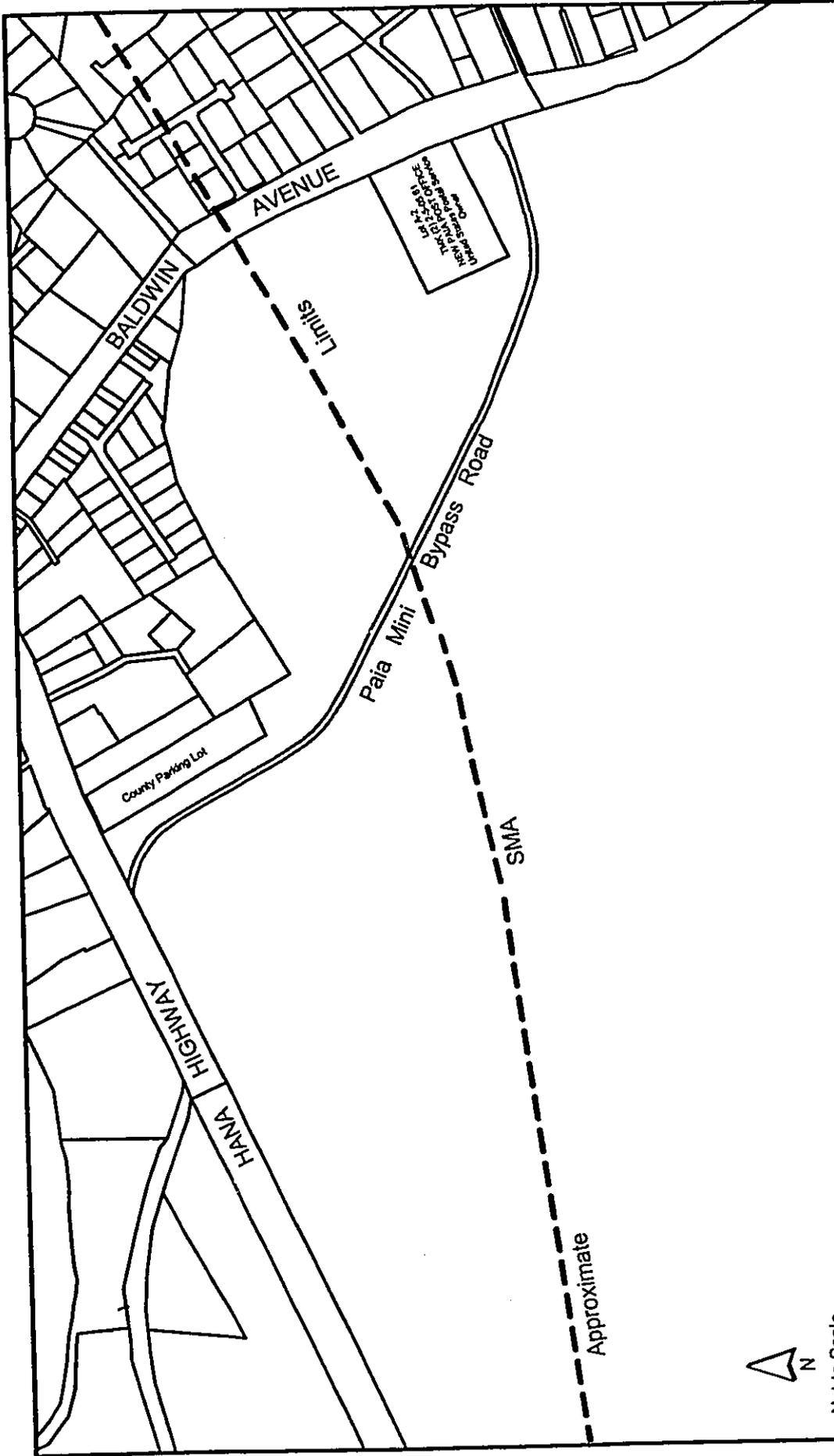
**(1) Recreational Resources**

**Objective:**

Provide coastal recreational opportunities accessible to the public.

**Policies:**

- (A) Improve coordination and funding of coastal recreational planning and management; and
- (B) Provide adequate, accessible, and diverse recreational opportunities in the coastal zone management area by:
  - (i) Protecting coastal resources uniquely suited for recreational activities that cannot be provided in other areas;
  - (ii) Requiring replacement of coastal resources having significant recreational value including, but not limited to, surfing sites, fishponds, and sand beaches, when



**PAIA MINI-BYPASS  
Approximate SMA Limits**

Figure 14



Not to Scale



Not to scale

PLANNING AND COMMUNITY DEVELOPMENT DEPARTMENT

---

such resources will be unavoidably damaged by development; or requiring reasonable monetary compensation to the state for recreation when replacement is not feasible or desirable;

- (iii) Providing and managing adequate public access, consistent with conservation of natural resources, to and along shorelines with recreational value;
- (iv) Providing an adequate supply of shoreline parks and other recreational facilities suitable for public recreation;
- (v) Ensuring public recreational uses of county, state, and federally owned or controlled shoreline lands and waters having recreational value consistent with public safety standards and conservation of natural resources;
- (vi) Adopting water quality standards and regulating point and non-point sources of pollution to protect, and where feasible, restore the recreational value of coastal waters;
- (vii) Developing new shoreline recreational opportunities, where appropriate, such as artificial lagoons, artificial beaches, and artificial reefs for surfing and fishing; and
- (viii) Encouraging reasonable dedication of shoreline areas with recreational value for public use as part of discretionary approvals or permits by the land use commission, board of land and natural resources, and county authorities; and crediting such dedication against the requirements of Section 46-6, HRS.

**Response:** Recreational resources will not be adversely impacted by the construction and operation of the Paia Mini-Bypass. Accessibility to shoreline areas also will not be impacted by the proposed action.

(2) **Historic Resources**

**Objective:**

Protect, preserve and, where desirable, restore those natural and manmade historic and prehistoric resources in the coastal zone management area that are significant in Hawaiian and American history and culture.

**Policies:**

- (A) Identify and analyze significant archeological resources;
- (B) Maximize information retention through preservation of remains and artifacts or salvage operations; and
- (C) Support state goals for protection, restoration, interpretation, and display of historic resources.

**Response:** Prior agricultural activities and roadway construction activities have significantly altered the underlying lands. An

---

archaeological assessment with subsurface testing was conducted for the portion of the proposed roadway traversing former sugar-cane lands. Although no significant material culture remains were found, there are intact marine sand deposits between Hana Highway and a point approximately 450 feet mauka of the highway. Archaeological monitoring will be conducted in this section. In the event that significant archaeological materials are encountered in any portion of the project, the office of the State Historic Preservation Division (SF.HPD) will be notified accordingly in order to determine the appropriate mitigation measures.

(3) **Scenic and Open Space Resources**

**Objective:**

Protect, preserve and, where desirable, restore or improve the quality of coastal scenic and open space resources.

**Policies:**

- (A) Identify valued scenic resources in the coastal zone management area;
- (B) Ensure that new developments are compatible with their visual environment by designing and locating such developments to minimize the alteration of natural landforms and existing public views to and along the shoreline;
- (C) Preserve, maintain, and, where desirable, improve and restore shoreline open space and scenic resources; and
- (D) Encourage those developments that are not coastal dependent to locate in inland areas.

**Response:** Development of the subject project should not result in adverse impacts to scenic and open space resources. The project will not involve significant alteration to the existing topography and will not adversely impact public views to and along the shoreline.

(4) **Coastal Ecosystems**

**Objective:**

Protect valuable coastal ecosystems, including reefs, from disruption and minimize adverse impacts on all coastal ecosystems.

**Policies:**

- 
- (A) Exercise an overall conservation ethic, and practice stewardship in the protection, use, and development of marine and coastal resources;
  - (B) Improve the technical basis for natural resource management;
  - (C) Preserve valuable coastal ecosystems, including reefs, of significant biological or economic importance;
  - (D) Minimize disruption or degradation of coastal water ecosystems by effective regulation of stream diversions, channelization, and similar land and water uses, recognizing competing water needs; and
  - (E) Promote water quantity and quality planning and management practices that reflect the tolerance of fresh water and marine ecosystems and maintain and enhance water quality through the development and implementation of point and nonpoint source water pollution control measures.

**Response:** Appropriate erosion-control measures will be implemented during the construction of the project to prevent significant impacts upon coastal water ecosystems. In the long term, the project will not result in any significant increase in peak flow rates and runoff volumes. The completion of the proposed project will not significantly disrupt or impact coastal ecosystems.

(5) **Economic Uses**

**Objective:**

Provide public or private facilities and improvements important to the State's economy in suitable locations.

**Policies:**

- (A) Concentrate coastal dependent development in appropriate areas;
- (B) Ensure that coastal dependent development such as harbors and ports, and coastal related development such as visitor facilities and energy generating facilities, are located, designed, and constructed to minimize adverse social, visual, and environmental impacts in the coastal zone management area; and
- (C) Direct the location and expansion of coastal dependent developments to areas presently designated and used for such developments and permit reasonable long-term growth at such areas, and permit coastal dependent development outside of presently designated areas when:
  - (i) Use of presently designated locations is not feasible;
  - (ii) Adverse environmental effects are minimized; and
  - (iii) The development is important to the State's economy.

---

**Response:** The project would result in short-term economic benefit due to construction-related employment. In the long term, the project should not have an adverse impact upon existing businesses in Paia Town. Drivers whose destinations are Central and Upper Paia would tend to utilize the mini-bypass while through traffic would still utilize Hana Highway. Since the mini-bypass would be open for two (2) hours during weekdays, excluding holidays, its intent is to reduce the long afternoon queue while still funneling most of the traffic through the town.

(6) **Coastal Hazards**

**Objective:**

Reduce hazard to life and property from tsunami, storm waves, stream flooding, erosion, subsidence and pollution.

**Policies:**

- (A) Develop and communicate adequate information about storm wave, tsunami, flood, erosion, subsidence, and point and nonpoint source pollution hazards;
- (B) Control development in areas subject to storm wave, tsunami, flood, erosion, hurricane, wind, subsidence, and point and nonpoint pollution hazards;
- (C) Ensure that developments comply with requirements of the Federal Flood Insurance Program; and
- (D) Prevent coastal flooding from inland projects.

**Response:** The proposed project will not adversely affect coastal hazard considerations. During future construction activities, erosion-control measures will be incorporated to minimize soil loss and erosion hazards. No adverse drainage impacts to downstream or adjacent properties are anticipated as a result of implementation of the project. Hana Highway is the mauka limit of the coastal high hazard zone.

(7) **Managing Development**

**Objective:**

Improve the development review process, communication, and public participation in the management of coastal resources and hazards.

---

**Policies:**

- (A) Use, implement, and enforce existing law effectively to the maximum extent possible in managing present and future coastal zone development;
- (B) Facilitate timely processing of applications for development permits and resolve overlapping of conflicting permit requirements; and
- (C) Communicate the potential short and long-term impacts of proposed significant coastal developments early in their life cycle and in terms understandable to the public to facilitate public participation in the planning and review process.

**Response:** In compliance with the requirements of Chapter 343, Hawaii Revised Statutes, this environmental assessment has been prepared to facilitate public review and comment. Public participation opportunities will be provided through the SMA permit review process, including a Maui Planning Commission public hearing.

(8) **Public Participation**

**Objective:**

Stimulate public awareness, education, and participation in coastal management.

**Policies:**

- (A) Promote public involvement in coastal zone management processes;
- (B) Disseminate information on coastal management issues by means of educational materials, published reports, staff contact, and public workshops for persons and organizations concerned with coastal issues, developments, and government activities; and
- (C) Organize workshops, policy dialogues, and site-specific mediations to respond to coastal issues and conflicts.

**Response:** As previously noted, public awareness of the project is being promoted through the environmental assessment and County development review process. The subject request is not contrary to the objectives of public awareness, education and participation.

(9) **Beach Protection**

**Objective:**

Protect beaches for public use and recreation.

---

**Policies:**

- (A) Locate new structures inland from the shoreline setback to conserve open space, minimize interference with natural shoreline processes, and minimize loss of improvements due to erosion;
- (B) Prohibit construction of private erosion-protection structures seaward of the shoreline, except when they result in improved aesthetic and engineering solutions to erosion at the sites and do not interfere with existing recreational and waterline activities; and
- (C) Minimize the construction of public erosion-protection structures seaward of the shoreline.

**Response:** The proposed project does not involve any construction work seaward of the shoreline setback and will have no significant effect on beaches in the region.

(10) **Marine Resources**

**Objective:**

Promote the protection, use, and development of marine and coastal resources to assure their sustainability.

**Policies:**

- (A) Ensure that the use and development of marine and coastal resources are ecologically and environmentally sound and economically beneficial;
- (B) Coordinate the management of marine and coastal resources and activities to improve effectiveness and efficiency;
- (C) Assert and articulate the interests of the State as a partner with federal agencies in the sound management of ocean resources within the United States exclusive economic zone;
- (D) Promote research, study, and understanding of ocean processes, marine life, and other ocean resources in order to acquire and inventory information necessary to understand how ocean development activities relate to and impact upon ocean and coastal resources; and
- (E) Encourage research and development of new, innovative technologies for exploring, using, or protecting marine and coastal resources.

**Response:** The proposed project is not anticipated to adversely affect marine and coastal resources. The project will not result in any significant increase in peak flow rates and runoff volumes.

---

**F. NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM**

It is noted that National Pollutant Discharge Elimination System (NPDES) permit coverage applies to the subject project. Necessary permit applications will be coordinated with the Department of Health.

---

**V. SUMMARY OF ADVERSE ENVIRONMENTAL EFFECTS WHICH CANNOT BE AVOIDED**

The Paia Mini-Bypass is not anticipated to result in adverse environmental effects. The future development of the project will result in unavoidable construction-related impacts, as described in Chapter III, Potential Impacts and Mitigation Measures.

Potential effects include noise generated impacts occurring from site preparation and construction activities. In addition, temporary impacts to air quality associated with dust generated from construction activities, as well as exhaust emissions discharged by construction equipment may result. However, these impacts will be mitigated through the use of appropriate BMPs, including use of silt fences to limit runoff, spraying to control fugitive dust, routine maintenance of emissions reducing systems and use of noise attenuating devices. There are no significant long-term adverse environmental effects anticipated with the implementation of the proposed project.

## **VI. ALTERNATIVES TO THE PROPOSED ACTION**

### **No Action Alternative**

The no action alternative would result in the continuation of the long queue of traffic in the afternoon peak hours. Traffic starts to deteriorate around mid-afternoon with peak backup occurring around 4:00 p.m. to 6:00 p.m. Traffic normally backs up to Ulupua Place in Spreckelsville, a distance of 1.2 miles from Paia Town. The backup can range as far as Stable Road, a distance of about 2.2 miles from Paia. With no action, it can be anticipated that the queue will likely get longer over time.

Moreover, driver impatience with afternoon peak hour traffic has led to other undesirable consequences. Some individuals have chosen to drive through Spreckelsville's neighborhood streets in an attempt to move up in the queue leading to Paia Town. This has led to safety issues within the neighborhoods and exacerbates driver irritation.

Traffic congestion has not left a positive impression upon local residents, as well as tourists and visitors alike. With worsening congestion over time, this scenario is not beneficial to the continuation of Paia's small town ambiance. Undue traffic congestion can also be considered as a negative factor in the perception of Paia or Haiku as a desirable place to live, play or do business.

### **Use of Kala/Sunnyside Road as a Two (2) Lane Road**

This alternative would involve use of an existing roadway which extends approximately 3.7 miles from the Kala Road intersection on Hana Highway to its intersection with Baldwin Avenue mauka of the Paia Mill. This alternative explores the use of an existing roadway as a temporary bypass or as a first phase of the permanent bypass. See Appendix E.

The first section of Kala Road extending from Hana Highway to its intersection with Spreckelsville Road (also known as Camp One Road) is considered an old government road. A connecting section of Spreckelsville Road (Camp One Road) extending from Hana Highway to its intersection with Kala Road) is also an old government road. Through an exchange deed, the subject roadways were

---

conveyed to the Territory of Hawaii by Hawaiian Commercial & Sugar Company, Ltd. (HC&S) in 1935. The land area involved is 11.94 acres. The right-of-way width is 40 feet. At this juncture, the State of Hawaii has not made a determination as to ownership of the road. There is no record of jurisdictional transfer of the road to the County of Maui, nor is there any record of conveyance and/or acceptance by the Maui County Council.

The remaining section of the road to Paia Town is named Sunnyside Road. Some older maps also refer to this section of road as Spreckelsville Road. The Maui County Council, through Resolution No. 350, dated November 3, 1949, abandoned and closed for public use, Spreckelsville Road, from its junction of Camp One Road to its intersection with Baldwin Avenue.

However, upon further review of the documents, it appears that some maps inadvertently showed Spreckelsville Road as Sunnyside Road. Thus, the Draft EA concluded that County Resolution No. 350 applied to Sunnyside Road. However, Resolution No. 350 does not apply. It apparently refers to another right-of-way which existed in the general vicinity of Hana Highway prior to the establishment of the current highway. The State of Hawaii has not made any claims on Sunnyside Road. There are also no known records of any conveyance and/or acceptance of Sunnyside Road by the County of Maui.

HC&S currently utilizes Sunnyside Road for its plantation operations. Although the Paia Mill has been closed, Sunnyside Road is part of the plantation's main roadway network which links its fields to the Puunene Mill. It is currently paved with oil and gravel approximately 40 to 50 feet in width.

Should the use of Kala/Sunnyside Road be desired, there are a number of issues which need to be resolved.

First, there are issues relating to safety and liability which need to be addressed. HC&S uses the road for its heavy equipment and truck traffic, as well as the cane haulers which transport sugar cane from the fields to the Puunene Mill. Because of safety and liability, HC&S has developed and maintained a roadway network separate from public roadways. Should the public use of Sunnyside Road be

---

desired, a separate plantation roadway would need to be planned and implemented. Cost and impacts of establishing this new road should be evaluated.

Second, an engineering analysis would need to be done to assess whether the existing road would meet acceptable standards for public safety. For example, the safety of the Kala Road-Hana Highway intersection needs to be examined. The Kala Road approach to Hana Highway is not at 90 degrees. Moreover, the intersection is located near the crest of a downslope on Hana Highway which limits sight distance. The intersection does not conform to current engineering standards and needs to be evaluated. Traffic studies would need to be done to determine appropriate control measures at the Kala Road-Hana Highway intersection, as well as the Sunnyside Road-Baldwin Avenue intersection.

Since Kala/Sunnyside Road traverses sugar cane fields, provisions also need to be made for plantation crossings. There may need to be traffic signals or underpasses considered. Moreover, the effect of a new public road through agricultural operations also needs to be considered.

To bring the road up to State or County standard which would be acceptable for public use, the appropriate base course and asphalt would need to be constructed. A general collector road standard is two (2) 12 foot travel lanes with six (6) foot wide paved shoulders.

Removal of the existing base course, asphalt and oil and gravel also must be done. The exact nature of the sub-base can be determined with the engineering analysis. In the case of Kala Road, there are also existing Elephant-Ear trees which line the road. Over the years, the roots have uplifted the road. Thus, in addition to removing the base course and asphalt, there is the issue of how to deal with the existing trees and root systems.

Drainage issues would need to be addressed as part of the engineering analysis. There are several drainageways which traverse the 3.7 mile long roadway, including the Kailua Gulch. Solutions to address these issues may involve construction of drainage culverts or bridges.

---

Third, planning issues have not been resolved as well. A new 3.7 mile roadway would require public review through an Environmental Assessment or Environmental Impact Statement. Other applicable discretionary and ministerial permits also need to be researched. Broader regional issues relating to route selection and alternatives analyses have not been thoroughly disclosed. This would include evaluating whether there will be a proposed Paia Bypass extending from Baldwin Avenue toward Haiku eventually connecting to Hana Highway. Assuming construction proceeds on the Kala/Sunnyside Road alternative, this would mean that any connection of the Haiku leg of the Bypass would need to be at the Paia Mill location. Rather than proceeding on a portion of the bypass without consideration of future segments, a comprehensive analysis should be done first. It should also be noted that any permanent Paia Bypass project would need to discuss possible impacts upon Paia Town and its business and residential community. Resolution of the foregoing issues would likely be a multi-year process.

Fourth, land acquisition issues need to be resolved. At a minimum, a 60 foot wide right-of-way would need to be acquired along Sunnyside Road. Along Kala Road, since the old government road right-of-way is 40 feet, an additional 20 feet of right-of-way is required. Sunnyside Road is approximately 16,236 lineal feet. If a 60 foot wide right-of-way is needed, that would total 974,160 square feet. Kala Road is about 3,300 lineal feet. If an additional 20 feet of right-of-way is needed, that would total 66,000 square feet. The total square footage (1,040,160 square feet) is approximately 24 acres. In addition to costs to purchase or condemn the land, there would be unspecified severance damages to compensate HC&S for its current use of the road.

Fifth, since substantial time and expenditures will be involved, it is likely that Federal funds will be sought for the construction of this road. This will involve a multi-year process which can substantially decrease the amount of State or County government's financial participation. Typically, the Federal government would pay 80 percent of the construction cost with the State or County paying the remainder. It is noted that Federal participation has not been secured.

Based on other roadways which have recently been constructed on the island of Maui, a rough rule of thumb for total project cost is approximately \$7-\$10 million per

---

mile. For 3.7 miles, total estimated roadway cost could range from \$25.9 million to \$37 million.

A rough timetable to implement this alternative would be six (6) to eight (8) years assuming there are no significant delays. This would include an alternatives analysis and route selection (one [1] to two [2] years), preliminary engineering (one [1] year), detailed design, land acquisition and permits (two [2] to three [3] years), and bid and construction (two [2] years).

While the Kala/Sunnyside Road alternative may hold promise for resolving traffic congestion, there are a number of unresolved issues which would relegate this alternative to a possible long-term solution. It would instead be more appropriate to consider the foregoing issues in the route selection, planning and design for a permanent Paia Bypass.

#### **Use of Kala/Sunnyside Road as a One (1) Lane Road**

This alternative involves the use of Kala/Sunnyside Road as a one (1) lane road. This would involve use of the road as a one (1) lane one (1) way thoroughfare which could be opened in the afternoon peak hours.

Many of the factors which are taken into consideration for a two (2) lane road also apply to a one (1) lane roadway, except as discussed below.

Safety and liability still need to be addressed. Joint use of the road between the HC&S and the public at the same time is still not possible or desirable. However, an alternative could involve public use of the road during afternoon peak hours with HC&S use during the remainder of the day. Since public use would be limited to afternoon peak hours, the issue of impacts to agricultural operations would need to be addressed. Currently, when harvesting occurs, transport of the sugar cane to the mill is done 24 hours a day. The interruption of sugar cane harvesting and mill operations for at least two (2) hours during afternoon peak hours would have to be evaluated.

Moreover, another issue involves clean up of the road every day. HC&S equipment and vehicles will likely involve vehicular trips to and from the sugar cane fields which

---

would leave dirt, mud and agricultural debris on the roadway. Costs of street sweeping and cleaning the road on a daily basis would need to be considered. If the road is used by the public and HC&S, albeit at different times of the day, it may be difficult to assign responsibility for road patching and repair.

Because of safety, liability and maintenance concerns, a separate plantation roadway would need to be implemented.

An engineering analysis would still be required. Safety issues concerning the Kala Road-Hana Highway intersection could perhaps be addressed with a separate right-turn lane on the Hana Highway approach. However, traffic studies would still be required. If a separate plantation road is being built, then plantation crossings or underpasses would need to be built.

Materials and construction cost for a 12 foot wide travel lane and three (3) foot shoulders should be approximately half of the two (2) lane version, not including removal of the existing base course, asphalt and oil and gravel. Drainage concerns would still need to be addressed.

Planning issues also need to be resolved. An Environmental Assessment or an Environmental Impact Statement would be required. The interface of this one (1) lane road with the permanent Paia Bypass Road would need to be discussed. The issue of whether the one (1) lane roadway is part of the permanent Paia Bypass route or whether there will be a new permanent Paia Bypass alignment should be discussed. The effect of a new road and its impact upon agricultural operations also needs to be addressed.

Land acquisition will also need to be resolved. Rough acquisition costs could be approximately half of the two (2) lane alternative. Severance damages also need to be paid.

With regard to funding of the road, it is questionable whether Federal funds can be utilized since one (1) lane roads do not meet collector or arterial roadway standards. Thus, State and/or County participation would have to be secured. A rough rule of thumb estimate for total project cost would be roughly half of the two (2) lane estimate or \$12.95 million to \$18.5 million.

---

Because of the substantial amounts of monies which would be needed to bring a one (1) lane road up to standard, the regional issues as to interface with a permanent Paia Bypass Road should be considered. As such, the timetable to implement this alternative would be roughly the same (six [6] to eight [8] years) as the two (2) lane alternative.

Because of the planning, design, acquisition, and funding issues, this alternative is also a multi-year long term proposition. Moreover, it is a costly alternative with a number of key costs which are unknown until more detailed engineering analysis is completed.

**Proposed Action**

The proposed action involves implementation of a shoulder lane within the Hana Highway right-of-way from Baldwin Park to the County parking lot, with a one (1) way one (1) lane road extending mauka around Paia Town connecting to Baldwin Avenue near the Paia Post Office. This is a distance of 0.9 mile.

This alternative has a minimal impact on agricultural operations since it involves land at the edge of Paia Town. The one (1) lane roadway through A&B's lands would remain under private ownership, but public use would be limited to afternoon peak hours. There would be no joint use of the roadway with HC&S. The design and engineering work has been completed. The Environmental Assessment and Special Management Area Use application are in process. While there are no Federal funds involved in this alternative, funding is in place with joint contributions from the State of Hawaii Department of Transportation, A&B Properties, Inc., and the County of Maui. The total cost is approximately \$650,000. Moreover, assuming the EA and SMA application can be approved in a timely manner, construction of the project could begin by the second quarter of 2005. Thus, this alternative can be implemented in a very short time frame.

The proposed action is clearly an interim solution to traffic issues in the region. The project will divert approximately 30 percent of the traffic from Hana Highway which will minimize traffic congestion during afternoon peak hours. It can be done relatively quickly, at a reasonable cost, and will have minimal adverse impact upon Paia's businesses and residents. It should also be noted that the discussion of a

---

permanent Paia Bypass is still a major project which can be addressed separately in a different time frame.

---

## **VII. IRREVERSIBLE AND IRRETRIEVABLE COMMITMENT OF RESOURCES**

The construction of the proposed improvements would involve the commitment of land, labor, funding and material resources for the proposed action. The State of Hawaii is in the process of planning for the permanent Paia Bypass Highway. While it is likely that the mini-bypass improvements within the Hana Highway right-of-way will remain for the long term, the one (1) lane roadway extending from the County parking lot to Baldwin Avenue will likely remain in use at least until the permanent Paia Bypass Highway is done, or until it is not warranted to address regional transportation needs.

---

## VIII. FINDINGS AND CONCLUSIONS

The "Significance Criteria", Section 12 of the Administrative Rules, Title 11, Chapter 200, "Environmental Impact Statement Rules", were reviewed and analyzed to determine whether the proposed action will have significant impacts to the environment. The following analysis is provided:

1. **No Irrevocable Commitment to Loss or Destruction of any Natural or Cultural Resources Would Occur as a Result of the Proposed Project**

There are no anticipated adverse environmental impacts as a result of the proposed project. There are no identified rare, threatened or endangered species in the vicinity of the project site.

The project will be constructed on fill material within the Hana Highway right-of-way. The portion of the project from Hana Highway to Baldwin Avenue has been significantly altered through many years of sugar cane cultivation. Archaeological monitoring will be conducted in the area extending from Hana Highway to a point approximately 450 feet mauka of the highway. Minor excavation is proposed within the Baldwin Avenue right-of-way. Adverse archaeological impacts related to the construction of Parcel 18 are not anticipated. However, should any evidence of human remains be encountered during project construction, work will stop immediately in the vicinity of the find and the State Historic Preservation Division will be consulted to establish an appropriate mitigation strategy.

2. **The Proposed Action Would Not Curtail the Range of Beneficial Uses of the Environment**

The proposed project and its commitment of land are not anticipated to curtail the range of beneficial uses of the environment.

3. **The Proposed Action Does Not Conflict with the State's Long-Term Environmental Policies or Goals or Guidelines as Expressed in Chapter 344, Hawaii Revised Statutes**

The State's Environmental Policy and Guidelines are set forth in Chapter 344, HRS, and were reviewed in connection with the proposed action. The

---

proposed action is in consonance with the State's long-term environmental policies and goals of Chapter 344, HRS.

4. **The Economic or Social Welfare of the Community or State Would Not Be Substantially Affected**

The proposed project provides a transportation enhancement which should not substantially affect the economic or social welfare of Paia Town.

5. **The Proposed Action Does Not Affect Public Health**

Adverse impacts to the public health and welfare are not anticipated as a result of the proposed action.

6. **No Substantial Secondary Impacts, Such as Population Changes or Effects on Public Facilities are Anticipated**

The construction of the proposed project should have a negligible effect on the island's population base. The proposed project should have a beneficial effect on the area's roadways. There will be no effects on the County's water and wastewater systems. The proposed project will not alter the natural drainage patterns and will not result in significant increases in peak flow rates and runoff volumes. The project is not expected to adversely impact public services such as police, fire and medical services. Impacts upon recreational, educational and solid waste parameters are also negligible.

7. **No Substantial Degradation of Environmental Quality is Anticipated**

The proposed project will not result in adverse degradation to the environment. Future construction will require the design and implementation of BMPs to mitigate construction activity impacts. In the long term, adverse impacts upon air quality and noise parameters are anticipated to be minimal. The proposed project is not anticipated to significantly affect the open space and scenic character of the area.

---

8. **The Proposed Project Does Not Involve a Commitment to Larger Actions, Nor Would Cumulative Impacts Result in Considerable Effects on the Environment**

The proposed project does not entail a commitment to larger actions. However, if a permanent Paia Bypass project is presented in the future, a full and separate environmental review will have to be undertaken.

9. **No Rare, Threatened or Endangered Species or Their Habitats Would Be Adversely Affected by the Proposed Action**

There are no identified rare, threatened or endangered species located within the vicinity of the subject project. Further, approval of the project will not adversely impact local habitats.

10. **Air Quality, Water Quality or Ambient Noise Levels Would Not Be Detrimentially Affected by the Proposed Project**

Development of the subject project will not adversely affect air, water or noise parameters.

11. **The Proposed Project Would Not Affect Environmentally Sensitive Areas, Such as Flood Plains, Tsunami Zones, Erosion-prone Areas, Geologically Hazardous Lands, Estuaries, Fresh Waters or Coastal Waters**

The proposed project would not affect environmentally sensitive areas. As such, construction of the Paia Mini-Bypass will not adversely affect flood plains, tsunami zones, erosion-prone areas, hazardous lands, estuaries, fresh waters or coastal waters.

12. **The Proposed Action Would Not Substantially Affect Scenic Views and Viewplanes Identified in County Plans or Studies**

The proposed project site is not part of an identified scenic view corridor or viewplane. As such, adverse impacts to coastal and scenic views are not anticipated.

---

13. **The Proposed Action Would Not Require Substantial Energy Consumption**

The proposed project is not anticipated to require substantial energy consumption. Moreover, since the project is anticipated to cut commuting time, this would result in savings in the use of gasoline and energy.

Based on the foregoing findings, it is concluded that the proposed action will not result in any significant impacts.

**IX. AGENCIES CONSULTED DURING THE PREPARATION OF THE DRAFT ENVIRONMENTAL ASSESSMENT; LETTERS RECEIVED AND RESPONSES TO SUBSTANTIVE COMMENTS**

The following agencies were consulted during the preparation of the Draft Environmental Assessment. Agency comments and responses to substantive comments are also included in this section.

1. Mr. Neal Fujiwara, Soil Conservationist 8.  
Natural Resources Conservation Service  
U.S. Department of Agriculture  
210 Imi Kala Street, Suite 209  
Wailuku, Hawaii 96793-2100
2. Mr. William Lennan  
Department of the Army  
U.S. Army Engineer District, Honolulu  
Attention: Operations Division  
Bldg. T-1, Room 105  
Fort Shafter, Hawaii 96858-5440
3. Mr. Jerry Leinecke  
Pacific Islands Manager  
U. S. Fish and Wildlife Service  
P.O. Box 50167  
Honolulu, Hawaii 96850
4. Mr. Laurence Lau, Deputy Director  
State of Hawaii  
Department of Health  
P.O. Box 3378  
Honolulu, Hawaii 96801
5. Mr. Herbert Matsubayashi  
District Environmental Health Program  
Chief  
State of Hawaii  
Department of Health  
54 South High Street  
Wailuku, Hawaii 96793
6. Mr. Michael W. Foley, Planning Director  
County of Maui  
Department of Planning  
250 South High Street  
Wailuku, Hawaii 96793
7. Ms. Phyllis P. "Holly" McEldowney  
Administrator  
State of Hawaii  
Department of Land and Natural Resources  
State Historic Preservation Division  
601 Kamokila Boulevard, Room 555  
Kapolei, Hawaii 96707
8. Mr. Clyde Namuo  
Office of Hawaiian Affairs  
711 Kapiolani Boulevard, Suite 500  
Honolulu, Hawaii 96813
9. Carl Kaupalolo, Fire Chief  
County of Maui  
Department of Fire and Public Safety  
200 Dairy Road  
Kahului, Hawaii 96732
10. Mr. Glenn Correa, Director  
County of Maui  
Department of Parks and Recreation  
1580-C Kaahumanu Avenue  
Wailuku, Hawaii 96793
11. Mr. Thomas Phillips, Chief of Police  
County of Maui  
Maui Police Department  
55 Mahalani Street  
Wailuku, Hawaii 96793
12. Mr. George Tengan, Director  
County of Maui  
Department of Water Supply  
200 South High Street  
Wailuku, Hawaii 96793
13. Paia Main Street Association  
P. O. Box 995  
Paia, Hawaii 96779-0995

United States  
Department of  
Agriculture



Natural  
Resources  
Conservation  
Service

210 Iml Kala St.  
Suite 209  
Wailuku, HI 96793

04 JAN 22 P 2:41

*Our People...Our Islands...In Harmony*

COUNTY OF MAUI  
PUBLIC WORKS

DATE: January 20, 2004

206  RUSH

Mr. Gilbert S. Coloma-Agaran, Director  
Department of Public Works & Environmental Mgt.  
County of Maui  
200 S. High Street  
Wailuku, Hawaii 96793

Dear Mr. Coloma-Agaran,

SUBJECT: Paia Mini-Bypass Project, Draft EA

We would like to recommend that coordination be made with Hawaiian Commercial & Sugar Company regarding any drainage concerns from mauka fields. A diversion may need to be designed and constructed above the proposed by-pass project.

Thank you for allowing us to comment.

Sincerely,

*Neal S. Fujiwara*  
Neal S. Fujiwara  
District Conservationist

c. Mr. Doug MacCluer, Chair, CMSWCD

DEPT. OF PUBLIC WORKS	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
DEP. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Return to \_\_\_\_\_ Due \_\_\_\_\_  
Ref. No. \_\_\_\_\_  
By: \_\_\_\_\_ Date: \_\_\_\_\_

ALAN M. ARAKAWA  
Mayor

GILBERT S. COLOMA-AGARAN  
Director

MILTON M. ARAKAWA, A.I.C.P.  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

LLOYD P.C.W. LEE, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

JOHN D. HARDER  
Solid Waste Division

February 24, 2004

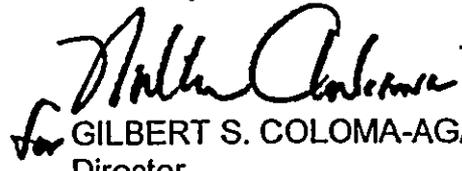
Mr. Neal S. Fujiwara  
District Conservationist  
Natural Resources Conservation Service  
210 Imi Kala Street, Suite 209  
Wailuku, Maui, Hawaii 96793

Dear Mr. Fujiwara:

We have received your letter of January 20, 2004 relating to early consultation comments on the Paia Mini-Bypass project. The Environmental Assessment will examine pre- and post-development drainage conditions, as well as potential impacts and mitigative measures. Drainage issues will be discussed with Hawaiian Commercial & Sugar Company regarding possible upstream diversions.

Thank you for your comments. If you have any questions, please call Milton Arakawa at 270-7845.

Sincerely,

  
GILBERT S. COLOMA-AGARAN  
Director

GSCA:MA:jso

xc: Mercer Vicens, Vice President, A&B Properties, Inc.  
Hideo Kawahara, Manager, Construction & Engineering, A&B Properties, Inc.  
Ferdinand Cajjal, District Engineer, Department of Transportation, Highways  
Division-Maui  
s:\milton\Fujiwara-NRCS-Paia Mini-Bypass resp



DEPARTMENT OF THE ARMY  
U. S. ARMY ENGINEER DISTRICT, HONOLULU  
FT. SHAFTER, HAWAII 96858-5440

RUSH

REPLY TO  
ATTENTION  
JAN 22 P2:40

January 20, 2004

Regulatory Branch  
COUNTY OF MAUI  
PUBLIC WORKS

Mr. Gilbert S. Coloma-Agaran, Director  
Department of Public Works and  
Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

DEPT. OF PUBLIC WORKS	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
DEP. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_

By \_\_\_\_\_ Date: \_\_\_\_\_

Dear Mr. Coloma-Agaran:

This letter responds to your request for comments on the proposed Paia Mini-Bypass Project environmental assessment (EA) preparation notice, dated January 14, 2004. Based on the information you provided I am unable to determine if a Department of the Army (DA) permit will be required for this project. Please include information in the EA concerning the presence or absence of streams or other water bodies or wetlands on the road alignment, and if present, what effect the project will have on them. Please place us on the mailing list for the draft EA.

If you have any questions concerning this matter, please contact Mr. William Lennan of my staff at (808) 438-6986 or by FAX (808) 438-4060, and reference File No. 200400133.

Sincerely,

for George P. Young, P.E.  
Chief, Regulatory Branch

ALAN M. ARAKAWA  
Mayor  
GILBERT S. COLOMA-AGARAN  
Director  
MILTON M. ARAKAWA, A.I.C.P.  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

LLOYD P.C.W. LEE, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

JOHN D. HARDER  
Solid Waste Division

February 24, 2004

Mr. George P. Young, P.E.  
Chief, Regulatory Branch  
Department of the Army  
U. S. Army Engineer District, Honolulu  
Fort Shafter, Hawaii 96858-5440

Dear Mr. Young:

We have received your letter of January 20, 2004 relating to early consultation comments on the Paia Mini-Bypass project. The Environmental Assessment (EA) will include information on streams or other water bodies or wetlands. We will also send you a copy of the Draft EA.

Thank you for your comments. If you have any questions, please call Milton Arakawa at (808) 270-7845.

Sincerely,

  
for GILBERT S. COLOMA-AGARAN  
Director

GSCA:MA:jso

xc: Mercer Vicens, Vice President, A&B Properties, Inc.  
Hideo Kawahara, Manager, Construction & Engineering, A&B Properties, Inc.  
Ferdinand Cajigal, District Engineer, Department of Transportation, Highways  
Division-Maui

s:\milton\GYoung-Army-Paia Mini-Bypass Resp

LINDA LINGLE  
GOVERNOR OF HAWAII



CHIYOME L. FUKINO, M. D.  
DIRECTOR OF HEALTH

LORRIN W. FANG, M. D., M. P. H.  
DISTRICT HEALTH OFFICER

*26/1/04*  
'04 JAN 29 12:06

COUNTY OF MAUI  
PUBLIC WORKS

STATE OF HAWAII  
DEPARTMENT OF HEALTH  
MAUI DISTRICT HEALTH OFFICE  
64 HIGH STREET  
WAILUKU, MAUI, HAWAII 96793-2102

January 22, 2004

*230*  RUSH

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
--	------	--------	--------	----------	------	------

DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
DEP. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
A+B/DOT	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

*forward 1/29/04*

Mr. Gilbert S. Coloma-Agaran  
Director  
Department of Public Works  
And Environmental Management  
200 South High Street  
Wailuku, Hawai'i 96793

Dear Mr. Coloma-Agaran:

Subject: Paia By-Pass Project

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_

By: \_\_\_\_\_ Date: \_\_\_\_\_

Thank you for the opportunity to participate in the early consultation process for the environmental assessment. The environmental assessment should address the following concerns.

- Construction Noise
- Fugitive Dust
- National Pollutant Discharge Elimination System permit coverage

Should you have any questions, please call me at 984-8230.

Sincerely,

Herbert S. Matsubayashi  
District Environmental Health Program Chief

ALAN M. ARAKAWA  
Mayor  
GILBERT S. COLOMA-AGARAN  
Director  
MILTON M. ARAKAWA, A.I.C.P.  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

LLOYD P.C.W. LEE, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

JOHN D. HARDER  
Solid Waste Division

February 24, 2004

Mr. Herbert Matsubayashi  
District Environmental Health Program Chief  
State of Hawaii  
Department of Health  
Maui District Office  
54 South High Street  
Wailuku, Maui, Hawaii 96793

Dear Mr. Matsubayashi:

We have received your letter of January 22, 2004 relating to early consultation comments on the Paia Mini-Bypass project. The Environmental Assessment will address construction noise, fugitive dust, and National Pollutant Discharge Elimination System permit coverage.

Thank you for your comments. If you have any questions, please call Milton Arakawa at 270-7845.

Sincerely,

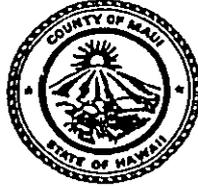
*Milton Arakawa*  
for GILBERT S. COLOMA-AGARAN  
Director

GSCA:MA:jso

xc: Mercer Vicens, Vice President, A&B Properties, Inc.  
Hideo Kawahara, Manager, Construction & Engineering, A&B Properties, Inc.  
Ferdinand Cajigal, District Engineer, Department of Transportation, Highways  
Division-Maui  
s:\milton\hmatsubayashi-doh-paia mini-bypass resp

'04 FEB -4 A9:35

COUNTY OF MAUI  
PUBLIC WORKS



**DEPARTMENT OF WATER SUPPLY**  
COUNTY OF MAUI  
P.O. BOX 1109  
WAILUKU, MAUI, HAWAII 96793-6109  
Telephone (808) 270-7816 • Fax (808) 270-7833

35  RUSH  
DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>					
DEP DIR	<input checked="" type="checkbox"/>					
PERS.						
STAFF CE						
LUCA						
WW RECL.						
SOLID W.						
ENGR.						
HWYS.						
SECTY.						
A+B/DOJ						

Return to \_\_\_\_\_ Due \_\_\_\_\_  
Ref. No. \_\_\_\_\_  
By: \_\_\_\_\_ Date: \_\_\_\_\_

2/4/04

January 28, 2004

Mr. Gilbert S. Coloma-Agaran, Director  
County of Maui  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

Subject: Paia Mini-Bypass Project, Draft Environmental Assessment Early Consultation

Dear Mr. Coloma-Agaran,

Thank you for the opportunity to provide comments in preparation of the Draft Environmental Assessment (DEA).

The Department 8 and 12-inch water lines and fire hydrants are located in the Hana Highway Right-of-Way. Construction plans need to be reviewed by the Department of Water Supply. Water valve covers must be lifted to match the finished grade of the roadway. Enclosed are sections of our water system map pertaining to the project area. Please contact our engineering division at 270-7835 in regard to these issues.

We have attached Best Management Practices (BMPs) for your reference. These are designed to minimize infiltration from all construction activities and related vehicle operations. Additional information is available from the State Department of Health.

In order to conserve water resources, we recommend that non-potable water be used, where feasible, for dust control and any irrigation during and after construction.

Should you have any questions, please contact the Water Resources Planning Division at 270-7199.

Sincerely,

George Y. Tengan  
Director  
emb

cc: Engineering Division

attachments:

Selected BMPs from "Guidance Specifying Management Measures For Sources Of Nonpoint Pollution In Coastal Waters"  
Portions of Department of Water Supply Fire Protection Map 18

C:\WPdocs\EAs EISs\Paia Mini Bypass early DEA.wpd

*By Water All Things Find Life*

ALAN M. ARAKAWA  
Mayor

GILBERT S. COLOMA-AGARAN  
Director

MILTON M. ARAKAWA, A.I.C.P.  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

LLOYD P.C.W. LEE, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

JOHN D. HARDER  
Solid Waste Division

February 24, 2004

Mr. George Y. Tengan, Director  
Department of Water Supply  
County of Maui  
200 South High Street  
Wailuku, Maui, Hawaii 96793

Dear Mr. Tengan:

We have received your letter of January 28, 2004 relating to early consultation comments on the Paia Mini-Bypass project. The Environmental Assessment will discuss raising of water valve covers, Best Management Practices and use of non-potable water for dust control.

Thank you for your comments. If you have any questions, please call Milton Arakawa at 270-7845.

Sincerely,

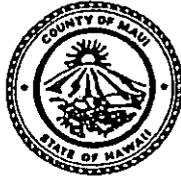
  
GILBERT S. COLOMA-AGARAN  
Director

GSCA:MA:jso

xc: Mercer Vicens, Vice President, A&B Properties, Inc.  
Hideo Kawahara, Manager, Construction & Engineering, A&B Properties, Inc.  
Ferdinand Cajigal, District Engineer, Department of Transportation, Highways  
Division-Maui

s:\milton\gtengan-dws-paia mini-bypass resp

ALAN M. ARAKAWA  
MAYOR



CARL M. KAUPALOLO  
CHIEF

NEAL A. BAL  
DEPUTY CHIEF

'04 FEB -5 P2:55

COUNTY OF MAUI  
PUBLIC WORKS

**COUNTY OF MAUI**  
DEPARTMENT OF FIRE AND PUBLIC SAFETY

200 DAIRY ROAD  
KAHULUI, MAUI, HAWAII 96732  
(808) 270-7561  
FAX (808) 270-7919  
February 5, 2004

326  RUSH

DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
--	------	--------	--------	----------	------	------

Mr. Milton Arakawa  
Department of Public Works  
County of Maui  
200 S. High St  
Wailuku, HI

Return to \_\_\_\_\_ Due \_\_\_\_\_

DIRECTOR	<input checked="" type="checkbox"/>					
DEP. DIR.	<input checked="" type="checkbox"/>					
PERS.						
STAFF CE						
LUCA						
WW RECL.						
SOLID W.						
ENGR.						
HWYS.						
SECTY.						
ADD/DO	<input checked="" type="checkbox"/>				<input checked="" type="checkbox"/>	

*Reundo me*

*fixed 2/5/04*

Dear Mr. Arakawa,

I have reviewed the Paia Mini-Bypass project. I have one question to clarify

By: \_\_\_\_\_ Date: \_\_\_\_\_

What will be the total width of the single lane roadway, including the shoulder for emergency vehicles? I was unable to determine the width throughout with the plans that was forwarded to me. You may call me if you have questions at 270-7568.

Sincerely,

Captain Valeriano F. Martin  
Fire Prevention Bureau

ALAN M. ARAKAWA  
Mayor  
GILBERT S. COLOMA-AGARAN  
Director  
MILTON M. ARAKAWA, A.I.C.P.  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

LLOYD P.C.W. LEE, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

JOHN D. HARDER  
Solid Waste Division

February 24, 2004

Captain Valeriano F. Martin  
Fire Prevention Bureau  
County of Maui  
Department of Fire and Public Safety  
200 Dairy Road  
Kahului, Maui, Hawaii 96732

Dear Captain Martin:

We have received your letter of February 5, 2004 relating to early consultation comments on the Paia Mini-Bypass project. The Environmental Assessment will provide information on single-lane roadway. Enclosed is a proposed cross section for your information.

Thank you for your comments. If you have any questions, please call Milton Arakawa at 270-7845.

Sincerely,

  
GILBERT S. COLOMA-AGARAN  
Director

GSCA:MA:jso  
Enclosure

xc: Mercer Vicens, Vice President, A&B Properties, Inc.  
Hideo Kawahara, Manager, Construction & Engineering, A&B Properties, Inc.  
Ferdinand Cajigal, District Engineer, Department of Transportation, Highways  
Division-Maui  
s:\milton\vmartin-fire-paia mini-bypass resp

**X. LETTERS RECEIVED DURING THE DRAFT ENVIRONMENTAL ASSESSMENT PUBLIC COMMENT PERIOD AND RESPONSES TO PUBLIC COMMENTS**

The public comment period for the Draft Environmental Assessment was from March 8, 2004 to April 7, 2004. However, due to requests received from several individuals, the public comment period was extended to April 23, 2004.

It is also noted that an evening public informational meeting was held on March 17, 2004, at the Paia Community Center.

---

List of Agencies/Organizations/Individuals  
Who Provided Comments to the Draft EA

Betty Neary Alberts  
Michele Anderson  
Harold Appleton  
Kathy Barr  
James A. Basco  
Patti Cadiz  
Christopher Caillarec  
Robert Carroll, Councilmember  
Ed Case, U. S. Congressman  
Georgiana Cook  
County of Maui Department of Parks and Recreation  
County of Maui Police Department  
County of Maui Department of Water Supply  
Paul Ehman, Ehman Productions, Inc.  
Jeff Fagerholm  
Karen Faria  
Lloyd Fischel  
David Futch  
Daniel Grantham  
Cindy Hanscam  
Christina Hemming  
Lance Holter  
Jay Jeter  
Henry Kahula, Jr.  
G. Kealoha  
Rebecca Kikumoto  
Deborah Kremins  
George Lee  
Sheila Magers  
Raun Marshall  
Stanley Marshall  
Bob Martin  
R. Bruce Martin  
Martin Family  
Maui Tomorrow  
Cissy McDonald  
Diane McDonald  
Norman & Joanne Mezin  
Sol Morey  
Paia Main Street Association  
Ann Pitcaithley  
Augustin C. Quinsaat, Pauline R. Quinsaat  
Hans Riecke FAIA  
Cary Ritt  
Nick Rusnak  
Smith Development  
Lisa Starr  
State of Hawaii Department of Accounting and General Services  
State of Hawaii Department of Land and Natural Resources

---

State of Hawaii Department of Transportation  
State of Hawaii, Office of Environmental Quality Control  
Gayln Sweet  
Christine Thevenard  
Jack Thompson  
U.S. Department of the Army  
Verna U'u  
Jay Van Zwalenburg  
Jason White  
Calvin Yang, Postmaster Paia

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Ms. Betty Neary Alberts  
P. O. Box 790285  
Paia, Maui, Hawaii 96779

Dear Ms. Neary Alberts:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your facsimile transmittal of March 15, 2004 to Messrs. Ronald Tsuzuki and Ferdinand Cajigal of the State Department of Transportation relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. Since the County Department of Public Works and Environmental Management is the lead agency for the EA, we would like to provide the following response.

We would like to note that a permanent Paia Bypass is a needed facility. The Draft EA discusses the use of Kala Road and Sunnyside Road as an alternative action to the mini-bypass. Kala Road is an old government road which extends from Hana Highway to Sunnyside Road. Sunnyside Road then extends to Baldwin Avenue just mauka of the Paia Mill. Sunnyside Road is a privately owned road which must be purchased if it is desired for public use. There are also a number of other issues which need to be resolved before use of Kala/Sunnyside Road by the public is warranted. These include safety and liability issues relating to the current agricultural use of Sunnyside Road. There are also planning and route selection issues which need to be discussed for the Haiku leg of the bypass. An engineering analysis also needs to be done which discusses drainage, desired cross section of the road, and intersection design. Our analysis showed that it could take approximately six (6) to eight (8) years before the necessary planning, public hearings, environmental review, permits, design, and funding can be obtained in order to address the foregoing issues. Based on other roadways recently constructed on the island of Maui, an estimated roadway cost for the Kala/Sunnyside Road segment (not including a Haiku leg) could range from \$25.9 to \$37 million.

Ms. Betty Neary Alberts  
March 2, 2005  
Page 2

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be started during the second quarter of 2005 and is scheduled to take three (3) months.

There have been a number of concerns raised that the Baldwin Avenue terminus of the mini-bypass makai of the Paia Post Office may conflict with traffic from the Paia Post Office driveway. Thus, we will be revising our plans to move the Baldwin Avenue terminus above or mauka of the Paia Post Office. The Final Environmental Assessment will reflect the change.

Moreover, we have done an additional traffic assessment in order to address traffic impacts on Baldwin Avenue. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The report will be included in the Final Environmental Assessment.

You have also suggested that the Paia Mini-Bypass project is being done to primarily benefit a large landowner. We would like to emphasize that there is no linkage between the Paia Mini-Bypass and possible future projects by any large or small landowner. Any future proposal would be required to be considered under rules and regulations in existence at the time of application.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours. Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-neary alberts

**IN FAVOR OF THE PA'IA-KALA ROAD BYPASS**

TO: State Office DOT - Ronald F. Tsuzuki

'04 MAR 31 A7:26

FAX #: 1-808-587-~~1520~~ 1787

March 15, 2004  
COUNTY OF MAUI  
PUBLIC WORKS

I urge you to promote the immediate adoption of the permanent Pa'ia Bypass (aka. KALA-SUNNYSIDE) as soon as possible. This is long overdue and is what the people want!!! As a kama'aina who grew up in Pa'ia and attended PA'IA AND KAUNOA SCHOOLS, we need a permanent solution to this ever growing traffic situation for the benefit of locals and tourists alike, as well as for the preservation of Pa'ia town, while also helping the shopkeepers and restaurants with their businesses.

The temporary mini by-pass suggested by A & B to join Hana Highway to Baldwin Avenue will cost taxpayers huge, unnecessary expenditures, largely for A & B's benefit.

I own the residence at 137A Baldwin Avenue, Pa'ia, very close to the A&B proposed mini-bypass and across from the Pa'ia Patsy Mink Post Office. Traffic congestion in the morning and at pau hana time already exists here on Baldwin Avenue. More traffic flowing into our local street will mean a more hazardous situation and huge problems for vehicles, pedestrians and school children going to and from school. This plan is not a solution to the Hana Highway traffic delays for Pa'ia, Haiku, and other upcountry residents and visitors alike.

Although I presently work in Washington DC, I am frequently at my Maui residence and bring a perspective both as a long-time local and as a visitor. I have carefully studied this problem for about 10 or more years and have attended Pa'ia/Haiku/Spreckelsville community meetings, have written letters of concern and have talked with a lot of people about this issue. I urge you to do what is right for the community and those most affected by this extremely frustrating situation. Make a good decision for the future of Maui, Pa'ia, Haiku, upcountry and not a short term, expensive one to benefit primarily the large landowner over the people!! Maui No Ka O'e.

Mahalo nui,

*Betty Neary Alberts*

Betty Neary Alberts  
PO Box 790285, Pa'ia, Maui, HI 96779 and  
2700 Virginia Ave. NW, apt. 1401, Washington, DC 20037  
Fax: 202-333-7624  
Email: BettyNAlberts@aol.com

**RUSH 1001**

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input type="checkbox"/>					
D.P. DIR	<input type="checkbox"/>					
PERS.	<input type="checkbox"/>					
STAFF CE	<input type="checkbox"/>					
LUCA	<input type="checkbox"/>					
WW RECL.	<input type="checkbox"/>					
SOLID W.	<input type="checkbox"/>					
ENGR.	<input type="checkbox"/>					
HWYS.	<input type="checkbox"/>					
SECTY.	<input type="checkbox"/>					
<b>A&amp;B</b>	<input checked="" type="checkbox"/>					

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_

*3/21/04*

**IN FAVOR OF THE PA'IA-KALA ROAD BYPASS**TO: Maui Office of DOT, Mr. Fred CajigalFAX #: 1-808-873-3544

March 15, 2004

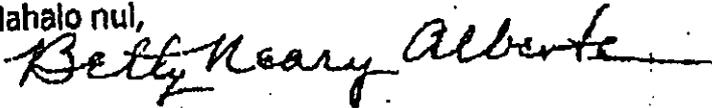
I urge you to promote the immediate adoption of the permanent Pa'ia Bypass (aka. KALA-SUNNYSIDE) as soon as possible. This is long overdue and is what the people want!!! As a kama'aina who grew up in Pa'ia and attended PA'IA AND KAUNOA SCHOOLS, we need a permanent solution to this ever growing traffic situation for the benefit of locals and tourists alike, as well as for the preservation of Pa'ia town, while also helping the shopkeepers and restaurants with their businesses.

The temporary mini by-pass suggested by A & B to join Hana Highway to Baldwin Avenue will cost taxpayers huge, unnecessary expenditures, largely for A & B's benefit.

I own the residence at 137A Baldwin Avenue, Pa'ia, very close to the A&B proposed mini-bypass and across from the Pa'ia Patsy Mink Post Office. Traffic congestion in the morning and at pau hana time already exists here on Baldwin Avenue. More traffic flowing into our local street will mean a more hazardous situation and huge problems for vehicles, pedestrians and school children going to and from school. This plan is not a solution to the Hana Highway traffic delays for Pa'ia, Haiku, and other upcountry residents and visitors alike.

Although I presently work in Washington DC, I am frequently at my Maui residence and bring a perspective both as a long-time local and as a visitor. I have carefully studied this problem for about 10 or more years and have attended Pa'ia/Haiku/Spreckelsville community meetings, have written letters of concern and have talked with a lot of people about this issue. I urge you to do what is right for the community and those most affected by this extremely frustrating situation. Make a good decision for the future of Maui, Pa'ia, Haiku, upcountry and not a short term, expensive one to benefit primarily the large landowner over the people!! Maui No Ka O'e.

Mahalo nui,



Betty Neary Alberts  
PO Box 790285, Pa'ia, Maui, HI 96779 and  
2700 Virginia Ave. NW, apt. 1401, Washington, DC 20037  
Fax: 202-333-7624  
Email: BettyNAlberts@aol.com

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Honorable Michelle Anderson  
c/o Council of the County of Maui  
200 South High Street  
Wailuku, Maui, Hawaii 96793

Dear Councilmember Anderson:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your letter dated April 23, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response.

Drawings in the Draft EA of the Department of Transportation (DOT) portion have been reduced in size in order to be included within the EA document. The figures in the document have been reduced in proportion to their original scale. The figures do show some adjacent land uses for contextual purposes.

It is noted that an additional traffic assessment was done in order to address possible traffic impacts on Baldwin Avenue which may result from the revised Paia Mini-Bypass project. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

We acknowledge that the traffic report utilized in the EA was done in 2002. However, with the foregoing update, we believe that this does provide sufficient data to reflect current conditions. While the hours of the mini-bypass are proposed as 4:00

Honorable Michelle Anderson  
March 2, 2005  
Page 2

p.m. to 6:00 p.m., excluding Saturdays, Sundays and holidays, we are also open to discussing the hours of operation for the mini-bypass.

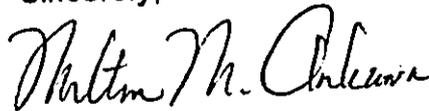
A detailed plan view and cross section of the proposed improvements to Hana Highway were included in the Draft EA. Regarding your concern on existing parking, we note that informal parking occurs in a number of areas in this vicinity. Parking occurs along the fence on the makai side of the Hana Highway right-of-way near the Paia Beach Park, the mauka side of Hana Highway, along Puna Road, and on the makai side of Hana Highway on the Wailuku side of Puna Road. With the proposed improvements, there would still be space for angle parking along the fence although parallel parking which currently takes place on the mauka side of Hana Highway would be curtailed. However, informal parking space is still available on the makai side of Hana Highway extending closer to Baldwin Park.

Regarding possible noise impacts to adjacent residential property, it is noted that any impacts would be limited to the hours of operation which are currently proposed for 4:00 p.m. to 6:00 p.m., excluding Saturdays, Sundays and holidays, and are not expected to be adverse.

Issues relating to possible future development of an A&B commercial center next to the Paia Mini-Bypass are not part of the mini-bypass application. Future applications would be subject to permit processes which are in effect at the time of application.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-anderson

April 23, 2004

'04 APR 26 P2:33

Mr. Milton Arakawa, Deputy Director  
County of Maui  
Department of Public Works and  
Environmental Management  
200 South High Street  
Wailuku, HI 96793

Dear Mr. Arakawa:

SUBJECT: DRAFT ENVIRONMENTAL ASSESSMENT  
FOR PA'IA MINI-BYPASS

Please address the following comments on the draft EA for this proposed action.

1. There are detailed drawings showing signage, pavement marking plans, and a profile of the proposed mini-bypass, but there is no drawing showing the overall site design of the proposed roadway in context to the surrounding area. A detailed site plan of the proposed roadway should be provided on a map showing the surrounding area, preferably to scale.
2. There is no feasibility study or analysis report that substantiates that the proposed roadway will actually alleviate traffic congestion in the area, rather than just shift the congestion from Hana Hwy to Baldwin Avenue.
3. The traffic report used in the EA was done in May 2002. This does not reflect current conditions. Anyone traveling this intersection on a daily basis knows that traffic congestion on Hana Hwy can occur at all hours, including weekends, not just from 4pm to 6 pm, which is the proposed operating time for the new roadway. Please provide a current traffic report that accurately reflects traffic counts for both Hana Highway and Baldwin Avenue.
4. There is no mention of the required alteration of Hana Hwy to accommodate the third lane. A detailed drawing should be

1293 RUSH

DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR						
DEP. DIR						
PER.S.						
STAFF CE						
LUCA						
WW RECL.						
SOLID W.						
ENGR.						
HWYS.						
SECTY.						
ADD/DO						

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_ Date: \_\_\_\_\_

*4/26*

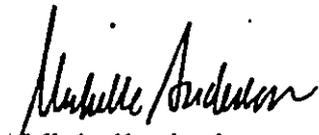
*Arakawa*

included in the EA to indicate what impact this will have on existing parking along the highway near the Paia Youth Center.

5. There is no assessment or proposed mitigation of roadway noise impacts to adjacent residential property behind the gas station.
6. There is no discussion of the commercial center A&B has planned next to the post office. This center was included in the Environmental Assessment prepared by A&B for the Paia Post Office last year. It indicated approximately 110,000 square feet of retail space and a large parking lot is planned for this 5 acre parcel. The proposed roadway will intersect at Baldwin Avenue between this parcel and the post office. Potential traffic impacts on the proposed roadway and Baldwin Avenue relative to this future development should be discussed and assessed in this EA.

Thank you for the opportunity to comment on this Draft Environmental Assessment. I look forward to your response.

Sincerely,



Michelle Anderson  
104A Kawalea Pl.  
Kula, HI 96793

Cc: OEQC

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration  
TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division  
CARY YAMASHITA, P.E.  
Engineering Division  
BRIAN HASHIRO, P.E.  
Highways Division  
Solid Waste Division

March 3, 2005

Mr. Harold Appleton  
HAROLDA@prodigy.net

Dear Mr. Appleton:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your e-mail transmittal of March 20, 2004 relating to the Draft Environmental Assessment of the Paia Mini-Bypass. We would like to provide the following response.

We appreciate your comments on the use of Kala Road. Certainly, this can be considered one segment for a permanent Paia Bypass. It is noted that Kala Road is a dead-end road which extends from Hana Highway along the row of elephant ear trees near the airport. It is an old government road which has not been claimed by either the State of Hawaii or the County of Maui. In order to connect to Baldwin Avenue near the Paia Mill, Kala Road must be connected to Sunnyside Road which is a private road currently utilized by HC&S. There are a number of unresolved issues which need to be addressed if Kala/Sunnyside Road is to be made available for public use, including safety and liability, planning, permitting, engineering and land acquisition. We estimate that resolution of these issues will take at least six (6) to eight (8) years. Based on other similar new roadway projects within the island of Maui, an estimate of the project cost ranges from \$25.9 million to \$37 million.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be started during the second quarter of 2005 and scheduled to take three (3) months.

Mr. Harold Appleton  
March 3, 2005  
Page 2

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-appleton

**From:** "Harold c Appleton" <HAROLDA@prodigy.net>  
**To:** <public.works@co.maui.hi.us>  
**Date:** 3/20/04 4:49PM  
**Subject:** Paia by-pass

I encourage the County to consider the use of Kala Road for a short term by pass around Paia rather than the proposed short term road by the Post Office. Kala Road is a government owned thoroughfare and can be quickly put to use as a bypass with minimal disturbance of Paia town.

-Harold Appleton

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Ms. Kathy Barr  
P. O. Box 791507  
Paia, Maui, Hawaii 96779

Dear Ms. Barr:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments from the March 17, 2004 Paia informational meeting relating to the Draft Environmental Assessment of the Paia Mini-Bypass. We would like to provide the following response.

An additional traffic assessment was done in order to address traffic impacts on Baldwin Avenue. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

The one (1) lane roadway connection to Baldwin Avenue will, however, be moved slightly more mauka. It will now connect mauka of the Paia Post Office instead of makai in order to minimize conflicts with the existing post office customer driveway and pedestrian traffic.

The hours of operation for the mini-bypass are currently being proposed for 4 p.m. to 6 p.m., excluding Saturdays, Sundays and holidays. After the project is implemented, the hours of operation can be adjusted accordingly depending on need and personnel availability.

Ms. Kathy Barr  
March 2, 2005  
Page 2

The proposed Hana Highway shoulder lane extends 0.53 mile from the County parking lot to Alawai Road. While an extension to the shoulder lane would provide additional capacity to divert more automobiles from the Hana Highway queue, it may also cause additional congestion to traffic at the Baldwin Avenue connection. We believe that the proposed 0.53 mile shoulder lane provides a significant increase in storage capacity compared to existing conditions and will help in mitigating existing congestion.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be started during the second quarter of 2005 and is scheduled to be completed three (3) months after start of construction.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-barr

Paia Mini-Bypass

'04 MAR 19 P3:08

COUNTY OF MAUI  
DEPT. OF PUBLIC WORKS

Name: Kathy Barr  
 Address: PO Box 791507  
16 Baldwin Ave. Paia 96779  
 Phone Number: 579.8880  
 e-mail address: kempmaui@hotmail.com

884

DEPT. OF PUBLIC WORKS	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DEP. DIR.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WV RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ADD/DET	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Return to \_\_\_\_\_ Due \_\_\_\_\_  
 Ref. No. \_\_\_\_\_  
 By: \_\_\_\_\_ Date: \_\_\_\_\_

*maui 3/19*

I have the following comments on the Paia Mini-Bypass Project.

- ① Traffic already backed up on Baldwin every afternoon.  
Mini should come out more mauka - closer to mill.
- ② Hours definitely are not long enough.  
2:30 - 7:30 would be better.
- ③ Can the shoulder lane be longer to hold the queue?

Thanks for keeping the meeting on track + not entertaining all those old ideas

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
 Director  
 Department of Public Works and Environmental Management  
 200 South High Street  
 Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. James A. Basco  
P. O. Box 790491  
Paia, Maui, Hawaii 96779

Dear Mr. Basco:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments from the March 17, 2004 Paia informational meeting relating to the Draft Environmental Assessment of the Paia Mini-Bypass. We would like to provide the following response.

The Paia Mini-Bypass includes a shoulder lane within the existing Hana Highway right-of-way from the County parking lot to Baldwin Park. Since approximately 30 percent (30%) of the traffic turns right onto Baldwin Avenue, the intent of the mini-bypass is to allow this traffic to get out of the main Hana Highway queue as soon as possible thereby reducing its length. An additional traffic assessment was done regarding the possible impacts upon Baldwin Avenue. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

Because of concerns about the impact of the connection to Baldwin Avenue adjacent to the Paia Post Office customer parking lot, the connection is now being proposed mauka of the post office to minimize possible conflicts with post office vehicular and pedestrian traffic.

Mr. James A. Basco  
March 2, 2005  
Page 2

We appreciate your comments on implementing a permanent Paia Bypass. However, there are a number of unresolved issues which need to be addressed including safety and liability, planning, permitting, engineering and land acquisition. We estimate that this will take at least six (6) to eight (8) years. Based on other similar new roadway projects within the island of Maui, an estimate of the project cost ranges from \$25.9 million to \$37 million.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 and is scheduled to be completed three (3) months after construction start.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-basco

Paia Mini-Bypass

Name James A. Basco  
Address P.O. Box 790491  
Paia Maui Hawaii 96779  
Phone Number 579-9923  
e-mail address \_\_\_\_\_

I have the following comments on the Paia Mini-Bypass Project.

The Mini Bypass will not have the effect  
it is intended for because 1) traffic backs  
up as far as stable road, the bypass begins  
at Baldwin Park 2) Most of the traffic is  
headed out to Haiku and beyond 3) The  
mini bypass will empty out onto Baldwin  
Ave, many of the roads feeding into our  
neighborhood, are one lane roads →

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

Over

This will cause a huge problem,  
with the Mine Bypass emptying out, with  
people turning in and out of the Post  
Office, with people turning in and  
out of our one lane roads, with  
people coming down Baldwin where there  
is a blind curve near Para Pit Stop.

A real bypass should be made where  
the road leaves Hana Highway four  
or five miles out and enter Baldwin  
Ave above the old mill. Then eventually  
on to Hailcy.

Jamei Basu

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Ms. Patti Cadiz  
2406 Waipua Street  
Paia, Maui, Hawaii 96779

Dear Ms. Cadiz:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your e-mail transmittal of March 25, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response.

- A. We agree that the Paia Mini-Bypass project should not delay the discussion on a permanent Paia Bypass. However, a permanent bypass would require the resolution of a number of procedural, as well as substantive issues, before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be finished in order to address safety, planning, permitting, engineering and land acquisition issues. Thus, the mini-bypass is seen as an interim solution to traffic issues in the region.
- B. Regarding public input, it is noted that the early consultation period included a request for comments from the Paia Main Street Association. The 30-day public comment period started on March 7, 2004 and was extended until April 23, 2004. In addition, an evening public informational meeting was held on March 17, 2004, at the Paia Community Center. The project is in compliance with all applicable rules and regulations pertaining to public input.

Ms. Patti Cadiz  
March 2, 2005  
Page 2

- C. We have done an additional traffic assessment in order to address traffic impacts on Baldwin Avenue. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.
- D. The closest public school is Paia Elementary School, which is located more than 1.5 miles away. Doris Todd Memorial Christian School, a private facility serving Grades K-6, is located slightly less than a mile away from the project. The closest public intermediate school, Kalama Intermediate, and high school, King Kekaulike High School, are located in Makawao Town, more than seven (7) miles away. Other private schools in the Upcountry region were not listed since potential impacts of these schools are localized. The proposed project provides another transportation alternative for afternoon peak hour (4:00 p.m. - 6:00 p.m.) commuters coming toward Paia which does not affect peak hour morning or afternoon school traffic. Thus, the project should have no adverse effect on school traffic.
- E. The scope of the traffic study was limited to afternoon peak hours. The scope did not include morning peak hour traffic. In the case of big surf being a cause of traffic congestion, such times are not predictable, but may also occur during the afternoon peak hours. However, it can be stated that the proposed project can only provide more traffic capacity than presently exists and can only provide an improvement to the existing traffic congestion on Hana Highway.
- F. With regard to your comment on reduction of available parking for surfers, bike tour companies and other users of the Paia Youth and Cultural Center, it is noted that informal parking occurs in the general vicinity of Paia Beach Park. This includes the area along the fence on the makai side of the Hana Highway right-of-way near the Paia Beach Park, the mauka side of Hana Highway, along

Puna Road, and on the makai side of Hana Highway on the Wailuku side of Puna Road. With the proposed improvements, there would still be space for angle parking along the fence, although parallel parking which currently takes place on the mauka side of Hana Highway would be curtailed. However, informal parking space is still available on the makai side of Hana Highway extending closer to Baldwin Park.

- G. There will be signage along the Hana Highway right-of-way in order to inform motorists that use of the shoulder lane is limited to afternoon peak hours. Enforcement on the use of the shoulder lane and any aggressive driving by individuals would be enforced by the Police Department.
- H. Regarding the issue of a permanent Paia Bypass, the comment in the Draft EA on community consensus pertained to the previous effort to implement the bypass. A detailed analysis of a preferred alignment for the permanent Paia Bypass has not yet been done.
- I. We believe that the use of the Paia Mini-Bypass should have a beneficial effect on mitigating traffic impacts during peak afternoon hours and justifies the cost.
- J. Regarding the existing traffic signal at Hana Highway and Baldwin Avenue, the timing has been and can be adjusted in the future, as appropriate. However, as the traffic study in the EA notes, there are factors which limit the amount of cars which can be processed through the intersection. These include diagonal parking, left turning vehicles and pedestrians crossing the street.
- K. Left-turn movements into the Paia Youth and Cultural Center is one factor contributing to the congestion through Paia Town. If left-turn lanes are being considered, it should be done comprehensively within Paia Town, as opposed to individually.
- L. A bikeway/greenway is not part of the subject proposal. The proposed project also does not preclude future implementation of a bikeway/greenway on nearby lands. Therefore, this has not been discussed in the EA.
- M. The alternative of Kala/Sunnyside Road was discussed objectively in the EA.

Ms. Patti Cadiz

March 2, 2005

Page 4

- N. When the permanent Paia Bypass gets implemented, long-term use of the mini-bypass improvements would depend on its possible role to address regional transportation needs.
- O. Section 11-200-9(a)(1) of the Hawaii Administrative Rules requires consultation with other agencies having jurisdiction or expertise, as well as those citizen groups and individuals. We have consulted with 13 agencies, including the Paia Main Street Association, prior to issuance of the Draft EA. This is in compliance with the foregoing section.
- P. A half mile deceleration lane is actually less than recommended by the traffic study in the EA. However, the shoulder lane is needed in order to provide additional lane length away from the source of congestion within Paia Town which takes a significant amount of traffic out the Hana Highway moving queue. A deceleration lane of 200 feet or less will not have a significant effect in mitigating traffic congestion.
- Q. The Paia -Haiku Community Plan contains a policy which states "discourage heavy truck traffic through Paia Town". Truck traffic currently utilizes Hana Highway through Paia. Truck traffic is being prohibited from using the shoulder lane (ten [10] feet) for safety reasons since it is not as wide as the regular travel lane (12 feet). The base course also has not been designed to withstand truck loads. Because Baldwin Avenue is relatively narrow at its connection to the mini-bypass, trucks would have difficulty in executing turn movements. It is noted that the proposal would not change existing truck traffic patterns.
- R. We appreciate the request to move the Baldwin Avenue terminus above the Paia Post Office. The Final EA includes revised plans to move the one (1) lane roadway above the post office. We believe that this improves the project proposal.
- S. The intent of the project is to divert traffic intending to turn right on Baldwin Avenue. With the proposed project, a prohibition on left turns at the Baldwin Avenue connection may result in less vehicles being able to utilize the mini-bypass. It is noted there are a number of residences and businesses which are located makai of the Baldwin Avenue connection. Thus, left turns at Baldwin Avenue are needed in order to maximize the amount of vehicles deleted

Ms. Patti Cadiz  
March 2, 2005  
Page 5

from the Hana Highway queue. Also, you have suggested that the right-turn lane on Baldwin Avenue could be lengthened to shorten the traffic queue on Baldwin. This could be considered as a separate issue since this may result in the loss of some existing diagonal parking stalls.

- T. All of the work on the shoulder lane will be within the Hana Highway right-of-way. The proposed cross section would involve six (6) feet of additional pavement on the mauka side and four (4) feet on the makai side. The remaining shoulder area would continue to be grassed just as it is now. Signage is necessary for informational and safety purposes. The project does not adversely affect scenic and open space qualities of the vicinity.
- U. Parking stalls in Paia Town are not proposed to be changed as part of the project proposal. Any unknown future proposals will be subject to processes applicable to those proposals.

In conclusion, we note that the State Department of Transportation intends to start the planning and design process for the permanent Paia Bypass. One of the issues which needs to be addressed is obtaining a consensus on the permanent alignment. We agree that the permanent Paia Bypass should be expedited. However, the Paia Mini-Bypass can provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-cadiz

**From:** "Cadiz" <info@hstwind surfing.com>  
**To:** "Milton Arakawa" <milton.arakawa@co.maui.hi.us>  
**Date:** 3/25/04 7:07PM  
**Subject:** A&B reliever road/ mini bypass

Mr. Milton Arakawa  
Department of Public Works and Environmental Planning  
Aloha Mr. Arakawa,

The A&B Reliever Road/Paia mini-by-pass project, as proposed, leaves much room for improvement. The Draft EA is flawed mostly because of failure to seek input from those citizens and organizations most affected. I urge you to withdraw the Draft EA and spend a little more time on planning to improve the results of this interim measure.

My concerns include:

- a.. This project should in no way delay progress on expediting the ONLY real solution which is an alternate route around the whole town for commuters.
- b.. Because no public input was taken before project design, the project is not reflective of community wishes and may cause unanticipated negative impacts. Not getting public input is in direct conflict with Hawaii Administrative Rules.
- c.. No traffic impact studies have been done to address impacts on Baldwin Avenue residents and merchants. The Draft EA doesn't address this.
- d.. The project will compound the afternoon school traffic on Baldwin Avenue. The Draft EA doesn't address this.
- e.. The Draft EA doesn't recognize that big surf is one of the major causes of traffic congestion in Paia.
- f.. The only traffic study that supports the project was done on April 16, 2002, (which was not a big surf day) and was grossly incomplete. (The travel time study ended at 4:35pm(l) and the traffic volume counts only covered 3 p.m. to 6 p.m.) Traffic is often bad earlier and later and even on weekends. This is wholly inadequate.
- g.. The project does nothing for morning commuter traffic or afternoon school related traffic. (There are 7 schools up Baldwin Avenue and more than 12 upcountry schools that may be serviced by Baldwin Avenue. The Draft EA doesn't mention but a fraction of the schools.)
- h.. The project will reduce the available parking for surfers, bike-tour companies and other users of the Paia Youth and Cultural Center. (No more parking mauka of the Highway and four feet of the makai diagonal parking at the basketball court will become roadway, making parallel parking the only remaining option.) The Draft EA doesn't recognize this.
- i.. A two-lane to three-lane back to two-lane design invites aggressive driving and is dangerous and is counter to the goal that interim measures should be "traffic calming."
- j.. The Draft EA, in reference to the full Paia Bypass, states on page 36, "Obtaining community consensus on its implementation was not successful..." This has not been true for over a decade (probably longer) and should be stricken from the EA. Community Associations and business associations have supported a full by-pass for many, many years. Where did this comment come from??? It must be stricken.
- k.. For nearly three-quarters of a million dollars, only 10 hours/week planned usage is wasteful.
- l.. A better use of public funds may include alternatives that have not even been studied or included in the Draft EA, for instance:
  - a.. upgrading the traffic signal to be able to accommodate fluctuating traffic patterns during the day (i.e. longer westbound for the morning commuters, longer makai bound for school traffic up Baldwin and longer eastbound in the evening for commuters) and that can be controlled from a monitoring office to accommodate traffic nuances, like accidents or high surf that brings throngs to the north shore.
  - b.. creating a left turn lane for the Youth and Cultural Center.
- m.. The bikeway/greenway is not considered in this plan as it should be. Another inadequacy of the Draft EA.
- n.. The evaluation of the Kala Road/Sunnyside Road option seemed to be done with a "can't do" or "wont do" attitude. Why is that?
- o.. If this is a "temporary" interim traffic calming measure, using considerable public funding, what will become of that investment when the real By-Pass comes on line?
- p.. Contrary to Hawaii Administrative Rules, Section 11-200-09 (1) public input was not received before

the Draft EA was published, possibly putting the whole project in legal jeopardy.

- q.. A half mile deceleration lane is unnecessarily long and costly and unsightly.
- r.. Signage for the "mini" will prohibit trucks and buses, instead directing them thru town, which is in conflict with the Paia/Haiku Community Plan which indicates that truck traffic should be diverted around town. Another omission in the Draft EA.
- s.. The Baldwin terminus below the Post Office is contradictory to the Paia petition of 2002 requesting that it align above the Post Office. Why isn't this petition included in the Draft EA?
- t.. Below the Post Office alignment "cuts off" pedestrian traffic from the Post Office. This consequence is overlooked in the Draft EA.
- u.. If the intent of the project is to service mauka bound traffic, it is inappropriate to allow left turns at the Baldwin terminus. The Paia petition of 2002 requested no left turn at the Baldwin terminus.
- v.. Page 27 (paragraph 1) of the Draft EA concludes, "...the project should not adversely affect the scenic and open space qualities of Paia town." Disagree. A half mile of additional pavement, with no landscaping, and 25 traffic signs on Hana Highway alone, are arguably adverse to the scenic and open space qualities of Paia.
- w.. What assurance is there that this will not lead to the taking of the parking stalls along Hana Hwy in town?

What should be done?

- a.. The Draft EA should be withdrawn until public input is incorporated.
- b.. Create a "Mayor's Task Force" comprised of representatives from Paia, Haiku, Spreckelsville communities and businesses to seek consensus on interim measures as well as the alignment of the REAL BYPASS.
- c.. Shorten the deceleration lane to about 200 feet or less.
- d.. Consider creating a turn lane to/from the Y&CC parking lot, west of the actual parking area.
- e.. Update the traffic signal.
- f.. Realign the Baldwin Avenue terminus to above the Post Office.
- g.. Allow right turns only at the Baldwin Avenue terminus.
- h.. Use any remaining funding on planning for the REAL BYPASS.

Respectfully submitted,  
Patricia Cadiz

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. Christopher Caillarec  
P. O. Box 791098  
Paia, Maui, Hawaii 96779

Dear Mr. Caillarec:

Thank you for your e-mail transmittal of April 5, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response.

The Draft EA discusses the use of Kala Road and Sunnyside Road as an alternative action to the mini-bypass. Kala Road is an old government road which extends from Hana Highway to Sunnyside Road which then extends to Baldwin Avenue just mauka of the Paia Mill. Sunnyside Road is a privately owned road which must be purchased if it is desired for public use. There are also a number of other issues which need to be resolved before use of Kala/Sunnyside Road by the public is warranted. These include safety and liability issues relating to the current agricultural use of Sunnyside Road. There are also planning and route selection issues which need to be discussed for the Haiku leg of the bypass. An engineering analysis also needs to be done which discusses drainage, desired cross section of the road, and intersection design. Our analysis showed that it could take approximately six (6) to eight (8) years before the necessary planning, public hearings, environmental review, permits, design, and funding can be obtained in order to address the foregoing issues. Based on other roadways recently constructed on the island of Maui, an estimated roadway cost for the Kala/Sunnyside Road segment (not including a Haiku leg) could range from \$25.9 to \$37 million.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from

Mr. Christopher Caillarec  
March 3, 2005  
Page 2

Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion scheduled three (3) months after start of construction.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-caillarec

**From:** Christopher Caillarec <imfishman\_21n@yahoo.com>  
**To:** <Milton.Arakawa@co.maui.hi.us>  
**Date:** 4/5/04 10:15AM

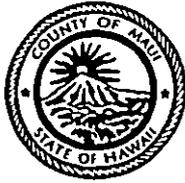
Speaking as a 30 year plus Paia resident I can't believe the foot dragging in the creation of a solution to our traffic problem. A real bypass should have been created YEARS ago to accomodate the accelerated growth in the Haiku-Peahi-Kaupakalua, etc. areas without funneling all the commuters trough a small foot-traffic tourist destination. This situation is tantamount to still having to get to Kaanapali via Front Street. I wonder; Is this the same set of planning geniuses that one year accomplished a beautiful resurfacing to Hana Hwy. only to tear it up within months to install the sewer system? Probably not, but the same thinking applies. Take back the cane haul road that runs by the Paia mill and connects to old Maui High AND CREATE A REAL BYPASS. Then really dazzle me; span Maliko with four lanes and we'll have a lasting solution to tommorrows challenges. Christopher Caillarec.

---

Do you Yahoo!  
Yahoo! Small Business \$15K Web Design Giveaway  
[http://promotions.yahoo.com/design\\_giveaway/](http://promotions.yahoo.com/design_giveaway/)

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955

RECEIVED  
2005 MAR -4 AM 7:53



OFFICE OF THE MAYOR COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

05 MAR -4 P 1:51  
COUNTY OF MAUI  
PUBLIC WORKS

Honorable Alan M. Arakawa  
Mayor, County of Maui  
200 South High Street  
Wailuku, Maui, Hawaii 96793

APPROVED FOR TRANSMITTAL

  
\_\_\_\_\_  
Mayor Date

For transmittal to:

Honorable Robert Carroll  
Councilmember  
Maui County Council  
200 South High Street  
Wailuku, Maui, Hawaii 96793

Dear Councilmember Carroll:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments from the March 17, 2004 Paia informational meeting on the Paia Mini-Bypass. We would like to provide the following response.

We appreciate your suggestion to ban left turns onto Baldwin Avenue from the mini-bypass by the Paia Post Office. However, the intent of the project is to divert traffic intending to turn right on Baldwin Avenue from Hana Highway. With the proposed project, a prohibition on left turns at the Baldwin Avenue connection may result in less vehicles being able to utilize the mini-bypass. It is noted there are a number of residences and businesses which are located makai of the Baldwin Avenue connection. Thus, left turns at Baldwin Avenue are needed in order to maximize the amount of vehicles deleted from the Hana Highway queue.

Honorable Alan M. Arakawa  
For transmittal to:  
Honorable Robert Carroll  
March 2, 2005  
Page 2

The hours of operation for the mini-bypass are currently being proposed for 4:00 p.m. to 6:00 p.m., excluding Saturdays, Sundays and holidays. After the project is implemented, the hours of operation can be adjusted accordingly depending on need and personnel availability.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion scheduled three (3) months after construction start.

While we believe that a permanent Paia Bypass should be expedited, it should be emphasized that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-carroll

Paia Mini-Bypass

Name Robert Carroll  
Address 200 South High St  
Phone Number 298-7246  
e-mail address \_\_\_\_\_

I have the following comments on the Paia Mini-Bypass Project.

No Left Turn on Baldwin Ave  
Hours - open 24 Hours  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Honorable Ed Case  
United States Congressman  
Hawaii, Second District  
5104 Prince Kuhio Building  
Honolulu, Hawaii 96850-4974

Dear Congressman Case:

We have received your letter of March 8, 2004 to Mr. Rodney Haraga, State of Hawaii Director of Transportation, pertaining to the Paia Mini-Bypass. Since our agency is the lead agency in processing the Environmental Assessment for this project, we would like to provide the following additional information:

We would like to emphasize that the permanent Paia Bypass is a needed facility. Planning should be expedited in order to facilitate a solution. However, a permanent bypass would require the resolution of a number of outstanding issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one (1) possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be completed in order to comply with necessary planning, public hearings, permits, and design requirements, as well as obtain funding.

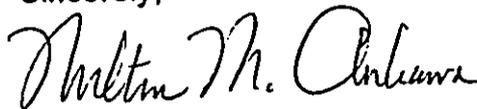
Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion three (3) months after start of completion.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Honorable Ed Case  
March 2, 2005  
Page 2

Thank you for your interest.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-case

WASHINGTON OFFICE:  
 128 CANNON HOUSE OFFICE BUILDING  
 WASHINGTON, DC 20515  
 (202) 225-4906  
 FAX: (202) 225-4987

HAWAII OFFICE:  
 5104 PRINCE KUHIO FEDERAL BUILDING  
 HONOLULU, HI 96850-4974  
 (808) 541-1986  
 FAX: (808) 538-0233

NEIGHBOR ISLAND TOLL FREE NUMBERS:  
 BIG ISLAND 935-3756  
 KAUAI / NI'HAU 245-1951  
 MAUI 242-1818  
 LANAI 565-7199  
 MOLOKA'I 552-0160

ed.case@mail.house.gov  
 www.house.gov/case



2004 MAR 12 12:11:04

**CONGRESSMAN ED CASE**  
**SECOND DISTRICT, HAWAII**

ISLANDS OF HAWAII, MAUI, KAHOLAWE, LANAI,  
 MOLOKA'I, O'AHU (WINDWARD, NORTH SHORE,  
 CENTRAL, LEEWARD), KAUAI AND NI'HAU,  
 AND NORTHWESTERN HAWAIIAN ISLANDS

March 8, 2004

Mr. Rodney Haraga  
 Director  
 State of Hawaii  
 Department of Transportation  
 869 Punchbowl St.  
 Honolulu, Hawaii 96813-5036

Dear Mr. Haraga:

I enclose a recent communication to Governor Lingle from a mutual constituent expressing concerns about the Paia Mini-Bypass, and suggesting an immediate solution to reducing the gridlock in Paia by opening Kala Road for mauka-bound traffic from 4:30 to 6:30 PM as a one-way reliever road.

As you know, traffic in and around Paia during rush hour continues to get worse as the volume of commuters to the north and east continues to grow. Therefore I would very much appreciate your attention to this matter and your consideration of the Kala Road Bypass idea.

If I can be of any assistance to the Department of Transportation on this matter, please do not hesitate to contact me or my legislative assistant, Tim Carson. I look forward to working with you to solve East Maui's growing traffic crisis.

With aloha,

**Ed Case**

ED CASE  
 United States Congressman  
 Hawaii, Second District

CC: Honorable Alan Arakawa, Maui County Mayor

4446

COMMITTEES:

- EDUCATION AND THE WORKFORCE  
EDUCATION REFORM
- EMPLOYER-EMPLOYEE RELATIONS
- AGRICULTURE  
CONSERVATION, CREDIT,  
RURAL DEVELOPMENT AND RESEARCH
- GENERAL FARM COMMODITIES  
AND RISK MANAGEMENT
- SMALL BUSINESS  
RURAL ENTERPRISES, AGRICULTURE  
AND TECHNOLOGY
- WORKFORCE EMPOWERMENT  
AND GOVERNMENT PROGRAMS
- REGULATORY REFORM  
AND OVERSIGHT

MAYOR/MGMT OFFICE	BY	HANDLE	COMMITTEE	OFFICE	DATE BACK FOR HOUSE FILE	FILED
MAYOR	<input type="checkbox"/>					
MD	<input type="checkbox"/>					
PIO	<input type="checkbox"/>					
BUDGET	<input type="checkbox"/>					
CORE	<input type="checkbox"/>					
QED	<input type="checkbox"/>					
DAVE D.	<input type="checkbox"/>					
ACCLEP.	<input type="checkbox"/>					
DAVE C.	<input type="checkbox"/>					
DON C.	<input type="checkbox"/>					
JAN B.	<input type="checkbox"/>					
HELE C.	<input type="checkbox"/>					
MERCY M.	<input type="checkbox"/>					
WENDY A.	<input type="checkbox"/>					
BENNY S.	<input type="checkbox"/>					
YEN H.	<input type="checkbox"/>					
3/12/04						
3/12/04						

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration  
TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division  
CARY YAMASHITA, P.E.  
Engineering Division  
BRIAN HASHIRO, P.E.  
Highways Division  
Solid Waste Division

March 2, 2005

Ms. Georgiana Cook  
P. O. Box 791029  
Paia, Maui, Hawaii 96779

Dear Ms. Cook:

Thank you for your comments from the March 17, 2004 informational meeting relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response.

In response to your concern that the Baldwin Avenue terminus of the mini-bypass makai of the Paia Post Office may conflict with traffic from the Paia Post Office driveway, we will be revising our plans to move the Baldwin Avenue terminus above or mauka of the Paia Post Office. The Final Environmental Assessment will reflect the change.

Moreover, we have done an additional traffic assessment in order to address traffic impacts on Baldwin Avenue. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

We would like to emphasize that the permanent Paia Bypass is a needed facility. Planning should be expedited in order to facilitate a solution. However, a permanent bypass would require the resolution of a number of outstanding issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be completed in order to

Ms. Georgiana Cook  
March 2, 2005  
Page 2

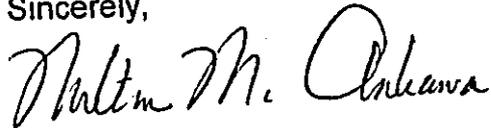
comply with necessary planning, public hearings, permits, and design requirements, as well as obtain funding.

Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion targeted for three (3) months after construction start.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-cook

## Paia Mini-Bypass

Name Georgiana Cook  
Address PO Box 791029  
Phone Number 808-579-9181  
e-mail address \_\_\_\_\_

I have the following comments on the Paia Mini-Bypass Project.

Please reconsider the Mini-Bypass, especially  
where it comes out next to the post office on Baldwin  
Ave. It seems that those who are proposing their  
route do not live in Paia and that the studies  
seem incorrect. The traffic seems to be  
concentrated in one small area even with  
the by-pass. The traffic will still be there, just  
in a different place!

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. Glenn Correa, Director  
Department of Parks and Recreation  
County of Maui  
700 Hali'a Nakoia Street, Unit 2  
Wailuku, Maui, Hawaii 96793

Dear Mr. Correa:

Thank you for your letter of April 1, 2004 indicating that you have no comments on the Draft Environmental Assessment (EA) for the Paia Mini-Bypass project.

We appreciate your interest and participation in the review phase of the Draft EA. Your letter will be included as part of the Final EA.

Sincerely,

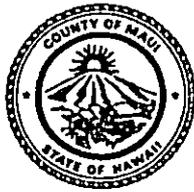
MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia\_gcorrea

ALAN M. ARAKAWA  
Mayor



GLENN T. CORREA  
Director

JOHN L. BUCK III  
Deputy Director

(808) 270-7230  
Fax (808) 270-7934

04 APR 29 P4:34

COUNTY OF MAUI  
PUBLIC WORKS

DEPARTMENT OF PARKS & RECREATION

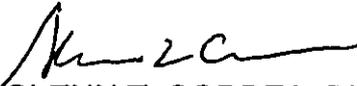
700 Hali'a Nakoa Street, Unit 2, Wailuku, Hawaii 96793

DEPT OF PLANNING  
COUNTY OF MAUI  
RECEIVER

04 APR -5 P3:10

April 1, 2004

MEMO TO: Michael W. Foley, Planning Director

FROM:   
GLENN T. CORREA, Director

SUBJECT: PAIA MINI-BYPASS PROJECT  
T.M.K. (2) 2-5-005:018  
SM1 2004/0004

We have reviewed the subject application and have no comments or objections to the proposed action.

Thank you for the opportunity to review and comment. Please contact me or Mr. Patrick Matsui, Chief of Planning and Development, at extension 7387 if there are any questions.

c: Patrick Matsui, Chief-Planning and Development

1348  RUSH

DEPT. OF PUBLIC WORKS	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DEP. DIR	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUDA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WM/PEPL	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENR 1	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
HWY'S	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECY	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>Asst. Dir.</b>	<input checked="" type="checkbox"/>	<input type="checkbox"/>				

Return to \_\_\_\_\_ Due \_\_\_\_\_  
Ref. No. \_\_\_\_\_  
By: \_\_\_\_\_ Date: \_\_\_\_\_

*4/30*  
*matsui*

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

MEMO TO: THOMAS PHILLIPS  
CHIEF OF POLICE

FROM: MILTON M. ARAKAWA, A.I.C.P. *Milton M. Arakawa*  
DIRECTOR OF PUBLIC WORKS AND ENVIRONMENTAL  
MANAGEMENT

SUBJECT: PAIA MINI-BYPASS

We have received a copy of your comments dated March 31, 2004 to Planning Director Michael Foley pertaining to the Paia Mini-Bypass. We would like to provide the following response:

We would like to apprise you that the opening and closing of the gate for the single-lane portion of the project near the County's parking lot and Baldwin Avenue will be done by HC&S. This portion of the road will be still within private property. It is noted that there is no turnaround area once a vehicle enters the single-lane roadway since traffic is one way. There is a proposed shoulder lane within the Hana Highway right-of-way which is intended to be utilized only during the operating hours of the mini-bypass. Should vehicles utilize this shoulder lane during non-operating hours, the vehicles could physically merge into the Hana Highway traffic queue entering Paia Town. However, the Police Department may also be asked to ticket drivers of vehicles utilizing the shoulder lane during non-operating hours. We acknowledge your concern on security at the Hana Highway and Baldwin Avenue gates in order to prevent use by unauthorized vehicles.

We understand your concern about including a weight limit on signs entering Paia Town to better define the term "trucks and buses". The intent is to exclude heavy trucks and slow-moving vehicles from using the one-way, single-lane portion of the project. We will consider appropriate signage to achieve that end.

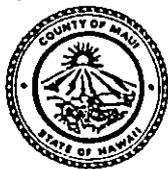
Memo to Thomas Phillips, Chief of Police  
March 2, 2005  
Page 2

Regarding your suggestion to include a raised crosswalk within the immediate Paia Town area, this request will be considered. It is noted, however, that there is a crosswalk at the existing traffic signal at Baldwin Avenue and Hana Highway. There is another crosswalk near the County parking lot at the entrance to Paia. A raised crosswalk in Paia Town raises issues relating to how it would affect the existing diagonal and parallel parking. There are also possible sight distance issues with automobiles backing out of stalls, pedestrians desiring to cross the street, and drivers in the main Hana Highway queue being able to each safely perform their desired tasks.

Thank you for your input to the project.

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-police



# POLICE DEPARTMENT COUNTY OF MAUI



ALAN M. ARAKAWA  
MAYOR

'04 APR 29 P 4:34

55 MAHALANI STREET  
WAILUKU, HAWAII 96793  
(808) 244-6400  
FAX (808) 244-6411

THOMAS M. PHILLIPS  
CHIEF OF POLICE

OUR REFERENCE  
YOUR REFERENCE

COUNTY OF MAUI  
PUBLIC WORKS

KEKUHAUPIO R. AKANA  
DEPUTY CHIEF OF POLICE

DEPT OF PLANNING  
COUNTY OF MAUI  
March 31, 2004 RECEIVED

## MEMORANDUM

TO : MICHAEL W. FOLEY, PLANNING DIRECTOR  
FROM : THOMAS M. PHILLIPS, CHIEF OF POLICE  
SUBJECT : I.D. : SM1 2004/0004  
TMK : (2) 2-5-005:018  
Project Name : Paia Mini-Bypass Project  
Applicant : Gilbert Coloma-Agaran

- No recommendation or comment to offer.  
 Refer to enclosed comments and/or recommendations.

Thank you for giving us the opportunity to comment on this project.

Assistant Chief Sydney Kikuchi  
For: THOMAS M. PHILLIPS  
Chief of Police

RUSH 1350

Enclosure

DEPT. OF  
PUBLIC  
WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
--	------	--------	--------	----------	------	------

DIRECTOR	<input type="checkbox"/>					
DEP. DIR	<input type="checkbox"/>					
PERS.	<input type="checkbox"/>					
STAFF CE	<input type="checkbox"/>					
LUCA	<input type="checkbox"/>					
WW RECL.	<input type="checkbox"/>					
SOLID W.	<input type="checkbox"/>					
ENGR.	<input type="checkbox"/>					
HWYS.	<input type="checkbox"/>					
SECTY	<input type="checkbox"/>					

At 10/04  
4/24  
mailed

Return to \_\_\_\_\_ Date: \_\_\_\_\_

Ref. No. \_\_\_\_\_

By: \_\_\_\_\_ Date: \_\_\_\_\_

COPY

TO : THOMAS PHILLIPS, CHIEF, MAUI POLICE DEPARTMENT  
VIA : CHANNELS ~~\_\_\_\_\_~~ 2/30/04  
FROM : RANDALL BURGESS, P.O. III, COMMUNITY POLICING  
SUBJECT : ADDITIONAL COMMENT TO THE PAIA MINI-BYPASS PROJECT

Sir, this To/From is being submitted in regards to additional police comment to the proposed Paia Mini-Bypass Project located in Paia, Hi..

Further project review via Wailuku District Supervisors revealed the following comments:

- \* In regards to the gated areas of the bypass during non-operational hours; will the State of Hawaii Department of Transportation be responsible for the securing/opening of the stated gate? Also, ensure adequate turn-around area for vehicles mistakenly entering the bypass and blocked by the gate. Lastly, ensure that the portion of the bypass located between the Hana Hwy. gate and the Baldwin Ave. gate is secure during non-operational hours to prevent access by unauthorized vehicles thereby deterring loitering, drag racing, drinking, etc..
- \* In regards to State DOT sign stating "No Trucks or Buses". Suggest that said sign be more definitive regarding "Trucks".

In closing, there are no further comments at this time.

Respectfully submitted,  
*Randall Burgess*  
Randall BURGESS #1023  
032904 @ 1200 hours

STATE DOT SHOULD BE RESPONSIBLE  
FOR OPENING & CLOSING GATE TO  
BY-PASS.

ALSO REGARDING BUS & TRUCK SIGNS,  
WEIGHT LIMIT SHOULD BE POSTED ON  
SIGN.

*Sgt. M. P. M.*  
3/29/04

*concur,*

*mf 3/29/04*

COPY

TO : THOMAS PHILLIPS, CHIEF, MAUI POLICE DEPARTMENT  
VIA : CHANNELS *[Signature]* 03/23/04  
FROM : RANDALL BURGESS, P.O.III, COMMUNITY POLICING  
SUBJECT : PAIA MINI-BYPASS PROJECT

Sir, this To/From is being submitted in regards to police comments/recommendations to the proposed Paia Mini-Bypass Project located in Paia, Hi..

Project review and site inspection revealed the following comment:

- \* Beat knowledge reveals that vehicles travelling both directions on Baldwin Ave. and within the immediate Paia Town area, often exceed the posted speed limit of 20 MPH and create pedestrian safety concern. Therefore, suggest that the Department of Public Works and Environmental Management Highways Division review the installation of "Traffic Calming" measures such as "Raised Crosswalks" on Baldwin Ave. in the immediate Paia Town area.
- \* Other comment in regards to roadways/traffic have been addressed within the Special Management Area Permit Application.

In closing, it is noted that the previous police comment report submitted and listed on page 58 of the Special Management Area Permit Application, is not included within the said Application.

Respectfully submitted,  
*Randall Burgess*  
Randall BURGESS #1023  
032204 @ 1400 hours

CONCURE WITH COMMENTS /  
RECOMMENDATION BY DR. BURGESS.

CONCURE  
*[Signature]*  
03/22/04

*[Signature]*  
3/22/04

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. George Tengan, Director  
Department of Water Supply  
County of Maui  
200 South High Street  
Wailuku, Maui, Hawaii 96793

Dear Mr. Tengan:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your letter of April 2, 2004 providing comments on the Draft Environmental Assessment for the Paia Mini-Bypass. We would like to provide the following response:

We acknowledge that construction plans need to be reviewed by the Department of Water Supply. We will be coordinating with your Engineering Division to lift water valve covers to match the finish grade of the roadway.

We will be pursuing the use of sugar cane irrigation water for dust and erosion control and irrigation purposes during the construction phase.

We appreciate the information on the planned 36 inch transmission line utilizing the Kala/Sunnyside Road as a bypass route.

Mr. George Tengan, Director  
March 2, 2005  
Page 2

Thank you for your input to the project.

Sincerely,



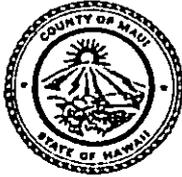
MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-dws

ALAN M. ARAKAWA  
Mayor



GEORGE Y. TENGAN  
Director

JEFFREY T. PEARSON, P.E.  
Deputy Director

04 APR -6 P2:10  
COUNTY OF MAUI  
PUBLIC WORKS

DEPARTMENT OF WATER SUPPLY  
COUNTY OF MAUI  
200 South High Street  
WAILUKU, MAUI, HAWAII 96793-2155  
Telephone (808) 270-7816 • Fax (808) 270-7833  
www.mauiwater.org

RUSH 1079

DEPT. OF  
PUBLIC  
WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DEP. DIR	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
APP/DOT	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_

By: \_\_\_\_\_ Date: \_\_\_\_\_

April 2, 2004

Ms. Robyn L. Loudermilk, Staff Planner  
Department of Planning  
County of Maui  
250 South High Street  
Wailuku HI 96793

Re: I.D.: SM1 2004/0004  
TMK: 2-5-005:018  
Project Name: Paia Mini-Bypass Project

Dear Ms. Loudermilk:

Thank you for the opportunity to comment on this application. We note that a copy of our comment letter to the Draft Environmental Assessment (DEA) is included the application material. We provide the following additional information:

Construction plans need to be reviewed by the Department of Water Supply. As stated in the DEA, water valve covers must be lifted to match the finished grade of the roadway. The applicant should contact our engineering division at 270-7835 with regard to these issues.

The DEA states that County potable water will be utilized for dust and erosion-control and irrigation purposes during the construction phase and where feasible, non-potable water will be utilized. We request that the applicant pursue using sugarcane irrigation water available on the project-site to alleviate demand on the Central Maui potable system.

In considering the alternative action of using Kala/Sunnyside Road as a bypass, it should be noted that the Department of Water Supply is planning installation of 36" transmission line from the Hamakuapoko wells partially along this route. A map showing approximate location of the transmission line is attached for your reference.

Should you have any questions, please contact our Water Resources and Planning Division at 270-7199.

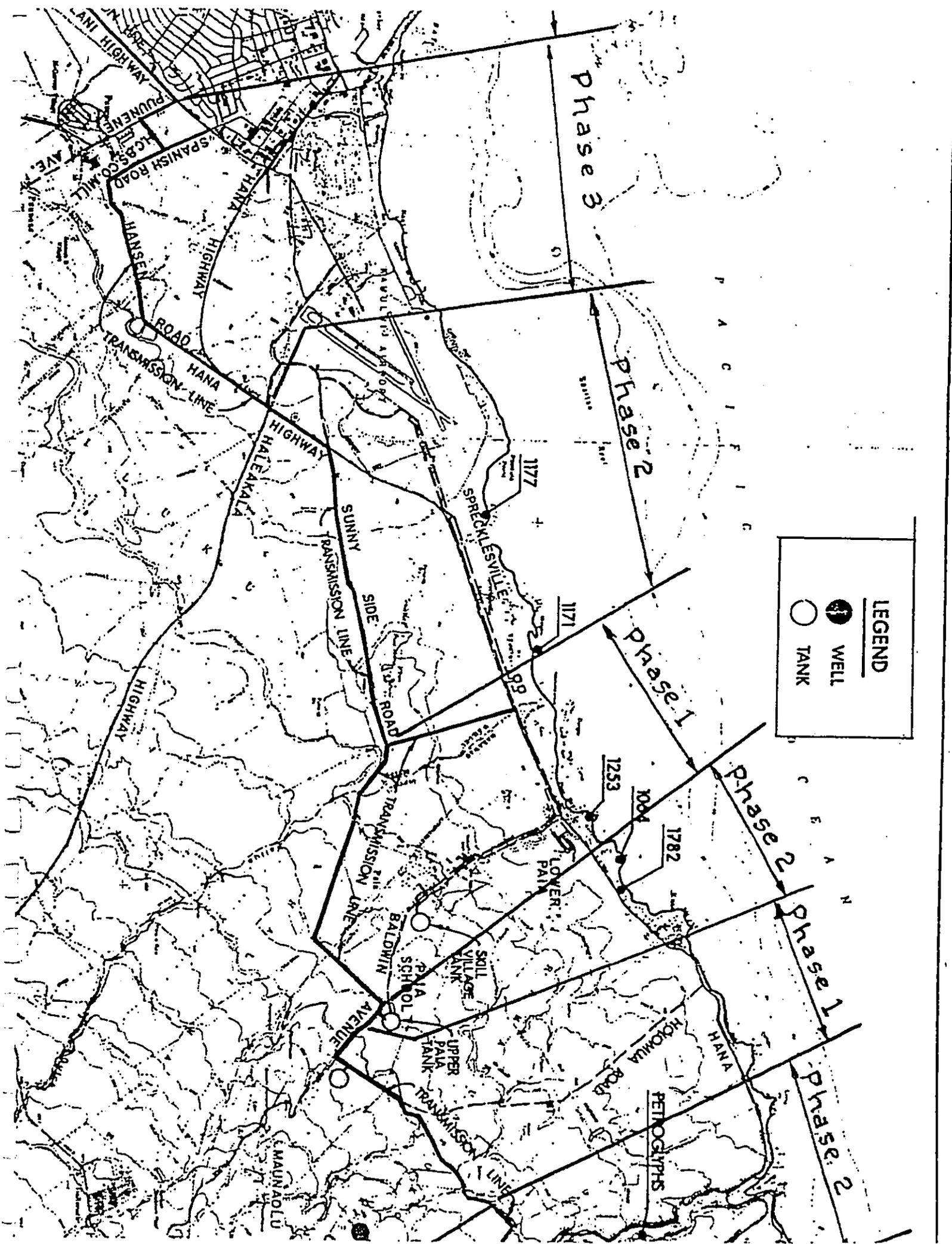
Sincerely,

George Y. Tengan  
Director  
emb

enclosure

c: engineering division  
applicant, with attachment

*By Water All Things Find Life*



**LEGEND**

- WELL
- TANK

C  
E  
A  
N

Phase 3

Phase 2

Phase 1

Phase 2

Phase 1

Phase 2

LANI HIGHWAY  
PUNENE  
H.C. BISCO MILL  
SPANISH ROAD  
HANSEN ROAD

HANA HIGHWAY  
HANA TRANSMISSION LINE  
HALEAKALA HIGHWAY

SUNNY SIDE ROAD  
TRANSMISSION LINE  
SPRINGVILLE  
1177  
1171

1253  
1004  
1782  
LOWER PALE  
BALDWIN  
PIA SCHOOL  
SKILL VILLAGE  
UPPER PALE TANK

HOKIOMIA ROAD  
HANA  
PETIOGLYPHS

MAUNAOLO

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. Paul Ehman  
P. O. Box 790479  
Paia, Maui, Hawaii 96779

Dear Mr. Ehman:

Thank you for your e-mail transmittal of April 6, 2004 relating to the Draft Environmental Assessment of the Paia Mini-Bypass. We would like to provide the following response:

We agree that the permanent Paia Bypass project needs to be expedited. However, a permanent bypass would require the resolution of a number of procedural, as well as substantive issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered as one possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be finished in order to address safety, planning, permitting, engineering and land acquisition issues. Thus, the mini-bypass is seen as an interim solution to traffic issues in the region. Assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion scheduled for three (3) months after start of construction.

We hope to be able to work to expedite implementation of both the mini-bypass and the permanent bypass. Thank you for your input to the project.

Sincerely,

MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-ehman

Jill-Anne Ono - Re: Paia bypass

**From:** Jill-Anne Ono  
**To:** Ehman, Paul  
**Subject:** Re: Paia bypass

>>> Paul Ehman <[ehman@maui.net](mailto:ehman@maui.net)> 4/6/2004 6:28:49 AM >>>

Aloha Milton,

Eh, you have a tough job! Hard to make most of the people happy some of the time, because not everyone can see the big picture.

This debate has been going on since before I was president of the Paia Business Assoc. over 20 years ago. The same situation: need more parking, less traffic.

*This problem still exists because nobody had the balls to plan for the future. You have an opportunity to do that now by implementing a real long-term solution. Don't take the easy way out by blowing a few dollars on a patch and pad project that offers no real solution. Leave a legacy by building a real road.*

Paul Ehman  
Ehman Productions, Inc.  
EPIC Events, Inc.  
Tel: 808-575-9151  
Fax: 720-596-7882  
[ehman@maui.net](mailto:ehman@maui.net)

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. and Mrs. Jeff Fagerholm  
P. O. Box 1219  
Haiku, Maui, Hawaii 96708

Dear Mr. and Mrs. Fagerholm:

Thank you for your e-mail transmittal of March 25, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

We would like to emphasize that the permanent Paia Bypass is a needed facility. Planning should be expedited in order to facilitate a solution. However, a permanent bypass would require the resolution of a number of outstanding issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be completed in order to comply with necessary planning, public hearings, permits, and design requirements, as well as to obtain funding.

Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

In reference to your proposal to hire a policeman to direct traffic in the intersection and to ticket people who turn left, we would note that there are three (3) basic factors which slow down traffic through Paia Town. These are diagonal parking, left-turning vehicles and pedestrians crossing the street. These constraints would still be present even if a policeman were present to direct traffic. Presently, left turns in Paia Town are permissible. Any ban on left turns through Paia Town should be

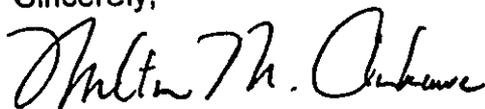
Mr. and Mrs. Jeff Fagerholm  
March 3, 2005  
Page 2

addressed comprehensively as there would be a number of businesses and residents who would be affected.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-fagerholm

**From:** "Fagerholm" <jlf@maui.net>  
**To:** <Milton.Arakawa@co.maui.hi.us>  
**Date:** 3/25/04 8:15PM  
**Subject:** Paia bypass

Dear Sir

I spend at least 100 minutes per week sitting in traffic going thru Paia. This is unnecessary, unproductive, and a negative situation for all involved.

There are many new subdivisions planed for the Haiku area, which in itself is a burden, but to put a Band-Aid fix on a huge and growing problem of traffic, is absurd.

Please reconsider this quick fix, and begin the real traffic solution, a real bypass of the parking and left turns that plague Paia.

By the way, a simple fix for a lot less capital is to hire a traffic cop to direct traffic in the intersection, and another to ticket people who turn left across the double yellow line.

Sincerely  
Jeff Fagerholm  
Haiku

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Ms. Karen Faria  
145 South Holokai Road  
Haiku, Maui, Hawaii 96708

Dear Ms. Faria:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your transmittal which we received on March 29, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We have the following response to your comments.

You have suggested that a policeman direct traffic at the Hana Highway-Baldwin Avenue intersection. However, the traffic study in the EA notes that there are factors which limit the amount of cars which can be processed through Paia Town. These include diagonal parking, left-turning vehicles and pedestrians crossing the street. Thus, these constraints would still limit the amount of traffic which can be processed through the intersection.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion targeted three (3) months after start of completion.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Ms. Karen Faria  
March 2, 2005  
Page 2

Thank you for your input.

Sincerely,

  
MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-faria

'04 MAR 29 12:47  
 COUNTY OF MAUI  
 PUBLIC WORKS  
 Address

Paia Mini-Bypass

Karen ~~Barbano~~  
 145 So. Holokai Rd. Haiku

Phone Number 572-9675  
 e-mail address

954  RUSH  
 DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
--	------	--------	--------	----------	------	------

DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
DEF. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>				
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>				
ADD/DET	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Return to \_\_\_\_\_ Due \_\_\_\_\_  
 Ref. No. \_\_\_\_\_  
 By: \_\_\_\_\_ Date: \_\_\_\_\_

I have the following comments on the Paia Mini-Bypass Project.

Time for studying a temporary solution is over. WE NEED TO DO  
 SOMETHING NOW!

Can we have a traffic cop for at least 1 hour for Monday thru Friday  
 say at 5 - 6pm to direct traffic? I see the police there anyway in  
 Paia town. It may make waiting for a temporary solution easier  
 to take. I'm soooo sick of the traffic!

Please turn in this sheet with your comments to a representative of the County of Maui  
 Department of Public Works and Environmental Management. Alternatively, it may be  
 mailed to:

Mr. Gilbert Coloma-Agaran  
 Director  
 Department of Public Works and Environmental Management  
 200 South High Street  
 Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. Lloyd Fischel  
Fragrant Orchids of Maui  
20 North Lanikai Place  
Haiku, Maui, Hawaii 96708

Dear Mr. Fischel:

Thank you for your letter dated April 20, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

With regard to your statement that the project has moved forward without following the law regarding documentation, analysis and community involvement, we would like to emphasize that all applicable laws are being followed in the processing of the Environmental Assessment.

There have been a number of concerns raised by members of the public that the Baldwin Avenue terminus of the mini-bypass makai of the Paia Post Office may conflict with traffic from the Paia Post Office driveway. Thus, in response to public comments, we will be revising our plans to move the Baldwin Avenue terminus above or mauka of the Paia Post Office. The Final Environmental Assessment will reflect the change.

An additional traffic assessment was done in order to address traffic impacts on Baldwin Avenue. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

Mr. Lloyd Fischel  
March 2, 2005  
Page 2

With regard to your comments relating to the Governor's Advisory Committee, we note that we have met with the Committee at two (2) public meetings. We believe that we have addressed concerns raised by the Committee as it relates to the project.

We would like to note that a permanent Paia Bypass is a needed facility. The Draft EA discusses the use of Kala Road and Sunnyside Road as an alternative action to the mini-bypass. Kala Road is an old government road which extends from Hana Highway to Sunnyside Road which then extends to Baldwin Avenue just mauka of the Paia Mill. Sunnyside Road is a privately owned road which must be purchased if it is desired for public use. There are also a number of other issues which need to be resolved before use of Kala/Sunnyside Road by the public is warranted. These include safety and liability issues relating to the current agricultural use of Sunnyside Road. There are also planning and route selection issues which need to be discussed for the Haiku leg of the bypass. An engineering analysis also needs to be done which discusses drainage, desired cross section of the road, plantation crossings and intersection design. Our analysis showed that it could take approximately six (6) to eight (8) years before the necessary planning, public hearings, environmental review, permits, design, and funding can be obtained in order to address the foregoing issues. Based on other roadways recently constructed on the island of Maui, an estimated roadway cost for the Kala/Sunnyside Road segment (not including a Haiku leg) could range from \$25.9 to \$37 million.

The EA process will comply with all applicable provisions of the Hawaii Administrative Rules. The mini-bypass project is intended to address afternoon commuter traffic coming towards Paia and should have no significant effect upon after school traffic, the use of the Youth and Cultural Center, or beaches adjoining the town of Paia.

Regarding the cost of the mini-bypass project which is estimated at \$650,000, we believe that the use of the project should have a beneficial effect on mitigating traffic impacts during peak afternoon hours and justifies the cost. We would like to clarify that State funds are being proposed for the Hana Highway improvements. Improvements for the single-lane roadway on A&B's property will be paid for by A&B and will remain the property of A&B. No public funding is being proposed to be utilized for portions of the mini-bypass located on private property. County funds are being proposed for improvements within Baldwin Avenue.

Regarding the existing traffic signal at Hana Highway and Baldwin Avenue, the timing has been and can be adjusted, as appropriate. However, as the traffic study in the EA notes, there are factors which limit the amount of cars which can be processed through the intersection. These include diagonal parking, left-turning vehicles and pedestrians crossing the street.

Mr. Lloyd Fischel  
March 2, 2005  
Page 3

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,

  
MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-fischel

1

DEPT. OF PUBLIC WORKS

	INFO	ACTION	ISSUE	COMMENTS	COPY	FILE
--	------	--------	-------	----------	------	------



'04 APR 23 P 3:43

COUNTY OF MAUI  
PUBLIC WORKS

# Fragrant Orchids Of Maui

Mr. Milton Arakawa  
Dept. of Public Works

DIRECTOR						
DEP DIR						
PERS.						
STAFF/CE						
PLANNING						
INSPECTION						
SOLID W.						
ENGR.						
HAZS.						
SLO/OT						

April 20, 2004

FAX: 270-7855 (2) pages total

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_

Dear Milton:

The Pa'ia mini-by-pass project proposed by A&B is greatly flawed in design and intent. Many, many residents in Pa'ia and Haiku have discussed this matter, and we have come to this conclusion.

Furthermore, the project has moved forward without following the law regarding documentation, analysis, and community involvement.

I urge you to withdraw the Draft EA. If you are looking for community support, there are many things to consider.

For example, we have heard that discussion is underway to move A&B's proposed road mauka side of the Post Office on Baldwin. This decision seems to have been made without a study, and thereby indicates that the project is subject to personal opinion. Using the same logic, the opinions of the residents should weigh more heavily than the statistic too. At the Pa'ia Community Center, the record shows that, you stated that the department developed its current plan based on a study (the one sponsored by A&B). This study failed to show the absurdity of putting the road makai of the Post Office, and now you have changed the plan to accommodate this newfound observation.

The study also indicated that a great percentage of persons turning onto Baldwin Ave. from Hana Highway were driving to Makawao. When the Governor's Advisory Board on Maui heard of this in yesterday's hearing, members who live in Makawao confirmed that they and everyone they know that lives in the area uses Haleakala Highway not Baldwin Avenue when driving to Makawao after work. For this reason and others, one committee leader stated emphatically, "This project stinks....", and by unanimous vote agreed to ask the Governor to take action towards by time so that your department can create a plan that realistically can succeed, and with the community involved in the process.

In my humble opinion, it would be irresponsible to make the financial

20 N. Lanikai Pl., Haiku, HI 96706

Phone: 808-572-2289

appropriation at this time. I request that you honor your citizenry and the aina, and take the leadership necessary to protect all of us from misuse of taxpayer funds, needless erosion of the environment and harm to the beauty of Maui based on what is increasingly being seen by some as politically motivated decisions.

Your attendance at the last meeting was fine, but you did not answer many questions. And, you deferred questions that the majority attending thought were germane. The record shows that you did not answer almost a dozen questions, in fact. It will be good to have the opportunity to discuss this project more at the next meeting.

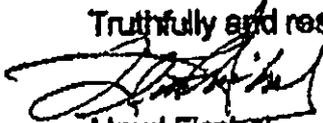
Having said this, please understand that it is our intent that these comments and any direction that the department takes should in no way delay progress on expediting a real solution, which puts a major road mauka of the town, thereby protecting the fragile coast and the unique character of the town of Pa'ia.

We also expect that the department will make all efforts to abide by the Hawaii Administrative Rules, plan for the use of roadways in league with the needs of children returning home from school, the use of the Youth and Cultural Center, as well the use of those public beaches adjoining the town of Pa'ia by our citizens and visitors.

Lastly, from a business standpoint, the use of over \$700,000 for only 10 hours per week of usage is sadly ignorant. Further, the majority of taxpayer money slated for this project is going into property improvement that we the people do not own nor control. Surely, our elected officials have better business savvy than this. If not, we will have to change them at the ballot.

There are many other uses of the money that will get the traffic moving better on Hana Highway. One would be to upgrade the dismally old traffic signal. It's amazing that your department did not do this prior to now. Further, we have identified four other things that would add greatly to relieve the traffic problem, without rolling the dice with taxpayer funds.

Truthfully and respectfully,



Lloyd Fischer

Board of Directors:  
Haiku Community Association  
Maui Flower Growers Association  
Maui Orchid Society

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**

200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. David Futch  
P. O. Box 792050  
Paia, Maui, Hawaii 96779

Dear Mr. Futch:

Thank you for your e-mail transmittal of March 25, 2004 relating to the Draft Environmental Assessment of the Paia Mini-Bypass. We would like to provide the following response:

It should be emphasized that the permanent Paia Bypass is a needed facility. Planning should be expedited in order to facilitate a solution. However, a permanent bypass would require the resolution of a number of outstanding issues before it can become a reality. For example, our analysis of the use of Kala Road and Sunnyside Road which can be considered one possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be completed in order to comply with necessary planning, studies, public hearings, permits, and design requirements, as well as to obtain funding. An order of magnitude cost to construct a two lane Kala/Sunnyside Road, from Hana Highway to Baldwin Avenue, is estimated between \$26 to \$37 million.

Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion scheduled for three (3) months after the start of construction.

With regard to your comment on the reduction of available parking for Paia Park, it is noted that parking occurs in the area along the fence on the makai side of the Hana Highway right-of-way near Paia Beach Park, the mauka side of Hana Highway, along Puna Road, and on the makai side of Hana Highway extending closer to Baldwin Park.

Mr. David Futch  
March 3, 2005  
Page 2

With the proposed improvements, there would still be space for angle parking along the fence although parallel parking which currently takes place on the mauka side of Hana Highway would be curtailed. However, informal parking space is still available on the makai side of Hana Highway extending closer to Baldwin Park.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia\_futch

**From:** "David Futch" <united@tiki.net>  
**To:** <Milton.Arakawa@co.maui.hi.us>  
**Date:** 3/25/04 8:22PM  
**Subject:** Paia By Pass!

Aloha Milton,

It is of great importance that serious consideration be taken to the potential ill effects caused by the "mini" bypass. In my opinion the community that will be impacted should be given a voting opportunity, with full disclosure of intent prior. While there will likely be minimal relief to the traffic situation at hand (and quite possibly an actual negative effect), there is a real possibility that the real bypass solution will either be delayed or cancelled - because of the mini. The funds dedicated to the mini, should be used solely for the real bypass, the best and only solution.

Take a vote of the community, you will see that the majority will not vote for the mini.

Meanwhile, another study should be conducted which includes both directions of traffic at all times of the day. I have experienced this bottleneck from both directions at various times of the day.

A byproduct of the mini is losing parking for the Paia park, which is counterproductive to all that enjoy it. Parks and open space is the key to retaining our lifestyle. The Paia Youth and Cultural Center will also be impacted.

Take a vote, you will see that the Maui community does not want San Diego or Los Angeles in the heart of our town.

We, the community, are looking for planning that provides relief to the congestion. We are not looking for a "band aid", as the "mini" is reputed to be.

It is imperative to consider the impact to the community. It is imperative to consider the impact to the visitor, the source of our livelihood. The effects of an ill planned mistake will be irreversible. Aside from the sizable investment wasted - especially when it becomes discovered that the traffic relief was not realized, the more important issue is that the land itself will be irreversibly negatively impacted .

We don't need both a mini and a real bypass. We need just one, the one that will actually solve the problem we now have, the real by pass. Do not allow a short sighted view to become a reality we all will regret.

Sincerely,

David K. Futch

Haiku Resident

---

<<http://promos.hotbar.com/promos/promodll.dll?RunPromo&El=hotbar%5felement%3bst%3b&SG=sg362&RAND=56854&partner=hotbar>> Upgrade Your Email - Click here!

CC: "Cadiz" <info@hstwindsurfing.com>

11/11/11 11:11:11 AM

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. Daniel Grantham  
HC1, Box 47  
Haiku, Maui, Hawaii 96708

Dear Mr. Grantham:

Thank you for your letter which we received on April 21, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

An additional traffic assessment was done in order to address traffic impacts on Baldwin Avenue. Moreover, we have done an additional traffic assessment in order to address traffic impacts on Baldwin Avenue. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

We would like to note that a permanent Paia Bypass is a needed facility. The Draft EA discusses the use of Kala Road and Sunnyside Road as an alternative action to the mini-bypass. Kala Road is an old government road which extends from Hana Highway to Sunnyside Road which then extends to Baldwin Avenue just mauka of the Paia Mill. Sunnyside Road is a privately owned road which must be purchased if it is desired for public use. There are also a number of other issues which need to be resolved before use of Kala/Sunnyside Road by the public is warranted. These include safety and liability issues relating to the current agricultural use of Sunnyside Road. There are also planning and route selection issues which need to be discussed for the Haiku leg of the bypass. An engineering analysis also needs to be done which

Mr. Daniel Grantham  
March 2, 2005  
Page 2

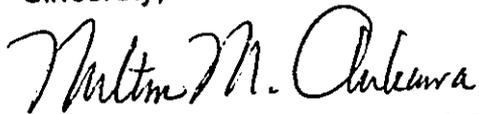
discusses drainage, desired cross section of the road, plantation crossings and intersection design. Our analysis showed that it could take approximately six (6) to eight (8) years before the necessary planning, public hearings, environmental review, permits, design, and funding can be obtained in order to address the foregoing issues. Based on other roadways recently constructed on the island of Maui, an estimated roadway cost for the Kala/Sunnyside Road segment (not including a Haiku leg) could range from \$25.9 to \$37 million.

Issues relating to possible future development between Hana Highway and a mauka bypass highway, as well as any expansion next to Maui Marketplace are not part of the mini-bypass application. Future applications would be subject to permit processes which are in effect at the time of application. County community plans are proposed to be updated within the next few years. Individual desires to retain or revise the land uses should be discussed at that time.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-grantham

From Daniel Grantham



RUSH

DEPT. OF PUBLIC WORKS	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
-----------------------	------	--------	--------	----------	------	------

Milton Arakawa '04 APR 21 A9:46  
 Dept. of Public Works  
**Regarding Paia Mini-bypass proposal**

Dear Milton,

The following are some further thoughts in addition to verbal comments made at the public meeting in Paia on the proposed mini-bypass from the entrance of Paia to the post office.

DIRECTOR						
DEP. DIR						
PERS.						
STAFF CE						
LUCA						
WW RECL.						
SOLID W.						
ENGR.						
HWYS.						
SECTY.						
Return to						
Due						

The traffic going through Paia is continuing to Kuau, Ho'okipa Beach, Haiku and beyond.

Ref. No. \_\_\_\_\_  
 By: \_\_\_\_\_ Date: \_\_\_\_\_

The traffic turning up Baldwin Ave. from Hana Highway is mostly going into Paia for business, shopping and residential purposes. Relatively few cars continue on up Baldwin to Makawao.

There is only minor traffic between Paia and Makawao unless the Haleakala Highway is closed.

The proposed mini-bypass will not much improve existing traffic, in fact, it may worsen it by putting more cars on Baldwin instead of guiding them to the parking lot on Hana Highway at the entrance to Paia.

It seems clear from the public meeting that there is strong community consensus on the need for a real bypass that crosses Baldwin up higher, above the mill, going on to connect Hana Highway after passing through Hamakua Poko. Such a bypass would divert most traffic going on beyond Kuau.

Strong community concerns about unwanted development between the existing Hana Highway and a mauka bypass need to be addressed up front in community plans.

There also appears to be strong consensus that existing government roads being used as cane roads would provide ready made roadways for a real bypass.

It does not seem it would be too difficult to arrange crossings for HC&S; after all, they now cross a busy Hana Highway as needed.

From Daniel Grantham



Page 2 of 2

Finally, it would seem to be sensible planning to hold off on A&B's 140 acre retail and light industrial expansion next to Maui Marketplace, between Hana Highway, Dairy Road and Pu'unene until there is a real bypass for Paia to help deal with the expanded traffic this development would bring.

Thank you for the opportunity to comment,

Daniel Grantham  
HC1, Box 47  
Haiku, HI 96708  
808-572-4571

Milton Arakawa - Paia mini-bypass comments

**From:** Daniel Grantham <dannyg@flex.com>  
**To:** <Milton.Arakawa@co.maui.hi.us>  
**Date:** 4/21/04 9:04AM  
**Subject:** Paia mini-bypass comments

Milton Arakawa  
Dept. of Public Works

Dear Milton,

The following are some further thoughts in addition to verbal comments made at the public meeting in Paia on the proposed mini-bypass from the entrance of Paia to the post office.

The traffic going through Paia is continuing to Kuau, Ho'okipa Beach, Haiku and beyond.

The traffic turning up Baldwin Ave. from Hana Highway is mostly going into Paia for business, shopping and residential purposes. Relatively few cars continue on up Baldwin to Makawao.

There is only minor traffic between Paia and Makawao unless the Haleakala Highway is closed.

The proposed mini-bypass will not much improve existing traffic, in fact, it may worsen it by putting more cars on Baldwin instead of guiding them to the municipal parking lot on Hana Highway at the entrance to Paia.

It seems clear from the public meeting that there is strong community consensus on the need for a real bypass that crosses Baldwin up higher, above the mill, going on to connect Hana Highway after passing through Hamakua Poko. Such a bypass would divert most traffic going on beyond Kuau.

Strong community concerns about unwanted development between the existing Hana Highway and a mauka bypass need to be addressed up front in community plans.

There also appears to be strong consensus that existing government roads being used as cane roads would provide ready made roadways for a real bypass.

It does not seem it would be too difficult to arrange crossings for HC&S; after all, they now cross a busy Hana Highway as needed.

Finally, it would seem to be sensible planning to hold off on A&B's 140 acre retail and light industrial expansion next to Maui Marketplace, between Hana Highway, Dairy Road and Pu'unene until there is a real bypass for Paia to help deal with the expanded traffic this development would bring.

Thank you for the opportunity to comment,

Daniel Grantham  
HC1, Box 47

Haiku, HI 96708  
808-572-4571

CC: <hstcadiz@maui.net>

11/11/11 10:11 AM

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Ms. Cindy Hanscam  
P. O. Box 790644  
Paia, Maui, Hawaii 96779

Dear Ms. Hanscam:

Thank you for your letter dated April 15, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response.

We would like to note that a permanent Paia Bypass is a needed facility. The Draft EA discusses the use of Kala Road and Sunnyside Road as an alternative action to the mini-bypass. Kala Road is an old government road which extends from Hana Highway to Sunnyside Road which then extends to Baldwin Avenue just mauka of the Paia Mill. Sunnyside Road is a privately owned road which must be purchased if it is desired for public use. There are also a number of other issues which need to be resolved before use of Kala/Sunnyside Road by the public is warranted. These include safety and liability issues relating to the current agricultural use of Sunnyside Road. There are also planning and route selection issues which need to be discussed for the Haiku leg of the bypass. An engineering analysis also needs to be done which discusses drainage, desired cross section of the road, and intersection design. Our analysis showed that it could take approximately six (6) to eight (8) years before the necessary planning, public hearings, environmental review, permits, design, and funding can be obtained in order to address the foregoing issues. Based on other roadways recently constructed on the island of Maui, an estimated roadway cost for the Kala/Sunnyside Road segment (not including a Haiku leg) could range from \$25.9 to \$37 million.

Ms. Cindy Hanscam  
March 2, 2005  
Page 2

Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion scheduled three (3) months after start of construction.

*It is noted that an additional traffic assessment was done in order to address possible traffic impacts on Baldwin Avenue which may result from the Paia Mini-Bypass. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.*

There have been a number of concerns raised that the Baldwin Avenue terminus of the mini-bypass makai of the Paia Post Office may conflict with traffic from the Paia Post Office driveway. Thus, we will be revising our plans to move the Baldwin Avenue terminus above or mauka of the Paia Post Office. The Final Environmental Assessment will reflect the change.

The right shoulder lane on Hana Highway would be restricted from use as a passing lane during closed hours by use of signage. Should there be non-compliance, then enforcement by the Police Department can be requested.

We would be open to discussing signage in terms of reducing the number of signs entering Paia Town.

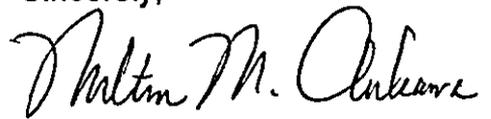
Regarding usage of the interim road as a community walk/bikeway once the Paia Bypass comes to fruition, we believe that any alternative uses of the road need to be discussed in concert with a comprehensive environmental impact analysis done for the Paia Bypass.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Ms. Cindy Hanscam  
March 2, 2005  
Page 3

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-hanscam

Paia Mini-Bypass

RUSH **1189**

04 APR 16 AIO:42

COUNTY OF MAUI  
PUBLIC WORKS

Address

CINDY HANSCAM

PO Box 790644

PAIA, HI 96779

Phone Number

579-9246

e-mail address

\_\_\_\_\_

DEPT. OF  
PUBLIC  
WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DEP. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>DOT/ARB</b>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

4/20  
4/16/04  
Jmked  
4/16/04

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_

By: \_\_\_\_\_ Date: \_\_\_\_\_

I have the following comments on the Paia Mini-Bypass Project.

see attached - 3 page letter

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran **Fax 270-7955**  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

April 15, 2004

Mr. Gilbert Coloma-Agaran, Director  
Department of Public Works  
200 South High Street  
Wailuku, HI 96793

Dear Mr. Coloma-Agaran:

**RE: Paia Mini-Bypass**

**Background**

Four years ago, the community was faced with addressing mounting traffic through Lower Paia Town. A&B stepped in to offer an interim reliever or connector road at little cost to the community. Unfortunately, the offer caused a misunderstanding by the DOT and prompted them to lower the priority of the Paia Bypass.

At that time, the community was open to an interim road and the Paia Main Street adopted it as a potential traffic calming measure knowing full well that its impact would be minimal as the majority of traffic was Haiku bound. A few public meetings that were held made mention of the reliever road, but until now, no formal public presentation of the road or its design had been made.

Now over four years later, we have been presented with a proposal for a redesigned road called the Paia Mini-Bypass. It includes a lengthy parallel lane on Hana Highway with numerous signs at the entryway to Paia and at a considerable cost to the taxpayers. It also egresses at a point already expressed by many as impractical as it puts Post Office left turn traffic head to head with right turns from the proposed exit and could exacerbate backups already occurring on Baldwin Avenue.

**Discussion**

First, it can be stated that since this road is late in coming this proposed interim measure may be obsolete since the Paia Bypass project is back on track. The data presented for the Mini Bypass needs to be updated and should be adjusted for seasonal fluctuations. This will provide more evidence to determine the road's true impact on traffic and will help determine optimal hours of use.

Mr. Kawahigashi, Transportation Engineer, on whose recommendations the redesigns were based, acknowledged that further study might be needed. In his letter dated May 22, 2002 that is included in the draft EA, he states the following:

"The above expresses our opinions of the merits of the proposed by-pass road without conducting a simulation model of the before and after effects of implementing the proposed by-pass road."

Second, the idea for the road was originally conceived by A&B as a "gift" to the community. Shortly thereafter, it was touted by A&B as a connector road to their Baldwin Avenue commercial center. Their extravagant plan was immediately found to be against the community country town guidelines and was met with much trepidation. A&B subsequently put their plans on hold until an undetermined future date.

The proposed redesigned road, the "Paia Mini-Bypass", is just an extravagant driveway connecting A&B's Baldwin property directly to Hana Highway. In light of this, how can the community be asked to pay for this? The community will be footing the bill for the "gift" that will ultimately benefit only A&B.

Third, the interim road that was envisioned by the community was a road that exited past the Post Office with a simple multi-car stack Hana Highway turn lane. The location of the exit was directly expressed in a community-generated petition done in May 2001 that consisted of over 500 signatures that was presented to Mr. Sairot and Mr. Goode in a public meeting.

The exit as proposed is in direct interference with Post Office traffic and is at the point where current backups on Baldwin Avenue begin.

Fourth, the proposed Hana Highway widening to allow for a lengthy stacking lane needs to be separated somehow to discourage using it as a right-hand passing lane. Time and time again, people have used the right shoulder grassy area as a passing lane. An asphalt lane will surely attract and encourage more of this kind of unsafe and illegal behavior. A narrow median or island about a foot across may be needed. Concrete barriers, orange coning, or flashing light sawhorses would be unsightly.

Fifth, if we find that the interim road can provide a benefit after design issues are addressed, we should incorporate a long-term community use for the interim road to further justify the large unexpected expenditure of taxpayer funds. Need to be included are provisions to return the affected Hana Highway portion to its original location and re-striping the road to comprise a walking/bike path on the makai side of the road which then crosses over and continues through the entire length of the interim road. This will not only confirm its temporary status, but will provide long-term benefit to the community.

A&B expressed giving to the community and this would be a great way to contribute to the overall goal of a Neighborhood Town Center with a focus on greenways and bikeways through the Hana Highway/Baldwin Avenue North Shore corridor.

#### Conclusions

We, as community members and civil servants, need to feel confident that this interim measure will be a true benefit to the community at large and will have minimal impact on its neighbors and the Paia Town long-term vision.

It is the consensus of the community that a permanent Paia Bypass is the only real solution and anything other than this is temporary in nature and a band-aid at best. Thus it is important that any taxpayer money used for these interim measures be used wisely and to its highest and best use. We can only accomplish this through prudent design that addresses community concerns and reflects the community's plans and vision.

Recommendations

Based upon these findings, proposed are the following:

1. Get more data on Baldwin Avenue traffic flow and make a true assessment of the need for and best use of this road. Keeping in mind that the Paia By-Pass project is back on track and is in its first phase. In the end, it may be more prudent to redirect these funds to Kaia/Sunnyside Road, a true Mini By-Pass road, which is already incorporated in designs of the Paia Bypass.

Excluding Option 1...

2. Begin redesign of the proposed Mini Bypass road by relocating the Baldwin Avenue exit mauka of the Post Office and reducing disruption to the parking shoulder adjacent to Paia Baby Park.
3. Remove any possibility of using the right lane as a passing lane during closed hours.
4. Work with organizations such as Paia Main Street and other organizations to reduce sign pollution and enhance the entryway to Paia Town.
5. Incorporate future plans for use of the interim road as a community walk/bikeway once the Paia Bypass comes to fruition.

I hope this letter is found to be helpful in addressing the concerns of the community and can be used to determine the best use of taxpayer funds. It is my desire to create a more practical interim road or move on towards a more comprehensive solution that will truly benefit the community in traffic relief.

If you have any questions, please call me at 808-579-9246.

Sincerely,



Cindy Hanscom  
Paia Resident

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Ms. Christina Hemming  
P. O. Box 791114  
Paia, Maui, Hawaii 96779

Dear Ms. Hemming:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your letter dated April 22, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response.

First, we would like to note that your reference to an "unadvertised rescheduled Paia Bypass meeting held at the Paia Community Center" is not true. A press release was issued by the County of Maui on March 5, 2004 announcing the public informational meeting on the mini-bypass. On March 6, 2004, the Maui News printed an article on the County Council's decision to fund planning monies for the project. In the article, the date, time and place of the informational meeting was announced. The public informational meeting was held on March 17, 2004. The meeting was not rescheduled.

There will be signage along the Hana Highway right-of-way in order to inform motorists that use of the shoulder lane is limited to afternoon peak hours. Enforcement on the use of the shoulder lane would be done by the Police Department.

With regard to your comment on pedestrian and bike pathways, and parking along the makai section of Hana Highway, it is noted that pedestrian and bike pathways from Paia Town to Paia Bay Parking Lot are still available through the Paia Beach Park. From the Paia Bay parking lot to Baldwin Beach Park, a four (4) foot shoulder would still be available for bicycle use. A grassed shoulder would also still exist for walkers. Informal parking presently takes place along the fence on the makai side of the Hana Highway right-of-way near the Paia Beach Park, the mauka side of Hana Highway, along Puna Road, and on the makai side of Hana Highway on the Wailuku side of Puna

Ms. Christina Hemming  
March 2, 2005  
Page 2

Road. With the proposed improvements, there would still be space for angle parking along the fence although parallel parking which currently takes place on the mauka side of Hana Highway would be curtailed. However, informal parking space is still available on the makai side of Hana Highway extending closer to Baldwin Park.

A contemplated walking/bikeway system in Paia which would connect the town to a future North Shore Regional Park is not a part of the proposed project. There is an area designated "Open Space" in the Paia-Haiku Community Plan which abuts the mauka boundary of the Hana Highway right-of-way in this vicinity. It is noted that the proposed Hana Highway shoulder lane is located entirely within the State right-of-way. The single-lane roadway extending mauka from the County parking lot traverses areas designated "Open Space" and "Agricultural". Roadways are not precluded within the Open Space designation and the portion of the project within the Open Space designation consists of a one (1) lane roadway which is quite minimal.

Your suggestion for a two (2) lane permanent road going from Baldwin Park to Baldwin Avenue should be considered as part of the discussion and analysis for a permanent Paia Bypass.

There is no linkage between the Paia Mini-Bypass and possible future projects such as A&B's commercial project. Any future proposal would be required to be considered under rules and regulations in existence at the time of application.

Drainage reports have been included in the Draft EA for review. The proposed development will not alter the natural drainage patterns of the area and will have negligible impacts upon existing drainage. However, this does not preclude the possibility of working with applicable government agencies such as the Natural Resources Conservation Service on upstream diversion or soil conservation measures in order to minimize runoff, flooding and erosional impacts on a regional basis.

In terms of a longer term solution to beach parking, we note that a portion of the land makai of Hana Highway between Paia Beach Park and Baldwin Beach Park is designated "Park". The future plan for this area should address issues relating to permanent facilities for beach parking.

Ms. Christina Hemming  
March 2, 2005  
Page 3

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-hemming

1d/2  RUSH

Christina Hemming  
PO 791114  
Paia, Hi 96779

'04 APR 23 P 2:53

COUNTY OF MAUI  
PUBLIC WORKS

Milton Arakawa  
Public Works  
Maui County

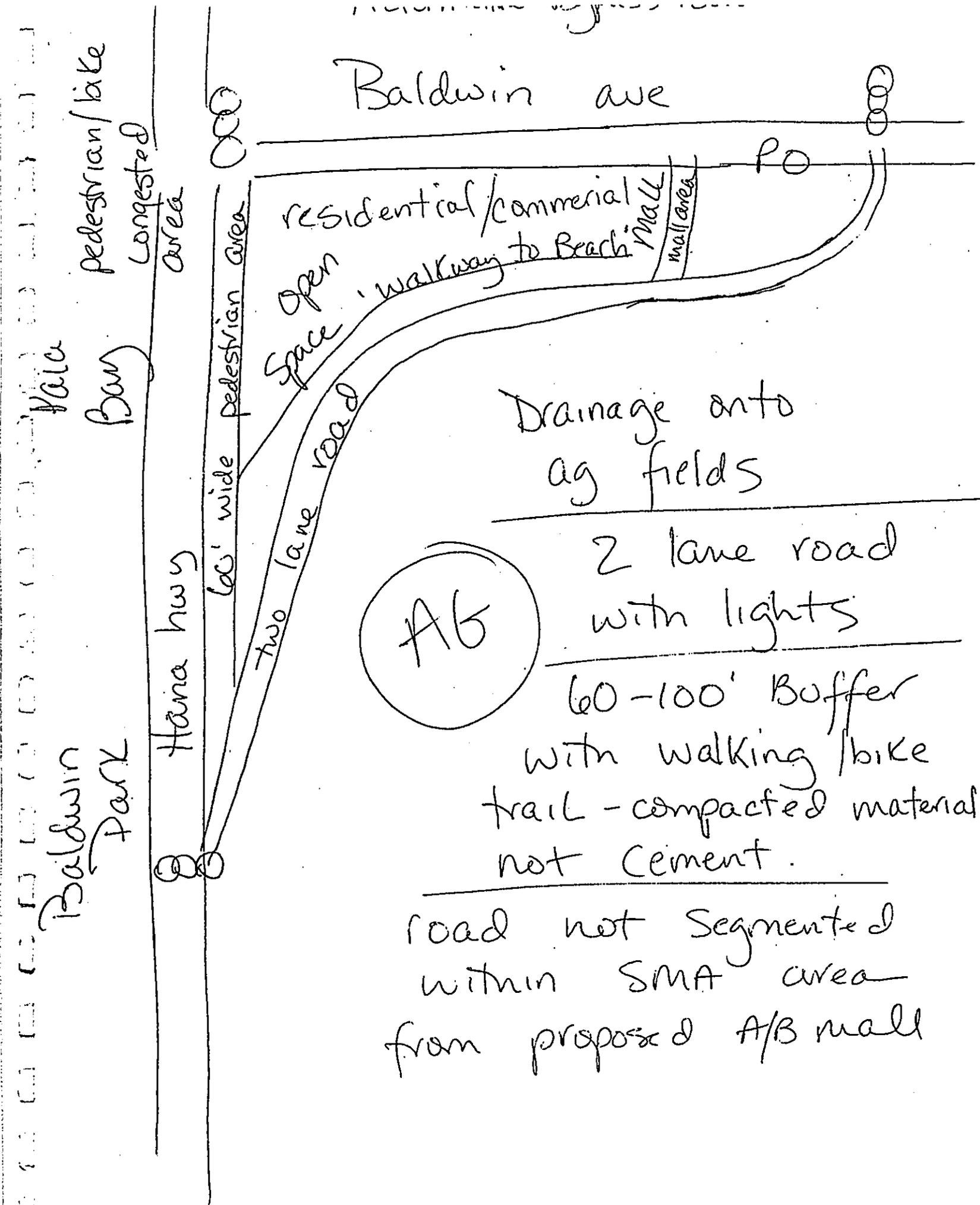
April 22, 2004

DEPT. OF PUBLIC WORKS	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input type="checkbox"/>	<input type="checkbox"/>				
DEP. DIR	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
PERS.	<input type="checkbox"/>	<input type="checkbox"/>				
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>				
LUCA	<input type="checkbox"/>	<input type="checkbox"/>				
WW RECL.	<input type="checkbox"/>	<input type="checkbox"/>				
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>				
HV/YS.	<input type="checkbox"/>	<input type="checkbox"/>				
SECTY	<input type="checkbox"/>	<input type="checkbox"/>				
LAB/REP	<input type="checkbox"/>	<input type="checkbox"/>				
Return to						
Due						
Ref. No.						

Thank you for taking the time to review these comments. I have copied the handwritten comments for the Project Overview presentation, given out at the unadvertised rescheduled Paia Bypass meeting held at the Paia Community Center. Some of the concerns raised are:

- ❖ The dangerous situation occurring when cars travel down the bypass acceleration lane, <sup>Daily</sup> only to find a locked gate, and the subsequent need to then cut back onto the Hana hwy just west of the exit to the Municipal Parking lot.
- ❖ The elimination of pedestrian and bike pathways, and parking along the makai (North) section of Hana hwy, creating a dangerous situation for (handicapped) tourists and locals alike who walk or bike from Paia town to Paia Bay parking lot.
- ❖ The lack of integral planning into a future walking/Biking pathway system in Paia, connecting the town with the "Northshore Regional Park".
- ❖ The area involved is Open Space in the Community Plan, and therefore should provide some sort of Open Space that is not cemented or a road. Although roads are allowed, a planted wide buffer, 60 feet with pathways, can be integrated into the "bypass road". Provide safe separation between the acceleration lane and the Hana Hwy, allowing for the "gate" to be put right along the Hana hwy clearly.
- ❖ No road is ever built for a temporary nature, so, the correct design should be done. A two lane "permanent" road, going from Baldwin Beach Park to Baldwin avenue, with a traffic light on either end.
- ❖ Ignoring the Cumulative Effects portion of the SMA rules, was heard in the lack of discussion or acknowledgement of the proposed entitlement request by A&B for a change in Zoning from Agriculture to Country Town/Business for a future "mall" (portions of parcels 61, and 18) to be located on Baldwin Ave. This is adjacent to the Post office, and where the proposed exit lane is for the bypass. Essentially the road would go through the mall's parking lot, and exit onto Baldwin Ave. A bike and walkway system can be required because the area is in the SMA area, and is subject to conditions.
- ❖ The drainage Plan for the road was not presented at the meeting, only a comment about discussing ideas for agricultural drainage areas with "Natural Resources Council", was provided. The drainage rules right now do not meet the Federal Non-Point Source pollution requirements. The current plan would increase upgrade sediments to pollute Paia Bay. The current plan is inadequate for future construction.
- ❖ Currently drainage is through culverts along the Hana hwy, which deposit road and agricultural runoff directly into Paia Bay contributing to a visually high amount of turbidity, and is usually brown color. There is heavy usage of the area by adolescent children and young adults, and presently there are infections, which happen.





Baldwin ave

pedestrian/bike  
congested  
area

Hana  
Bus

Baldwin  
Park

Hana hwy

60' wide pedestrian area

two lane road

residential/commercial  
open space  
walkway to Beach

mall  
mall area

PO

Drainage onto  
ag fields

A/B

2 lane road  
with lights

60-100' Buffer  
with walking/bike  
trail - compacted material  
not cement.

road not segmented  
within SMA area  
from proposed A/B mall

## I. PROJECT OVERVIEW

The State of Hawaii Department of Transportation (DOT), A&B Properties, Inc. and the County of Maui Department of Public Works and Environmental Management (DPWEM) are proposing the Paia Mini-Bypass Project in Paia, Maui, Hawaii. See Figure 1.

The project would create a one-way, single-lane roadway extending from the vicinity of Baldwin Park to the new Paia Post Office. From Baldwin Park, a shoulder lane is proposed within the Hana Highway right-of-way extending to the vicinity of the existing Paia Municipal Parking Lot, a lineal distance of approximately 2,800 feet or .53 mile. The Hana Highway shoulders will be widened and reinforced so that the vehicular travel lanes would be restriped approximately four (4) feet makai of the existing lanes. The vehicular travel lanes will remain 12 feet in width. A 12 foot wide shoulder travel lane will then be created on the mauka side of the travel lanes along with new four (4) foot wide paved shoulders. All work within this segment of the project is proposed within the existing Hana Highway right-of-way. See Figures 2, 3 and 4. *eliminating any walkway makai of Hana*

The shoulder lane would then turn mauka to a single-lane roadway near the existing municipal parking lot, skirting existing developed portions of Paia Town, then connecting with Baldwin Avenue just makai of the new Paia Post Office, a lineal distance of approximately 1,800 feet. A 12 foot wide travel lane with two (2) foot wide shoulders on each side are proposed. At the Baldwin Avenue connection, separate left- and right-turn lanes are proposed. The portion of the project which extends beyond the Hana Highway right-of-way to Baldwin Avenue involves TMK 2-5-5:portion of 18. Transition work within the Baldwin Avenue right-of-way including curb returns, resurfacing and striping are also proposed. See Figures 5, 6 and 7.

The intent of the project is to mitigate some of the existing traffic congestion on Hana Highway in the afternoon peak hour by providing an additional travel lane for vehicles desiring to turn right on Baldwin Avenue.

Hours of operation for the single-lane roadway are 4:00 p.m. to 6:00 p.m., excluding Saturdays, Sundays and holidays. During non-afternoon peak hours, the single-

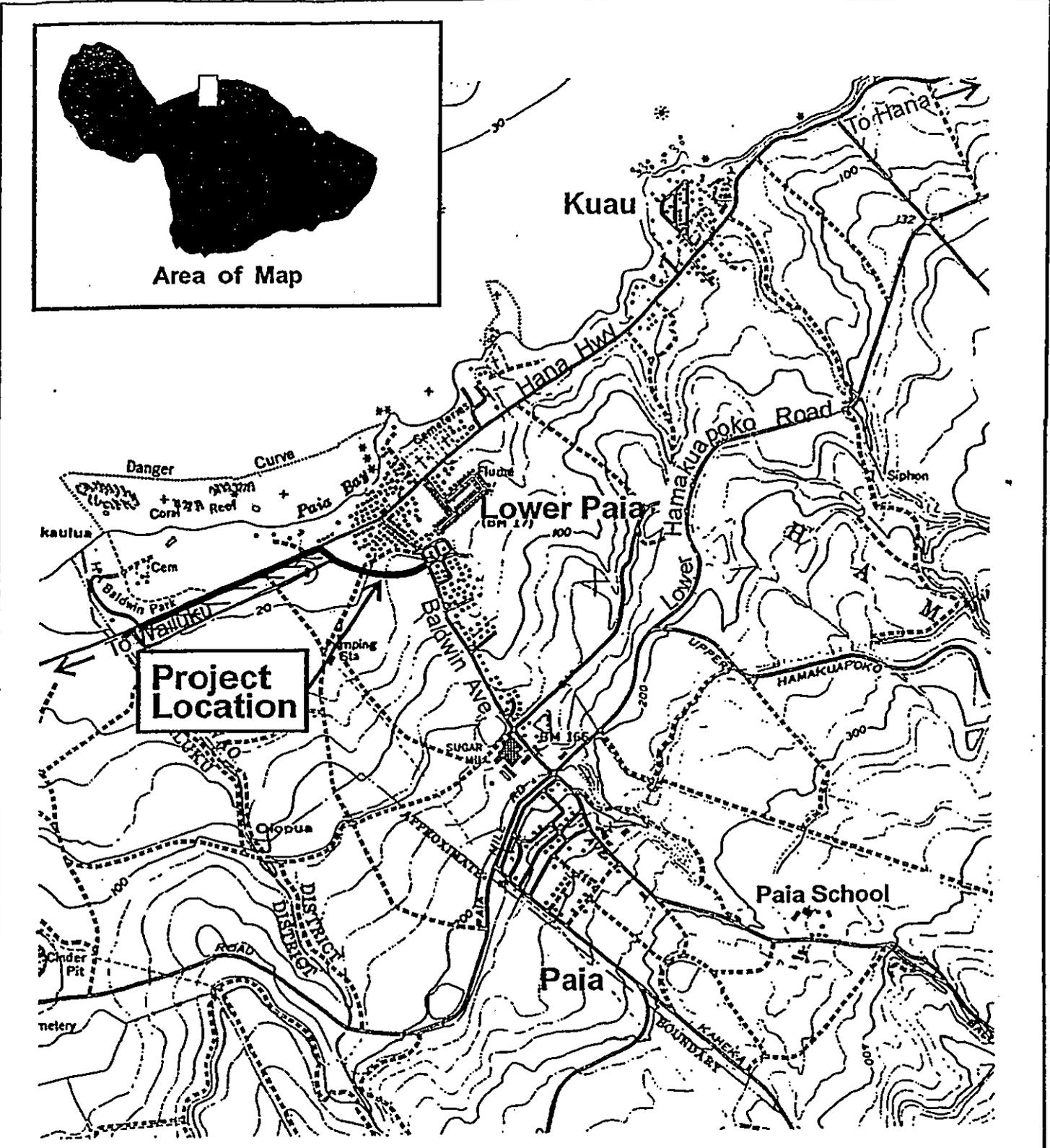
---

lane roadway will be gated near the municipal parking lot and at its Baldwin Avenue connection.

The State DOT is proposing to construct the portion of the project within the Hana Highway right-of-way which is a State highway. A&B Properties, Inc. will be building the portion of the project from Hana Highway to Baldwin Avenue. The County DPWEM will be constructing improvements within the Baldwin Avenue right-of-way, which is a County roadway. Since the project involves the use of State and County land and funds, an Environmental Assessment (EA) is being prepared pursuant to Chapter 343, Hawaii Revised Statutes (HRS).

The estimated construction cost for the entire project is \$650,000. Assuming all applicable approvals are obtained, construction is anticipated to begin during early fall 2004. Construction is anticipated to be complete approximately three (3) months after construction start.

*EA should be paid for by A/B, NOT  
County.*

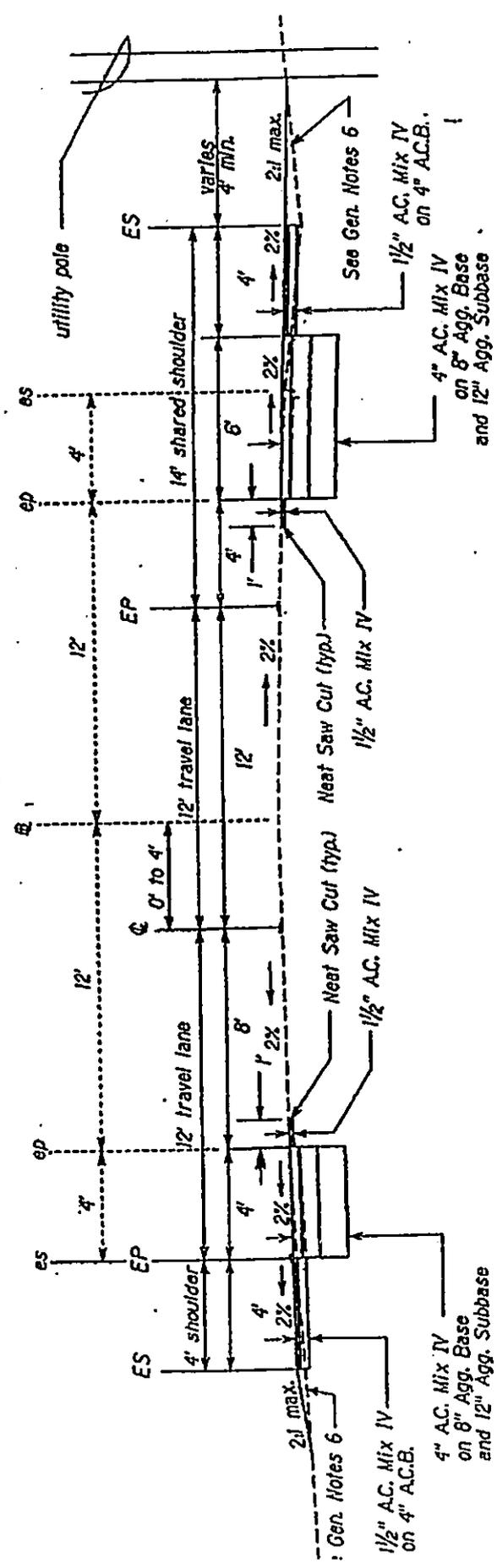


Source: U.S. Geological Survey

**Figure 1 PAIA MINI-BYPASS  
Regional Location Map**



1 in. = 2000 ft.



Source: State Department of Transportation

**PAIA MINI-BYPASS**  
**Typical Section Within Hana**  
**Highway Right-of-Way**

Figure 4

Not to scale

**PAIA MINI-BYPASS**

Informational Meeting

Paia Community Center  
March 17, 2004

County did not discuss EA requirement up front - used this packet as presentation only written comments

**Introduction**

- Joint project of the State Department of Transportation, A&B Properties Inc., and County Department of Public Works and Environmental Management.
- Construction of one-way, single-lane roadway from vicinity of Baldwin Park to the Paia Post Office.
- Purpose of project is to mitigate some of the existing traffic congestion on Hana Highway during the afternoon peak hour.
- Provides an additional travel lane for vehicles desiring to turn right on Baldwin Avenue.

only right

were encouraged with little discussion.

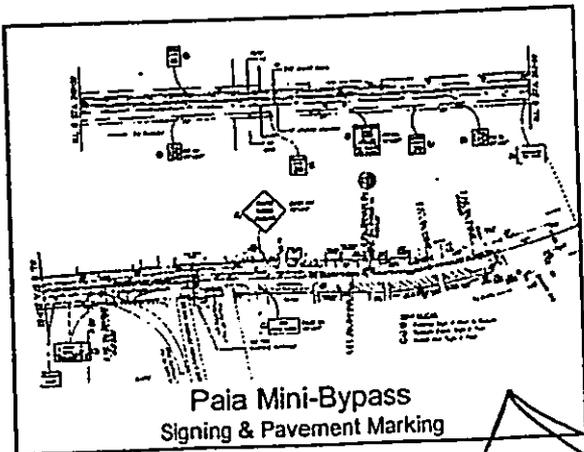
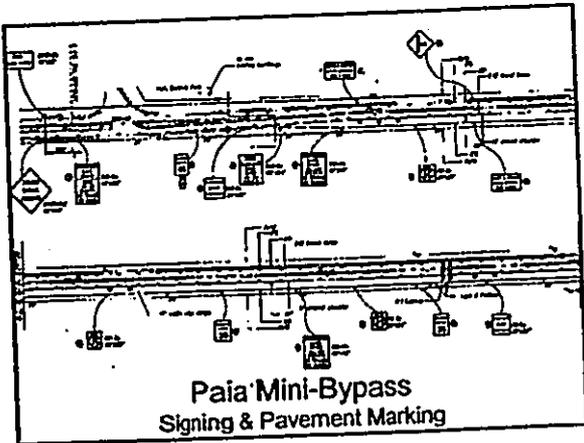
once a road is built it is NOT "temporary"

Travel lane on Hana hwy will cause more accidents due to reentry onto Hana hwy. If road is closed or traffic is backed up. Spilling onto Hana hwy.

**Paia Mini-Bypass  
Regional Location**

**Portion of Project Within  
Hana Highway**

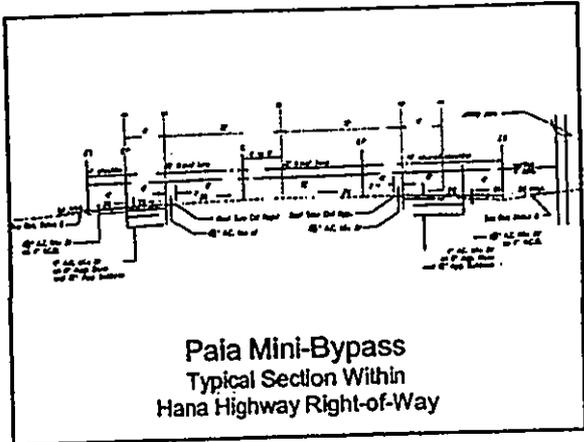
- From Baldwin Park, a shoulder lane is proposed within the Hana Highway right-of-way
- Extends to vicinity of Paia Parking Lot, a lineal distance of approximately 2,800 (0.53 mile)
- Hana Highway shoulders will be widened and reinforced so that vehicular travel lanes may be re-stripped approximately 4 feet makai of the existing travel lanes.
- Vehicular travel lanes will remain 12 feet in width. There will be a 10-foot wide shoulder lane on the mauka edge of the vehicular travel lanes with 4-foot shoulders on each side.



existing line placement  
allows for pedestrian  
traffic, bike lane,  
and angle parking  
These are needed

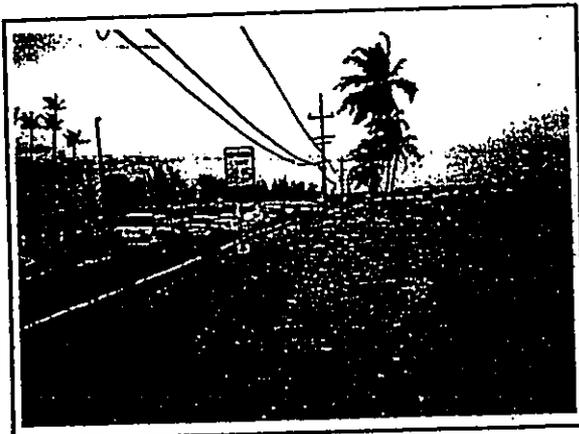
Road "should be"  
located 40' mauka  
of existing Hana hwy  
with bike lane and  
pedestrian sidewalk  
to allow for safe  
travel from Paia to  
Paia Bay and  
Baldwin Beach.

Where is drainage?



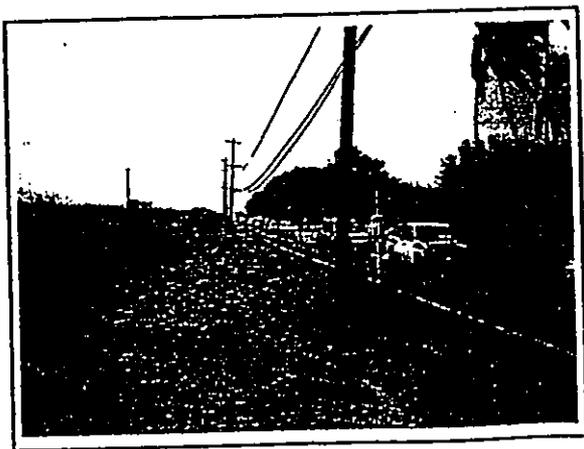
Drainage must be into cane fields and not into Colvert and into Paia Bay / ocean where turbidity and existing drainage is currently

Beach



Shows Hana hwy into Paia mauka side -- shows where travel lane can go there is room for sidewalk bike path along hwy

Widening road will increase ACCIDENTS from people speeding



Eliminates parking in very congested and highly used area -

← Area can be bikeway walkway / greenway w/ access road mauka

Gate can go along hana hwy - only open when road open



Area should be walkway  
gate should go @  
beginning of road  
@ Hana entry for  
entire length instead  
of here - too congested



Important parking  
area / bike lane  
pedestrian walkway -  
existing drainage now  
into Paia Bay  
where is  
Drainage Plan?



Important parking  
area - must be  
improved w/ pavers -  
not asphalt

Width must be retained  
already too congested



existing parking  
area ->  
prone to flooding  
with high use  
especially by  
biting companies

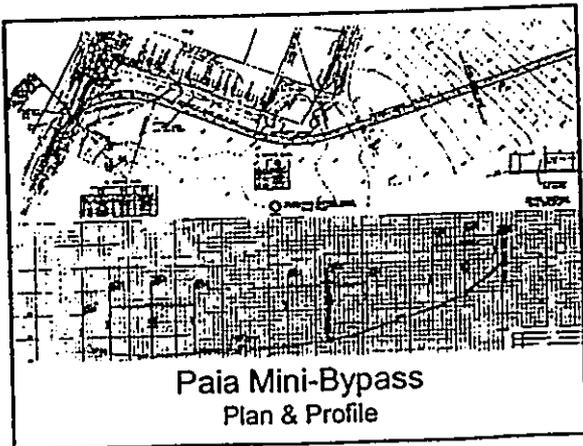
### Single-lane Roadway

- Single-lane roadway extends mauka near County Parking lot around Paia Town and connects with Baldwin Avenue just makai of the Paia Post Office.
- From Hana Highway to Baldwin Avenue, this portion of the project extends approximately 1,800 feet.
- A 12-foot wide travel lane with 2-foot shoulders are proposed on each side.
- At the Baldwin Avenue connection, separate left and right turn lanes are proposed.
- Transition work proposed within the Baldwin Avenue right-of-way include curb returns, resurfacing, and striping.

must be mauka of P.C.  
must be two lanes  
w/ light

only right turn onto  
Baldwin ave as  
Baldwin ave already  
F during daytime

Where is Drainage??



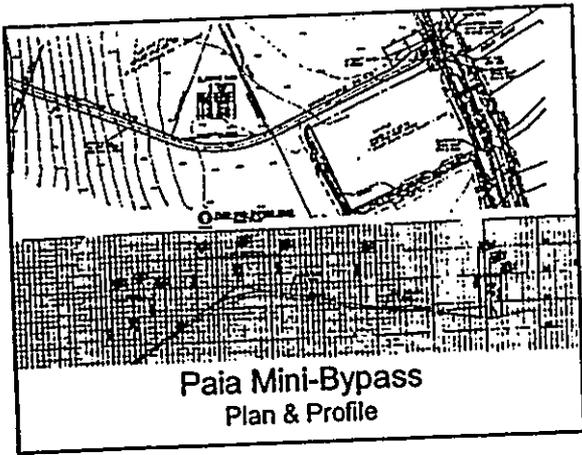
Part of SMA  
Segmentation for  
A/B mall

All drainage must  
be into Sugar field

NOT PAIA BAY

Where are trails/  
walkway connecting town w/ beach

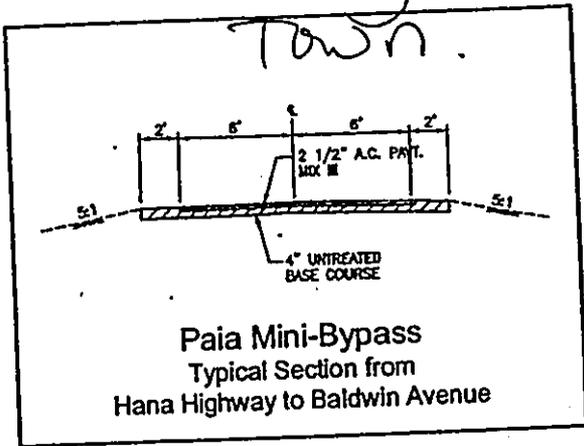
Located in O/S, Ag



Does NOT show  
A/B mall as  
part of project

Does NOT show  
Drainage  
P.O. too close  
Accidents !!

GOAL = Friendly walking



Where is Drainage?

If temporary - why  
not gravel?

If temporary / NOT permanent  
future expansion should not  
be allowed.

Gravel  
not pavement



exit onto Baldwin

Highly Congested  
Area

must be Right  
turn only or  
signalized intersection  
with 2 lane road.



traffic NOT  
Shown by this

picture -  
also - "temporary"  
sidewalk - where  
is permanent  
sidewalk?

F rated from 3-5pm  
traffic jam daily

### Paia Mini-Bypass

- Hours of operation for the single-lane roadway are from 4:00 pm to 6:00 pm, excluding Saturdays, Sundays and Holidays.
- During non-afternoon peak hours, the single lane roadway will be gated near the municipal parking lot and the Baldwin Avenue connection.
- Estimated construction cost for the entire project is \$650,000.
- Assuming all applicable approvals are obtained, construction may begin in Fall 2004. Completion should be approximately 3 months after the start of construction.

gate must be on  
hana hwy @  
beginning feeder lane.  
otherwise it will become  
speed lane w/  
Accidents

### Traffic Issues

- Existing traffic operation on Hana Highway breaks down through Paia due to:
  - 1) Diagonal parking
  - 2) Left turning vehicles
  - 3) Pedestrians
- Approximately 30 percent of traffic turn right on Baldwin Avenue.
- Single Lane Roadway from County parking lot to Baldwin Avenue, will not significantly reduce length of moving queue because access is located too close to the source of congestion.

too much development  
these aspects are  
part of a town center  
Right turn onto Baldwin  
to shop/ then return  
onto Hana hwy.

**Traffic Issues**

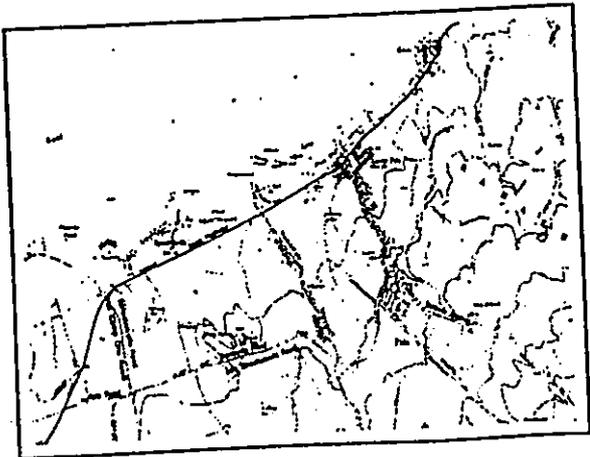
- While the traffic study recommended a parallel access road one mile outside of Paia Town, the proposed project would include a shoulder lane approximately 0.53 mile outside of Paia Town.
- This represents a significant increase in storage capacity compared to existing conditions.

NO it doesn't

**Evaluation of Kala/Sunnyside Road as an alternative**

Gravel and ←  
 Use Now

3-6 pm



Shoulder lane  
must be made  
of existing Hana hwy  
w/ green belt / walkway

This will prevent speed  
lane with Dangerous  
entry into Hana hwy

Will NOT improve  
overcrowded roads

Where is BUS  
TERMINAL / Carpool area

public road

has A/B paid taxes  
for use of road

Govt road

for the people

public / sugar can  
work together...



Gravel

Now



looks great!





Handwritten lines for notes.



Handwritten lines for notes.

**Kala/Sunnyside Road Alternative**

- Kala Road is considered an "Old Government Road".
- 1) Roadway was conveyed to the Territory of Hawaii by HC&S in 1935.
- 2) The State of Hawaii has made no determination as to ownership.
- 3) The County of Maui has not accepted dedication.
- 4) The road is 40 feet wide.

Accept dedication

move on it

Now w/ restrictions -

no development around road improvement

**Kala/Sunnyside Road Alternative**

- Sunnyside Road is privately owned.
  - 1) Draft EA needs correction. The draft mentions County Council Resolution No. 350 which abandons Spreckelsville Road. Maps inadvertently showed Spreckelsville Road as Sunnyside Road, and therefore, it was concluded that the County resolution applied to Sunnyside Road.
  - 2) However, Resolution No. 350 does not apply. The State or County does not have any documentation on ownership of Sunnyside Road. There are no records of dedication to the State or County.

NO public road

**Kala/Sunnyside Road Alternative**

- HC&S currently utilizes Sunnyside Road for it's plantation operations. The roadway is part of the plantation's main roadway network which links the fields to the Puunene Mill.

NO F/simple road

**Kala/Sunnyside Road Alternative**

- Should public use of Kala/Sunnyside Road be desired, there are a number of issues which need to be resolved
  - 1) Safety and Liability
    - a) HC&S uses Sunnyside Road for it's heavy equipment and truck traffic as well as cane haulers.
    - b) This is an agricultural road. Heavy equipment does not utilize public roadways except to cross. Dirt, mud, agricultural debris are typically left on roads.
    - c) Incompatible with joint public use. Separate plantation roadway is warranted.

Gravel

**Kala/Sunnyside Road Alternative**

- Should public use of Kala/Sunnyside Road be desired, there are a number of issues which need to be resolved
- 2) Planning and Route Selection
  - a) New 3.7 mile roadway would require public review through EA or EIS process.
  - b) The Kala/Sunnyside Road option should be planned in concert with K's Haku leg connecting back to Hana Highway.

---

---

---

---

---

---

---

---

**Kala/Sunnyside Road Alternative**

- Should public use of Kala/Sunnyside Road be desired, there are a number of issues which need to be resolved
- 3) Engineering Analysis
  - a) Kala Road approach to Hana Highway raises safety issues.
    - i) Intersection is not at 90 degrees.
    - ii) Located at a crest on Hana Highway which results in inadequate sight distance for the intersection.
  - b) Need for plantation crossings.

Change intersection  
easy

Gravel

---

---

---

---

---

---

---

---

**Kala/Sunnyside Road Alternative**

- Should public use of Kala/Sunnyside Road be desired, there are a number of issues which need to be resolved
- 3) Engineering Analysis (continued)
  - c) Two Lane or One Lane Road?
    - i) Need to remove the existing oil and gravel surface; Replace with appropriate base course and asphalt.
    - ii) Tree roots have also uplifted the Kala Road pavement.
  - d) Drainage issues need to be addressed
    - i) Several drainage ways traverse the 3.7 mile long roadway, including Kailua Gulch.

---

---

---

---

---

---

---

---

**Kala/Sunnyside Road Alternative**

- Should public use of Kala/Sunnyside Road be desired, there are a number of issues which need to be resolved

**4) Land Acquisition**

- a) Purchase of 60-foot ROW (2 lanes) would equate to approximately 24 acres.
- b) A one lane road would require 12 acres.
- c) Appraisal would determine fair market value.

---

---

---

---

---

---

---

---

**Kala/Sunnyside Road Alternative**

- Should public use of Kala/Sunnyside Road be desired, there are a number of issues which need to be resolved

**5) Estimated Cost of Kala/Sunnyside Road Alternative**

- a) Based on other roadways recently constructed on the island of Maui, a rough rule of thumb for the total project cost is 7-10 million dollars per mile.
- b) For 3.7 miles, total estimated roadway cost could range from \$26 million to \$37 million.
- c) For a one lane road, it may cost roughly half of a two lane alternative.

---

---

---

---

---

---

---

---

**Kala/Sunnyside Road Alternative**

- Should public use of Kala/Sunnyside Road be desired, there are a number of issues which need to be resolved

**6) Timing**

- a) A rough timetable to implement this alternative would be 6-8 years.

---

---

---

---

---

---

---

---

**Conclusion on the  
Kala/Sunnyside Road Alternative**

- While the Kala/Sunnyside Road may hold promise for resolving traffic congestion, there are a number of unresolved issues which would relegate this to a long term solution.

---

---

---

---

---

---

---

---

**Paia Mini-Bypass**

- Existing afternoon traffic congestion often backs up past Spreckelsville. With no action, it is anticipated that the queue will likely get longer and last longer over time.

---

---

---

---

---

---

---

---

**Paia Mini-Bypass**

- Paia Mini-Bypass is clearly an interim solution to traffic issues in the region.
  - 1) Project will divert approximately 30 percent of the traffic from Hana Highway.
  - 2) Construction can be completed by end of 2004.
  - 3) Cost is reasonable and funding is available.
  - 4) Will have minimal impacts upon Paia's businesses and residents.

---

---

---

---

---

---

---

---

### Paia Mini-Bypass

- A permanent Paia bypass is still a needed facility. Planning should be expedited in order to facilitate a solution.

---

---

---

---

---

---

---

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. Lance Holter  
P. O. Box 790656  
Paia, Maui, Hawaii 96779

Dear Mr. Holter:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your letter of March 26, 2004 providing comments on the Draft Environmental Assessment (EA) for the Paia Mini-Bypass, as well as your undated handwritten comments. We would like to provide the following response:

We have reviewed your letter detailing the history of the Paia Alternate Route. We would like to note that the permanent Paia Bypass is a needed facility. However, a permanent bypass would require the resolution of a number of procedural, as well as substantive issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one (1) possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be finished in order to address safety, planning, permitting, engineering and land acquisition issues. Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. Assuming required permits can be obtained, construction of the mini-bypass can be implemented during the second quarter of 2005 with completion three (3) months after the start of construction.

The traffic generation figures presented in your letter for several listed subdivisions are overstated based on generally accepted traffic engineering standards.

The Paia-Haiku Community Plan mentions planning, funding and constructing an alternative roadway mauka of Paia Town. It is noted that the subject mini-bypass proposal does not negate the need for a permanent alternative roadway around Paia Town. However, it would be able to address afternoon traffic congestion in the interim and is not contrary to Paia-Haiku Community Plan policies.

Mr. Lance Holter  
March 2, 2005  
Page 2

The Paia Haiku Community Plan contains a policy which states "discourage heavy truck traffic through Paia Town". Truck traffic currently utilizes Hana Highway through Paia. Truck traffic is being prohibited from using the shoulder lane (ten [10] feet) for safety reasons since it is not as wide as the regular travel lane (12 feet). The base course also has not been designed to withstand truck loads. Because Baldwin Avenue is relatively narrow at its connection to the mini-bypass, trucks would have difficulty in executing turn movements. It is noted that the proposal would not change existing truck traffic patterns.

The traffic letter report was based on a proposal to do a Paia Reliever Road which consisted of a single travel lane extending mauka from Hana Highway just before the Paia Municipal Parking Lot and connecting to Baldwin Avenue just makai of the Paia Post Office. The report concluded that the single travel lane would not significantly reduce afternoon traffic congestion because access is located too close to the source of congestion within Paia Town itself. While the traffic report recommended a parallel access road one (1) mile outside of Paia Town, the proposed project would include a shoulder lane approximately 0.53 mile outside of Paia Town. This represents a significant increase in storage capacity compared to existing conditions. The study was not for the Paia Post Office and A&B Commercial Area.

The cultural assessment that was included in the subject EA for the mini-bypass was also utilized for the Paia Post Office and an adjacent commercial and parking area. The conclusions of the cultural assessment are that sugar cane cultivation has most likely eliminated the possibility of the study area containing significant surface cultural materials. Moreover, the report did not identify any traditional access ways or significant cultural practices in the vicinity. We believe that the conclusions of the cultural assessment relating to sugar cane cultivation also apply to the proposed mini-bypass project. Moreover, it is noted that work within the Hana Highway right-of-way will affect only areas of fill. Minor excavation is anticipated within the Baldwin Avenue right-of-way. We believe it is unlikely that items of historical or cultural significance will be affected by the project.

An additional traffic assessment was done in order to address traffic impacts on Baldwin Avenue. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

Mr. Lance Holter  
March 2, 2005  
Page 3

Work within the Hana Highway right-of-way are presented in plan view, as well as in cross section, which provides an accurate depiction of the proposal.

The Draft EA (p.2) mentions that the Department of Public Works and Environmental Management will be constructing improvements within the Baldwin Avenue right-of-way. Thus, County funds would be utilized for this purpose.

The Paia Mini-Bypass would be open only for the hours between 4:00 p.m. to 6:00 p.m., excluding Saturdays, Sundays and holidays. The proposed project would not affect morning peak hour traffic and thus this subject was not discussed in the Draft EA. The Maui Police Department was contacted for comments during the early consultation phase of the EA, as well as during the public comment phase of the EA.

As mentioned earlier, the shoulder lane along Hana Highway is needed in order to divert traffic desiring to turn right on Baldwin Avenue from the Hana Highway moving queue. If the shoulder lane is not provided, the single travel lane extending mauka from the Paia parking lot to the Post Office will be less effective in mitigating traffic congestion.

The comment on the Draft EA on community consensus pertained to the previous effort to implement the bypass. We would like to reiterate that the permanent Paia Bypass is a needed facility.

A feasibility study was not done prior to the EA. However, it should be emphasized that the Draft EA is a document which analyzed the proposed project in a comprehensive manner.

It is noted that afternoon peak hour traffic volume on Hana Highway is constrained by the factors noted in the traffic report. This includes diagonal parking, left-turning vehicles and pedestrians crossing the street within Paia Town. In order to better process the existing traffic volume, the mini-bypass proposes diversion of a portion of the Hana Highway traffic before reaching Paia Town. The operations of the existing Hana Highway-Baldwin Avenue traffic signal can be monitored and adjusted, as appropriate. However, the foregoing constraints within Paia Town limit the amount of cars which can be processed through the traffic signal.

A community alternate route task force can be considered during the planning and site selection for the permanent Paia Bypass.

In the case of big surf being a cause of traffic congestion, such times are not predictable, but may also occur during the afternoon peak hours. However, it can be stated that the proposed project can only provide more traffic capacity than presently

Mr. Lance Holter  
March 2, 2005  
Page 4

exists and can only provide an improvement to the existing traffic congestion on Hana Highway.

A press release was issued by the County of Maui on March 5, 2004 announcing the public informational meeting on the mini-bypass. On March 6, 2004, the Maui News printed an article on the County Council's decision to fund planning monies for the project. In the article, the date, time and place of the informational meeting was announced. The public informational meeting was held on March 17, 2004.

We appreciate the comment in your letter and handwritten comments to move the Baldwin Avenue terminus above the Paia Post Office. The Final EA includes revised plans to move the one (1) lane roadway above the post office. We believe that this improves the project proposal.

Our analysis assessed the use of the Kala/Sunnyside Road as one (1) possible route for a permanent bypass. As noted earlier, it could take approximately six (6) to eight (8) years before it can be finished in order to address safety, planning, permitting, engineering and land acquisition issues. The total estimated project cost of \$25.9 to \$37 million is based on the cost per mile of other new roadways which have recently been constructed on the island of Maui. It is not a cost estimate of the project itself, but we believe that it provides an order of magnitude estimate to improve Kala/Sunnyside Road.

The Draft EA has been reviewed during the public comment period. Written comments, as well as our responses to substantive comments, have been included in the Final EA.

Regarding public awareness of the exit impact onto Baldwin Avenue, it is noted that there was notification of the public informational meeting through the media, as well as information on providing comments to the Draft EA. Applicable provisions of Hawaii Administrative Rules pertaining to environmental review were followed.

At the public informational meeting, the fact that the Maui County Council would be discussing funding for the Paia Mini-Bypass on March 19 was mentioned.

A bikeway/greenway is not part of the subject proposal. The proposed project also does not preclude future implementation of a bikeway/greenway on nearby lands. Therefore, this has not been discussed in the EA.

Left-turn movements into the Paia Youth and Cultural Center is one (1) factor contributing to the congestion through Paia Town. If left-turn lanes are being

Mr. Lance Holter  
March 2, 2005  
Page 5

considered, it should be done comprehensively within Paia Town, as opposed to individually.

There is no linkage between the Paia Mini-Bypass and possible future projects such as A&B's commercial project and Spreckelsville Mauka. Any future proposal would be required to be considered under rules and regulations in existence at the time of application.

With regard to your comment on your handwritten note relating to loss of parking at Paia Bay, it is noted that informal parking occurs in the general vicinity of Paia Beach Park. This includes the area along the fence on the makai side of the Hana Highway right-of-way near the Paia Beach Park, the mauka side of Hana Highway, along Puna Road, and on the makai side of Hana Highway on the Wailuku side of Puna Road. With the proposed improvements, there would still be space for angle parking along the fence, although parallel parking which currently takes place on the mauka side of Hana Highway would be curtailed. However, informal parking space is still available on the makai side of Hana Highway extending closer to Baldwin Park.

Regarding your comment on your handwritten note on truck traffic, it is noted that heavy truck traffic would be prohibited on the mini-bypass. It is noted that the proposal would not change existing truck traffic patterns.

In conclusion, we note that the State Department of Transportation intends to start the planning and design process for the permanent Paia Bypass. One of the issues which needs to be addressed is obtaining a consensus on the permanent alignment. We agree that the permanent Paia Bypass should be expedited. However, the Paia Mini-Bypass can provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-holter

Paia Mini-Bypass

Name Lance Holter  
Address P.O. Box 790656  
Paia, HI 96779  
Phone Number 579-9442  
e-mail address holter@maui.net

The mini by-pass, if built, should come out above the post office and relieve the volume from Town.  
I have the following comments on the Paia Mini-Bypass Project.

The neighbors in the immediate area were not aware that this mini by pass was existing in their neighborhood. Further, they were not aware of this public meeting. I notified them and many were surprised. The draft EA was published March 8 in the OEQC Journal, without any public comments enclosed in the document. I believe this is in violation of the administrative rules. I've

read the EA and the alternative Kala Road Sunny side road has been given a negative report

Please turn in this sheet with your comments to a representative of the County of Maui, Department of Public Works and Environmental Management. Alternatively, it may be mailed to: back side

We need to have traffic volumes out of town up near the mill to avoid congestion.  
Mr. Gilbert Coloma-Agaran  
Director

Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

? Question How many cars that turn up Baldwin go all the way up Baldwin Ave out of the 30% that turn right? How many go shopping or post office then go back down Baldwin to Hamakua or to Hailu?

The alternative Paia/Sunnyside Road has been given a negative vote. I believe unfairly. Had there been adequate public meetings before hand you would see that the Kala-Sunnyside route is the route desired by the residents of the area. It simply gets the volume of traffic, including trucks, out of Paia and is the real solution. Without public testimony and input how can you implement any plan. Written comments are not enough. Your EA does not address traffic from Haiku thru Paia in the morning. It does not address the Baldwin traffic to Hana Hwy in the afternoon. It does not address the traffic during big wave sessions on the North Shore when people from all over the Island come to Paia to view the show. The EA does not address the impacts on Bike paths along Paia Bay and the Paia Youth Center, nor does it address the loss of parking at Paia Bay because of Hana Hwy road widening. Further it does not eliminate TRUCK TRAFFIC thru town.

March 26, 2004

Milton Arakawa  
Maui County Public Works  
200 South High Street  
Wailuku, HI 96793

'04 MAR 30 A7:38

COUNTY OF MAUI  
PUBLIC WORKS

Fax 270-7955

RE: Draft EA for PAIA MINI-BYPASS

Dear Milton Arakawa,

Thank you for the opportunity to comment on the Draft EA mentioned above. The history of the Paia Alternate Route needs to be addressed. I have spoken to many local long time residents who remember the issue. In the 1970's and early 80's Paia merchants were concerned that traffic routed around Paia would impact their businesses, this was during a period of recession with many businesses boarded up and closed down in Paia and before the North Shore boom of the late 80's. There were other issues, one being that the proposed Hana Belt Road would be too close to the Coast causing urban sprawl along Hana Hwy to Hookipa and that the Hana Belt Road design would impact the quality of life for residential homeowners in Paia itself. No one wanted a major Highway coming thru the area above the Ikeda building (The Old Princess Theater location) creating noise and safety problems for the residents and Paia Town.

In 1992 the Paia-Haiku Community Plan (adopted in 1995) located the Proposed Paia Alternate Roadway Mauka above Skill village and below the Paia School generally following the Kala Road- Old Makawao Highway route. The terminus would be near Maliko Gulch and back onto Hana Highway. This design and location was met with approval by the Haiku-Paia Community and thus adopted by the Council in 1995. The Plan satisfied the needs and concerns of the community and merchants. Most importantly The Community Plan asked, under TRANSPORTATION: IMPLEMENTING ACTIONS (page 27 No.1) to Plan, fund and construct an alternative roadway mauka of Pa'ia Town. This has never been done nor the recommendations of the Community Plan followed, this failure is one of the reasons why we have the traffic gridlock we see today.

176  RUSH

DEPT. OF PUBLIC WORKS

	RIFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DEP. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<i>Bob Dot</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_

By: \_\_\_\_\_ Date: \_\_\_\_\_

*3/31*  
*Fixed 3/20/04*

Page Two, Paia Mini By-Pass Draft EA

Sometime around the late 1990's, instead of seeking the funding for planning and purchasing the Right Of Way for the Paia Alternate Route, the State Department of Transportation arbitrarily chose to remove the parking on Hana Highway thru Paia Town. This was met with a lawsuit and a injunction. Further, it continued to show the dysfunctional communication by the State DOT with the Community, as well as, the DOT's inability to follow the recommendations of the Paia-Haiku Community Plan.

It has recently been suggested by some members of the County Council, The State DOT and A&B that the Paia Community lost monies for the Alternate Route because the community could not agree on a route or that the Merchants did not support the idea of Paia's Alternate By-Pass. This is erroneous as the 1992-1995 Community Plan adopted the Communities' views on the subject. Further, letters from the Community, the Paia Merchants, Paia Main street, Spreckelsville Community Association and the Paia Strategic Plan itself continue to ask that The Alternate Route be built. Finally, never, except around 1974, have there been monies to fund the Planning, purchase of Rights of Way and Construction of the Alternative By-Pass. Only until now, year 2004, because of efforts by State Senator Tsutsui, Representative Nakasone, Congressman Ed Case and Governor Lingle's Maui Liaison Advisory Committee do we have a proposal of funding and planning for the Paia Alternate Route. Yet we see no relief until the year 2012 even if everything runs smoothly and the landowner A&B understands and respects the economic needs of society and the community. It has been A&B's unwillingness to plan the Paia Alternative Route around HC&S sugar operations which has led to the intolerable traffic conditions we see today, as well as, the build out in Haiku which A&B has benefited from (see enclosed comments) with their numerous subdivisions and projects built on their former landholdings.

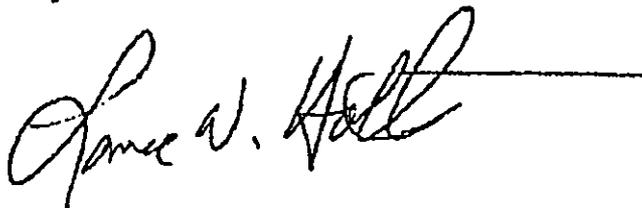
Please find enclosed my comments and related information to the Paia Mini-ByPass Draft EA.

Sincerely yours,

Lance W. Holter  
P.O. Box 790656

Paia, Maui, Hi 96779

Cc; Mayor Alan Arakawa, Michael Foley



Largest subdivisions beyond Paia and Baldwin avenue all belong to A&B.

Kuau Bayview---96 lots

Haiku Hills-----36 lots X2 houses= 72

Haiku Makai-----27 lots X2 =54

Giggle Hill-----46 lots X2 =92

Blue Tile Roof----7 lots plus condominiumization= 14 units

Total housing units---328

2.3 cars per house= 655

Average is 4 vehicle trips/ day. ....2168 to 2620 car trips per day.

Volume of traffic through Baldwin Ave/ Hana Hwy is about 650 cars/ hour eastbound at rush hour.

These subdivisions alone represent almost 25% of that traffic impact.

Mama's Fish House parking lot on A&B property approximately 50 spaces.

#### PAIA MINI BY-PASS EA

#### COMMENTS AND INADEQUACIES

- 1) Does not follow 1995 community plan
- 2) Does not address truck traffic thru Hana Hwy
- 3) Incomplete traffic analysis. Traffic recommendations appear to be based upon "traffic letter report" commissioned by A&B in April of 2002 for Paia Post Office A&B Commercial area.
- 4) Interview Informants section pertained to Paia Post Office & Commercial, not mini-bypass
- 5) Doesn't address Baldwin Ave. traffic onto Hana Hwy, or Hana Hwy Westbound traffic and contains no data for traffic patterns going up and down Baldwin to Hana Hwy and to Skill Village or coming from Haiku.
- 6) Incomplete design depiction of State roadway widening
- 7) Failure to disclose how County funds would be used
- 8) Makes no mention of opening road for hours other than 4-6pm, M-F
- 9) No comment from MPD, who are listed as enforcement for not driving on Hana Hwy shoulder 22hrs/day
- 10) 3rd lane of Hana hwy is not warranted to address regional traffic needs
- 11) EA stated that community consensus couldn't be reached on bypass but Paia has insisted in full bypass since 1992 Paia-Community Plan update.

Page two; EA Comments and Inadequacies

- 12) No feasibility study was ever done prior to EA.
- 13) Traffic congestion is caused by traffic volume, not by "area of Paia Town that is causing traffic to congest"
- 14) No mention of possible effects or costs of installing modern traffic signal
- 15) Need community alternate route task force.
- 16) No traffic data during high surf periods.
- 17) No public announcements of meeting in news papers prior to Mini Bypass Meeting at Paia Community Center. All announcements were by citizens using signs on Highway, personal handouts and letters to the Editor, none by Public Works.
- 18) No discussion of Re-aligning route around PO as better alignment and third alternative or that present design comes out onto Post Office crosswalk.
- 19) The EA made the Kala Road route, the obvious and quickest solution, unattainable by their comments and by A&B's unwillingness to co-operate with community. Other estimates of costs are considerably less than 6-7 million dollars per mile called for in EA. An independent study (not A&B) should be taken on costs for upgrading Kala Road.
- 20) No public comments in Draft EA.
- 21) No public awareness in neighborhood of exit impact on to Baldwin Ave.. A survey of the immediate neighborhood found residents there unaware of the impending Mini BY-Pass.
- 22) No announcement at Mini-Dy Pass meeting in Paia that County Council would be voting for funds to build Mini By-pass Friday.
- 23) EA does not address impacts on Bikeway and Greenway plan nor show bikeway in drawings and specs.
- 24) A left turn lane is necessary for traffic to turn into Paia Park and Youth Center
- 25) There is a failure to show or admit the future linkage between A&B's commercial project on Baldwin Avenue, A&B's Spreckelsville Mauka project and the proposed mini- By-Pass.

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. Jay Jeter  
2006 West Lelehuna Place  
Haiku, Maui, Hawaii 96708

Dear Mr. Jeter:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your facsimile transmittal of March 31, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

The Draft EA discusses the use of Kala Road and Sunnyside Road as an alternative action to the mini-bypass. Kala Road is an old government road which extends from Hana Highway to Sunnyside Road which then extends to Baldwin Avenue just mauka of the Paia Mill. Sunnyside Road is a privately owned road which must be purchased if it is desired for public use. There are also a number of other issues which need to be resolved before use of Kala/Sunnyside Road by the public is warranted. These include safety and liability issues relating to the current agricultural use of Sunnyside Road. There are also planning and route selection issues which need to be discussed for the Haiku leg of the bypass. An engineering analysis also needs to be done which discusses drainage, desired cross section of the road, and intersection design. Our analysis showed that it could take approximately six (6) to eight (8) years before the necessary planning, public hearings, environmental review, permits, design, and funding can be obtained in order to address the foregoing issues. Based on other roadways recently constructed on the island of Maui, an estimated roadway cost for the Kala/Sunnyside Road segment (not including a Haiku leg) could range from \$25.9 to \$37 million.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately

Mr. Jay Jeter  
March 2, 2005  
Page 2

\$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after the start of construction.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-jeter

FROM : CustomView : Jay Jeter



**CustomView**  
 WINDOW BLINDS SHUTTERS SHADES

CUSTOM • BLINDS • SHUTTERS • SHADES  
 ISLAND WIDE MOBILE SERVICE  
 (808) 283-0000 FAX (808) 575-7555  
 Email: customviewmaui@hawaii.rr.com

2006 W. Lelehuana Pl. Haiku, HI 96708

04 MAR 31 A 7:26  
 COUNTY OF MAUI  
 PUBLIC WORKS

Aloha from Haiku,

Please support our community in its  
 desire to have a Paia Bypass Now!

The Kala - Sunnyside road is an excellent  
immediate and intermediate solution.

Government has had enough time, 38 years.

RUSH 1500

DEPT. OF PUBLIC WORKS	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input type="checkbox"/>					
DIR. DIR	<input type="checkbox"/>					
PERS.	<input type="checkbox"/>					
STAFF CE	<input type="checkbox"/>					
LUCAS	<input type="checkbox"/>					
WATER/ECOL.	<input type="checkbox"/>					
SOLID W.	<input type="checkbox"/>					
ENGR.	<input type="checkbox"/>					
HWYS.	<input type="checkbox"/>					
SECTY.	<input type="checkbox"/>					
A+B	<input checked="" type="checkbox"/>					

Mahalo,  
 JAY JETER

Return to \_\_\_\_\_ Due \_\_\_\_\_  
 Ref. No. \_\_\_\_\_  
 By: \_\_\_\_\_ Date: \_\_\_\_\_

*Jeter*  
 3/21/04

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration  
TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division  
CARY YAMASHITA, P.E.  
Engineering Division  
BRIAN HASHIRO, P.E.  
Highways Division  
Solid Waste Division

March 2, 2005

Mr. Henry Kahula, Jr.  
176 Maoli Place  
Paia, Maui, Hawaii 96779

Dear Mr. Kahula:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments from the March 17, 2004 Paia informational meeting on the Paia Mini-Bypass. We would like to provide the following response:

We appreciate your suggestion to turn off the traffic signal or place a policeman at the Baldwin Avenue/Hana Highway intersection to direct traffic. However, the volume of traffic going through Paia Town has increased steadily over the years. Our traffic analysis shows that constraints to processing more traffic through Paia Town occurs before the signal itself. These constraints are diagonal parking, left-turning vehicles and pedestrians crossing the street. Thus, turning off the signal or having a policeman direct traffic at the intersection will not help in reducing the afternoon traffic congestion on Hana Highway.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, completion of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While we believe that a permanent Paia Bypass should be expedited, it should be emphasized that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Mr. Henry Kahula, Jr.  
March 2, 2005  
Page 2

Thank you for your input to the project.

Sincerely,

  
MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\hkahula\_paia mini bypass

## Paia Mini-Bypass

Name Henry Kahala Jr  
Address 176 MAOLI PL.  
PAIA, HI 96779  
Phone Number 808-579-9000  
e-mail address pfsalijomai@aol.com

I have the following comments on the Paia Mini-Bypass Project.

I have lived in Paia since 1972. We never had the traffic problems we have now until the traffic light got installed. Since then we've continued to have backed up traffic in Paia, including on weekends. Try not using the traffic lights for one or two weeks and study the traffic flow + see what the results are. After 3 p.m. the Baldwin Ave traffic has a long red light (over)

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

and Hana Highway traffic has a long green light. My experience has been that due to people going in & out of stalls on Hana Highway causes gaps in Hana Highway traffic still leaving traffic backed up on Baldwin Avenue to the Pan Mart.

An immediate solution would be to first of all check the traffic moving through Pan each day and try it without the traffic light and see what happens. Or put a police there to direct traffic at certain times of the day - An immediate solution is needed. Pan shouldn't have a traffic problem. It just looks that way.

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration  
TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division  
CARY YAMASHITA, P.E.  
Engineering Division  
BRIAN HASHIRO, P.E.  
Highways Division  
Solid Waste Division

March 2, 2005

Ms. G. Kealoha  
38 South Laelua Place  
Paia, Maui, Hawaii 96779

Dear Ms. Kealoha:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments received on March 23, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

We acknowledge your suggestion to eliminate parking in Paia during peak traffic. While this could help in providing additional traffic capacity through Paia Town during the afternoon rush hour, it has been proposed in the past and was met with significant community opposition.

We appreciate your support of the Paia Mini-Bypass project. We note that the Environmental Assessment and Special Management Area Use Permit are required by law prior to starting construction and cannot be deferred or eliminated. However, assuming these required permits can be obtained in a timely manner, the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Ms. G. Kealoha  
March 2, 2005  
Page 2

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-kealoha



ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Ms. Rebecca Kikumoto  
100 Hana Highway  
Paia, Maui, Hawaii 96779

Dear Ms. Kikumoto:

Thank you for your comments from the March 17, 2004 informational meeting relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

In response to your concern that there may be additional congestion on Baldwin Avenue as a result of the project, we have done an additional traffic analysis in order to address these issues. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

Moreover, in response to concerns that the Baldwin Avenue terminus of the mini-bypass makai of the Paia Post Office may conflict with traffic from the Paia Post Office driveway, we will be revising our plans to move the Baldwin Avenue terminus above or mauka of the Paia Post Office. The Final Environmental Assessment will reflect the change.

We would like to emphasize that the permanent Paia Bypass is a needed facility. Planning should be expedited in order to facilitate a solution. However, a permanent bypass would require the resolution of a number of outstanding issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one (1) possible route for a permanent bypass, showed that it could

Ms. Rebecca Kikumoto  
March 2, 2005  
Page 2

take approximately six (6) to eight (8) years before it can be completed in order to comply with necessary planning, public hearings, permits, and design requirements, as well as to obtain funding.

Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-kikumoto

Paia Mini-Bypass

Name REBECCA KIKUMOTO  
Address 100 HANA HWY  
Phone Number 579-9227  
e-mail address \_\_\_\_\_

I have the following comments on the Paia Mini-Bypass Project.

THIS IS AN INADEQUATE SOLUTION  
TO A MAJOR PROBLEM. MORE CONGESTION  
ON BALDWIN AVE IS NOT A SOLUTION.  
A MINI-BYPASS WILL NOT HELP  
WE NEED THE MAJOR BYPASS NOW.  
PARKING AT DIAGONAL PARKING IS NOT  
AN OPTION.

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration  
TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division  
CARY YAMASHITA, P.E.  
Engineering Division  
BRIAN HASHIRO, P.E.  
Highways Division  
Solid Waste Division

March 2, 2005

Ms. Deborah Kremins  
P. O. Box 790303  
Paia, Maui, Hawaii 96779

Dear Ms. Kremins:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments from the March 17, 2004 Paia informational meeting on the Paia Mini-Bypass. We would like to provide the following response:

We agree that the Paia Mini-Bypass project should not delay the discussion and implementation of a permanent Paia Bypass. However, a permanent bypass would require the resolution of a number of procedural, as well as substantive issues, before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one (1) possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be finished in order to address safety, planning, permitting, engineering and land acquisition issues. Thus, the mini-bypass is seen as an interim solution to traffic issues in the region.

We appreciate your suggestion to conduct additional traffic analysis along Baldwin Avenue. Additional analyses was done and the traffic assessment states that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

Ms. Deborah Kremins  
March 2, 2005  
Page 2

We acknowledge your suggestion to eliminate diagonal parking in Paia from the County parking lot to Baldwin Avenue. While this could help in providing additional traffic capacity through Paia Town, it has been proposed in the past and was met with significant community opposition.

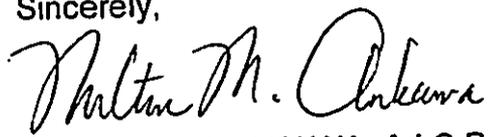
The hours of operation for the mini-bypass are currently being proposed for 4:00 p.m. to 6:00 p.m., excluding Saturdays, Sundays and holidays. After the project is implemented, the hours of operation can be adjusted accordingly depending on need and personnel availability.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While we believe that a permanent Paia Bypass should be expedited, it should be emphasized that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-kremins

## PAIA MINI-BYPASS

NAME Deborah Kremms  
ADDRESS PO Box 790303  
PAIA HI 96779  
PHONE ~~573~~ 573-3704 V.M.  
EMAIL \_\_\_\_\_

COMMENTS: I have several concerns regarding this "mini-bypass" and a few suggestions that I would appreciate you considering:

- ① That this temporary solution in no way delay the funding, planning & building of a real solution.
- ② That more study be done regarding the flow of traffic along Baldwin Ave between Hana Hwy & the Post Office. It is my sense that people who do turn right on Baldwin are doing a few errands such as going to the post office & then turning around & getting right back into that traffic. My concern is that the 'bypass' with a left turn lane will only increase the traffic congestion. My suggestion: to not negatively impact the merchants in the area, to increase safety, and to significantly reduce the congestion - I recommend removing the diagonal parking between the public parking lot (at the start of town) & the corner of Baldwin Ave & increase public parking ~~so that~~ & walking right of way so that we may WALK to get our mail/groceries, etc. MAKE SURE THE PUBLIC PARKING IS BEFORE THE LIGHT @ BALDWIN.
- ③ HOURS OF OPERATION & TRAFFIC IS BAD 2-7pm M-F PWS ALL DAY SAT + SUNDAY.

Mr. GILBERT COLONA-AGARAN, DIRECTOR  
PUBLIC WORKS  
200 S. HIGH ST  
WAILUKU, HI 96793

Thank you for  
your time & consideration.

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. George Lee  
P. O. Box 791366  
Paia, Maui, Hawaii 96779

Dear Mr. Lee:

Thank you for your comments on the Paia Mini-Bypass which we received on May 12, 2004. We would like to provide the following response:

We note that there have been a number of concerns raised that the Baldwin Avenue terminus of the mini-bypass makai of the Paia Post Office may conflict with traffic from the Paia Post Office driveway. You have also raised the point that the location of the Baldwin Avenue terminus will conflict with traffic from Melia Place. Thus, we will be revising our plans to move the Baldwin Avenue terminus above or mauka of the Paia Post Office. The Final Environmental Assessment will reflect the change.

It is noted that an additional traffic assessment was done in order to address possible traffic impacts on Baldwin Avenue which may result from the revised Paia Mini-Bypass project. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

Mr. George Lee  
March 2, 2005  
Page 2

Thank you for your input.

Sincerely,

  
MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-lee

Paia Mini-Bypass

04 MAY 12 AM 11:26

COUNTY OF MAUI  
PUBLIC WORKS

Address

GEORGE LEE  
21 MELIA PLACE  
P.O. Box 79 136 Paia, HI

Phone Number

579-8383

e-mail address

RUSH

DEPT. OF  
PUBLIC  
WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR						
DEPT. DIR.						
PLNG.						
STAFF CE						
LEGAL						
VEH. RECL.						
SOLID W.						
ENGR.						
H.A.Y.S.						
SAFETY						
ADD'L						

5/12

Return to \_\_\_\_\_ Date: 5/12/14  
Ref. No. \_\_\_\_\_  
By: \_\_\_\_\_ Date: \_\_\_\_\_

I have the following comments on the Paia Mini-Bypass Project.

THE PROPOSED EXIT ON TO BALDWIN AVENUE  
WILL CREATE A DANGEROUS TRAFFIC SITUATION AND  
BOTTLENECK TRAFFIC. POST OFFICE PATRONS WOULD  
BE BLOCKED FROM EXITING OR ENTERING THE POST  
OFFICE PARKING LOT SAFELY. RESIDENTS OF MELIA PLACE  
WILL NOT BE ABLE TO EXIT MELIA NORTH ON BALDWIN  
SAFELY. ENTERING MELIA PLACE WOULD ALSO BE  
DANGEROUS OR IMPOSSIBLE BETWEEN 4-6 PM WHEN  
MANY PEOPLE ARE GOING HOME AFTER WORK.

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

~~(Signature)~~

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Ms. Sheila Magers  
P. O. Box 1653  
Kahului, Maui, Hawaii 96733

Dear Ms. Magers:

Thank you for your e-mail transmittal of April 6, 2004 relating to the Draft Environmental Assessment for the Paia Mini-Bypass. We would like to provide the following response:

We agree that a permanent Paia Bypass is a needed facility. However, a permanent bypass would require the resolution of a number of procedural, as well as substantive issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one (1) possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be finished in order to address safety, planning, permitting, engineering and land acquisition issues. Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. Assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

You might recall that a past proposal had been to remove diagonal parking on Hana Highway within Paia Town in order to create additional travel lanes. However, this was met with significant community opposition.

Ms. Sheila Magers  
March 2, 2005  
Page 2

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-magers

Milton Arakawa - Paia traffic

**From:** "Sheila Magers" <smagers@mauiair.com>  
**To:** <Milton.Arakawa@co.maui.hi.us>  
**Date:** 4/6/04 3:32PM  
**Subject:** Paia traffic

Aloha Milton,

We need an alternate route around Paia, not just a short cut used only certain times of the day. Traffic has been getting worse for 10 + years, and now it is unbearable at all times of the day. A quick fix would be to take out parking on Hana Hwy. and make two lanes through the signal at Baldwin, but you will still need another road to really keep the traffic moving. It would be nice if you could keep Hana Hyw. the "scenic" route and have another road for the locals to get around.

Thanks for your time.

Sheila Magers  
Spreckelsville homeowner

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. Raun Marshall  
120 Hana Highway  
Paia, Maui, Hawaii 96779

Dear Mr. Marshall:

Thank you for your comments from the March 17, 2004 informational meeting relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

We would like to emphasize that the permanent Paia Bypass is a needed facility. Planning should be expedited in order to facilitate a solution. However, a permanent bypass would require the resolution of a number of outstanding issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one (1) possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be completed in order to comply with necessary planning, public hearings, permits, and design requirements, as well as obtain funding.

Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.



Paia Mini-Bypass

Name RAUN MARSHALL  
Address 120 HANA HWY-9140  
PAIA, HI 96779  
Phone Number 579 9041  
e-mail address MARSHALL DESIGN@VERIZON.NET

I have the following comments on the Paia Mini-Bypass Project.

THIS PROPOSED SOLUTION WILL NOT RELIEVE THE  
PROBLEM. IT IS MERELY A BAND-AID AND A WASTE  
OF MONEY. IT DOES NOT ADDRESS THE TRAFFIC IN  
THE FUTURE OR THE PEOPLE LIVING IN HAIKU.

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. Stanley Marshall  
marshalldesign@verizon.net

Dear Mr. Marshall:

Thank you for your comments from the March 17, 2004 informational meeting relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

We would like to emphasize that the permanent Paia Bypass is a needed facility. Planning should be expedited in order to facilitate a solution. While a general alignment for a permanent bypass highway has been laid out in the Paia-Haiku Community Plan, a permanent bypass would require the resolution of a number of outstanding issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one (1) possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be completed in order to comply with necessary planning, public hearings, permits, and design requirements, as well as obtain funding.

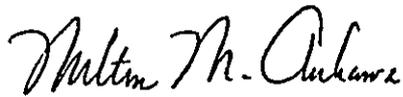
Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Mr. Stanley Marshall  
March 3, 2005  
Page 2

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-marshall s

**From:** "STANLEY MARSHALL" <marshalldesign@verizon.net>  
**To:** <letters@mauinews.com>  
**Date:** 3/31/04 4:30PM  
**Subject:** Fw: Mini by-pass EA

----- Original Message -----

**From:** STANLEY MARSHALL  
**To:** Lance Holter  
**Sent:** Tuesday, March 30, 2004 6:39 PM  
**Subject:** Re: Mini by-pass EA

Please honor the hours of discussion, meetings etc. that went into the creation of the 1992 Paia-Haiku Community Plan. Twelve years have gone by since this was devised by the residents of these two towns. This is the plan that made sense then and this is still the plan everyone wants now. Many residents devoted their time and energy to making sure this was the best solution to everyone's concerns and problems. This is the plan that the people who live here want and need. The people who don't have any agenda other than making life better for all involved.

Raun Marshall  
Paia

----- Original Message -----

**From:** Lance Holter  
**To:** holter@maui.net  
**Sent:** Sunday, March 28, 2004 1:35 PM  
**Subject:** Mini by-pass EA

The Paia Town Alternate route will take a concerted effort from all citizens to task force the implementation of the Transportation Section of the 1992 Paia-Haiku Community Plan. Silence from the community will not fund, plan or build the necessary solutions. Please use this opportunity to send comments on the Paia Mini-bypass and your feelings, needs and experiences regarding the full implementation of the Paia Town Alternate Route as described in the 1992 Paia-Haiku Community Plan. Public works fax 270-7955. Gill Coloma-Agaran, Director. Mayor Alan Arakawa's fax 270-7870. Highways Division fax 270-7466. All members of the Council to County Council Services fax 270-7686. State DOT fax 873-3544. and email Milton.Arakawa@co.maui.hi.us

**CC:** <Milton.Arakawa@co.maui.hi.us>

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. Bob Martin  
P. O. Box 792139  
Paia, Maui, Hawaii 96779

Dear Mr. Martin:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments from the March 17, 2004 informational meeting relating to the Draft Environmental Assessment of the Paia Mini-Bypass. We would like to provide the following response:

In order to address concerns on possible traffic impacts on Baldwin Avenue, an additional traffic assessment was done. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

Thank you for your input to the project.

Sincerely,

MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-bob martin

## Paia Mini-Bypass

Name Bob Martin  
Address 141 Baldwin Ave (PO Box 792139)  
Phone Number 579-9988  
e-mail address —

I have the following comments on the Paia Mini-Bypass Project.

Paia Mini-Bypass may no longer be an interim solution to traffic  
issues in the region. It will open a 2nd way for cars to  
get into lower Paia allowing more cars to enter per unit of  
time. This might plug traffic around Bank of Hi, Mann's,  
and the public parking lot. Extra cars milling around  
there will seriously back cars up Baldwin Ave. At best  
it may be a trade-off - it might make things far worse.

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. R. Bruce Martin  
P. O. Box 790926  
Paia, Maui, Hawaii 96779-0926

Dear Mr. Martin:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments dated March 22, 2004 relating to the Draft Environmental Assessment of the Paia Mini-Bypass. We would like to provide the following response:

We do not believe that the suggestion to turn off the traffic signal and post a traffic policeman at the intersection to direct traffic would significantly reduce the length of the moving queue on Hana Highway. The reason is that the constraints in processing more traffic through the traffic signal occurs before the signal itself. These constraints are diagonal parking, left-turning vehicles and pedestrians crossing the street.

We have done an additional traffic assessment of the effect of the Paia Mini-Bypass on Baldwin Avenue traffic. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-

Mr. R. Bruce Martin  
March 2, 2005  
Page 2

bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While a permanent Paia Bypass should be expedited, issues relating to route selection are best analyzed through an environmental review process which includes public review and will take a number of years to complete. We believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input to the project.

Sincerely,

  
MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-b.martin

Department of Public Works and Environmental Management

Milton M. Arakawa, A.I.C.P.  
Deputy Director

County of Maui  
200 South High Street  
Wailuku, Maui, Hawaii 96793

March 22, 2004

Dear Mr. Arakawa:

Comments on the Paia MiniBypass from the 3/17/04 public comment meeting:

- 1) I agree with several speakers that the most effective short term step would be to turn off the traffic light during peak congestion, and post a traffic director for the times involved. (S)he would result in greater queue throughput by minimizing/eliminating the periods no cars are going through the intersection. There are significant gaps in traffic now, both random and due to signal direction change pauses. This would probably also be safer for pedestrian crossings during that period, and if a police officer, minimize jaywalking and hopefully gather some currently much needed community goodwill.
- 2) I also fear with several speakers that once the proposed mini"bypass" is opened, Baldwin Avenue traffic will significantly worsen. Your study suggested 30% of the Hana bound traffic turned up Baldwin Ave. I believe a significant amount of that traffic intends to do some shopping or errands in Paia. Allowing faster access to Mana Foods from the other direction will affect traffic on Baldwin.

A very small amount of additional traffic access velocity could easily stack traffic past the post office, and effectively gridlock Baldwin Ave. Does your office have access to any traffic modeling programs?

Finally, as I'm sure we can all agree, the only effective solution will be a true bypass, allowing traffic to bypass the town entirely. Your presentation on the Sunnyside Road problems and timeline was very interesting. What route do you feel would be best, and what would its probable timeline/costs be?

Sincerely,

R. Bruce Martin  
P.O. Box 790926  
Paia, HI 96779-0926

Tel: (H) 579-9623  
(W) 579-9988

934  RUSH

DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
DEP. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW/RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ADD/DO	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_

By \_\_\_\_\_ Date: \_\_\_\_\_

*3/21*  
*forward 3/20*

04 MAR 24 P3:15  
COUNTY OF MAUI  
PUBLIC WORKS

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

The Martin Family  
23 Hoe Place  
Paia, Maui, Hawaii 96779

Dear Martin Family:

Thank you for your comments which we received on April 5, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

First, we would like to emphasize that the permanent Paia Bypass is a needed facility. Planning should be expedited in order to facilitate a solution. However, a permanent bypass would require the resolution of a number of outstanding issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one (1) possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be completed in order to comply with necessary planning, public hearings, permits, and design requirements, as well as obtain funding.

Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

The hours of operation for the mini-bypass are proposed to be 4:00 p.m. to 6:00 p.m., excluding Saturdays, Sundays and holidays. Extension and/or adjustment of the hours of operation can be considered depending on need and personnel availability.

The Martin Family  
March 2, 2005  
Page 2

Thank you for your proposal to remove parking on Hana Highway. You might recall, however, that a proposal to remove diagonal parking had been raised in the past and did not meet with community support.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours. Thank you for your input and support.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-martin

04 APR -5 A9:24

### Paia Mini-Bypass

RUSH 104

COUNTY OF MAUI  
PUBLIC WORKS

Name Martin family

Address 23 Hoe Pl.  
Paia, HI 96779

Phone Number 579-9268

e-mail address Buoyweather@yahoo.com

DEPT. OF PUBLIC WORKS	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR						
DEP. DIR						
PERS.						
STAFF CE						
LICA						
W/ENCL						
SO. W.						
ENGR.						
H.A.S.						
SECTY.						
ADD/DET						
Return to						
Due						
Ref. No.						
By:						
Date:						

I have the following comments on the Paia Mini-Bypass Project.

The road should be open seven  
days a week, 24 hours a day.  
If by pass (mini) is not built  
please remove parking on Hana Hwy and  
Baldwin Ave. and extend the existing  
parking lot on Hana Hwy (best solution).

Even though the minipass is built,  
we still need a ~~United Bypass~~ Bypass  
Major

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
 Director  
 Department of Public Works and Environmental Management  
 200 South High Street  
 Wailuku, Hawaii 96793

**PAIA MINI BYPASS TESTIMONY FROM MARCY MARTIN ON 4/3/04**

I have been driving through Paia since 1989 when I began my first full time job after graduating from college. Since that time traffic has gotten worse and worse and nothing has been done to ease the congestion.

I attended my first Paia Bypass meeting in February 2000. Like the majority of people who wait in the traffic, I am in favor of the large bypass. I submitted testimony in favor of the project. Unfortunately, a small, vocal, minority, special interest group was against the project and it got put on hold.

Some time after that, there was a proposal to remove street parking during rush hour. Like the majority of people who wait in the traffic, I am in favor of removing the parking. I submitted testimony in favor of the project. Unfortunately, a small, vocal, minority, special interest group was against the project and it got put on hold.

Some time after that, there was a proposal to change the parking to parallel parking during rush hour. Like the majority of people who wait in the traffic, I am in favor of reducing the parking during rush hour. I submitted testimony in favor of the project. Unfortunately, a small, vocal, minority, special interest group was against the project and it got put on hold.

Now, a mini bypass or right turn lane is being proposed. Like the majority of people who wait in the traffic, I am in favor of the mini bypass. I have been submitting testimony in favor of the project. Unfortunately, as usual, there is a small, vocal, minority, special interest group against the project.

It is commendable that the government is taking time to hear public input. However, at some point, discussion has to end and a solution implemented. During the four year period of indecision, traffic has doubled. At what point is government going to say, "There is a problem. We are going to fix it. The solution is not going to please everyone but it will please the majority."

I am for the mini bypass. However, in my opinion, the mini bypass is a compromise. Removing the street parking on Hana Highway and Baldwin Avenue would be a more effective and less expensive alternative. There are several, nice parking lots in Paia. I can't park in front of Costco but I still shop there. Removing the street parking in Paia will not hurt business (Costco, Kaahumanu Center, Maui Mall, Maui Marketplace, Front Street Lahaina being proof). However, because both of these solutions are interim, work on the Paia major bypass must begin immediately. If nothing is done, I have no doubt that the result will be a building moratorium in Paia, Spreckelsville and Haiku.

Testimony for February 5, 2004 Maui County Council Meeting  
 Subject: Paia mini bypass

I am in favor of the Paia mini bypass.

Name.	Address	Phone Number
Erin Fitzsimmons	160 Keonekai Rd. #13-106 Kihui, HI	264-1147
<del>Erin Fitzsimmons</del>	350-1135 MAKAWAO	572-5222
S. D. D.	12 HOLE LIO PL HIKU	575-7325
Pete Baker	POB 790476 Paia	475-9050
Edward T. Poa	2195A Liika Rd. Haiku, HI	573-5923
DOUG PAULSON	29 HOE PLACE	579-8719
Lisa Paulson	29 Hoe Place, Paia	579-8719
Ann Pitcaithley	43 S. Laelua Pl. Paia	579-9889
Bridget Reardon	25 HOE PLACE	579-8909
Dave DePietty	25 Hoe Pl.	579-8909
Zak McVale	20 HOE PL	579-8314
Rockelle R. McAbb	20 HOE PL	579-8314
Jennifer Maskrey	12 HOE PL	579-8797
Ken Maskrey	12 HOE PL	579-8797
FRANK FIRESTONE	6 HOE PL	579-9299
Nancy Luester	6 HOE PL	579-9299
Tami Goosby	3 HOE ST.	579-6240
Marey Martin	23 HOE PL	579-9268
DAN MARTIN	23 HOE PL. Paia	579-9268





ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. Ron Sturtz, President  
Maui Tomorrow  
P. O. Box 429  
Makawao, Maui, Hawaii 96768

Dear Mr. Sturtz:

Thank you for your letter dated April 23, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

First, we would like to note that a permanent Paia Bypass is a needed facility. The Draft EA discusses the use of Kala Road and Sunnyside Road as an alternative action to the mini-bypass. Kala Road is an old government road which extends from Hana Highway to Sunnyside Road which then extends to Baldwin Avenue just mauka of the Paia Mill. Sunnyside Road is a privately owned road which must be purchased if it is desired for public use. There are also a number of other issues which need to be resolved before use of Kala/Sunnyside Road by the public is warranted. These include safety and liability issues relating to the current agricultural use of Sunnyside Road. There are also planning and route selection issues which need to be discussed. An engineering analysis also needs to be done which discusses drainage, desired cross section of the road, and intersection design. Our analysis showed that it could take approximately six (6) to eight (8) years before the necessary planning, public hearings, environmental review, permits, design, and funding can be obtained in order to address the foregoing issues. Based on other roadways recently constructed on the island of Maui, an estimated roadway cost for the Kala/Sunnyside Road segment (not including a Haiku leg) could range from \$25.9 to \$37 million.

Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately

Mr. Ron Sturtz, President  
March 2, 2005  
Page 2

\$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

Regarding public input, it is noted that the early consultation period included a request for comments from the Paia Main Street Association. The 30-day public comment period started on March 7, 2004 and was extended until April 23, 2004. In addition, an evening public informational meeting was held on March 17, 2004 at the Paia Community Center. The project is in compliance with all applicable rules and regulations pertaining to public input. We note that a Special Management Area (SMA) minor permit was granted for a portion of the project involving the single-lane roadway extending mauka from the vicinity of the Paia Parking Lot to Baldwin Avenue. However, an SMA major permit application has been submitted and is being processed by the Department of Planning.

There have been a number of concerns raised that the Baldwin Avenue terminus of the mini-bypass makai of the Paia Post Office may conflict with traffic from the Paia Post Office driveway. Thus, we will be revising our plans to move the Baldwin Avenue terminus above or mauka of the Paia Post Office. The Final Environmental Assessment will reflect the change.

It is noted that an additional traffic assessment was done in order to address possible traffic impacts on Baldwin Avenue which may result from the revised Paia Mini-Bypass project. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

Regarding the proposed shoulder lane within the Hana Highway right-of-way, this is needed in order to provide additional lane length away from the source of congestion within Paia Town which takes a significant amount of traffic out the Hana Highway moving queue. The elimination of this portion of the project will not have a significant effect in mitigating traffic congestion.

Although the shoulder lane is being constructed closer to existing power poles, it is also noted that the speed limit is being reduced to 35 mph in this vicinity. We do not believe that there are significant safety issues resulting from the project relating to vehicles, bicycles and pedestrians. Moreover, it is noted that the County of Maui is

Mr. Ron Sturtz, President  
March 2, 2005  
Page 3

planning to implement a bicycle path parallel and makai of the existing Hana Highway right-of-way from Paia Town to Baldwin Beach Park.

With regard to your comment on parking in front of Paia Beach Park, it is noted that informal parking occurs in this general vicinity. This includes the area along the fence on the makai side of the Hana Highway right-of-way near the Paia Beach Park, the mauka side of Hana Highway, along Puna Road, and on the makai side of Hana Highway on the Wailuku side of Puna Road. With the proposed improvements, there would still be space for angle parking along the fence although parallel parking which currently takes place on the mauka side of Hana Highway would be curtailed. However, informal parking space is still available on the makai side of Hana Highway extending closer to Baldwin Park.

There will be signage along the Hana Highway right-of-way in order to inform motorists that use of the shoulder lane is limited to afternoon peak hours. Enforcement on the use of the shoulder lane and any aggressive driving or "lane jumping" by individuals would be enforced by the Police Department. It is noted that the Police Department was given the opportunity to provide comments during the early consultation, EA public comment, and SMA process. Comments were provided via two (2) memos and will be included in the Final EA.

The proposed project does not include any proposal to eliminate diagonal parking spaces within Paia Town. Any unknown future proposals will be subject to processes applicable to those proposals.

It is acknowledged that construction and restriping activities would involve some disruption to existing traffic conditions during the period of construction. After the project is done, however, traffic conditions should be improved during the afternoon hours.

It is noted that we would be open to discussing signage in terms of reducing the number of signs entering Paia Town.

We believe that the use of the Paia Mini-Bypass should have a beneficial effect on mitigating traffic impacts during peak afternoon hours and justifies the cost.

Drawings in the Draft EA of the Department of Transportation (DOT) portion have been reduced in size in order to be included within the EA document. The figures in the document have been reduced in proportion to their original scale. The figures do show some adjacent land uses for contextual purposes.

Mr. Ron Sturtz, President  
March 2, 2005  
Page 4

As far as proposing a one (1) lane Kala/Sunnyside alternative, it is noted that this needs to be considered in any planning and environmental analysis. The frequency of usage of the cane-haul roads need to be considered as part of an overall assessment of safety and liability, as well as possible effects on agriculture. Any discussion on possible alignments for a permanent Paia Bypass needs to be considered as part of a comprehensive environmental review.

We note that we have disclosed all relevant and applicable information to the public as part of the EA process. The alternatives have all been discussed thoroughly in the EA. While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-sturtz

# Maui Tomorrow

04 APR 26 08:01

Land Use Planning, Community Design and Responsible Growth for Maui County

COUNTY OF MAUI  
PUBLIC WORKS

RUSH 1285

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
--	------	--------	--------	----------	------	------

DIRECTOR						
DEP. DIR						
PERS.						
STAFF CE						
LUCA						
WW RECL.						
SOLID W.						
ENGR.						
HWYS.						
SEC TY.						
<i>DOT/APP</i>						

Writer's Direct Contact:  
 Ron Sturtz, President  
 874 Kumulani Drive  
 Kihei, Hawaii 96753  
 Tel: (808) 891-0425  
 Fax: (808) 891-0269  
 ronsturtz@aol.com

BY FAX 270 7955  
and U. S. Mail

April 23, 2004

Return to \_\_\_\_\_ Due \_\_\_\_\_

Milton Arakawa, Deputy Director. No. \_\_\_\_\_  
 Dept. of Public Works and Environmental Management  
 County of Maui  
 200 South High Street  
 Wailuku, HI 96793

Re: Comment on Draft Environmental Assessment for Paia Mini-Bypass

Dear Mr. Arakawa,

### INTRODUCTION

As you may know, Maui Tomorrow Foundation works with government decision makers and citizens to promote growth management strategies, to implement sustainable development policies for Maui, and to preserve irreplaceable open space and natural areas. We are grateful for the opportunity to offer our input to the Draft Environmental Assessment for the proposed Paia Mini-Bypass project. As our office is located in the immediate vicinity of the proposed project, we feel we may offer first-hand knowledge of the current conditions, as well as specific insight on appropriate solutions for the current traffic backups.

### OVERVIEW

Since the adoption of the Paia-Haiku Community Plan in 1995 and before, residents of the north shore have asked for an alternate route besides the Hana Highway. The Community Plan asked, under TRANSPORTATION: IMPLEMENTING ACTIONS (page

- Board of Directors
- Ron Sturtz, President
- Lucienne de Nais, Vice President
- Gregory Wolf, Treasurer
- Lance Hulten, Secretary
- Susan Bradford
- Kelly Takaya King
- Paul Lander
- Sean Lester
- Ed Lindsay
- Melie Lu'uwai
- Judith Michaels
- Richard Michaels
- Anthony Ranken
- Mark Sheehan

27, No.1) to "Plan, fund and construct an alternative roadway mauka of Pa'ia Town." Nearly ten years later, little progress has occurred. Four years ago, during discussions of their proposed Speckelsville Mauka subdivision, A&B Properties proposed a temporary alternate "mini-bypass", to be constructed on their land, and with their own funding. This sounded simple enough, and the community generally supported the idea, while reiterating that a true alternate route was the only real solution to rush hour traffic. The current proposal, a joint venture with the State, County, and A&B, has strayed off course from the simple temporary solution initially offered. However, increasing traffic flow each year has brought to us a sense of urgency for immediate relief. May we preface our official comments by stating simply, that anything worth doing is worth doing right. We urge you to carefully consider all comments as constructive input, rather than dismissing potential solutions as not viable because they could take longer to plan and implement.

#### COMPLIANCE WITH ENVIRONMENTAL REVIEW

We believe the environmental review process for issuing the Draft EA was insufficient. According to state law, the agency preparing a Draft EA or EIS must consult with community groups and individuals early in the process:

"§11-200-09 (a) (1) Seek, at the earliest practicable time, the advice and input of the county agency responsible for implementing the county's general plan for each county in which the proposed action is to occur, and consult with other agencies having jurisdiction or expertise as well as those citizen groups and individuals which the proposing agency reasonably believes to be affected;"

While 13 "consulted agencies" were solicited for comment, only one was a citizen group. The Paia Main Street Association did not comment because they still had not seen a presentation of the project. In fact, comments were received from only 5 of 13 agencies. Additionally, neither the leadership nor members of important interested parties, including the Sierra Club, Speckelsville Homeowners' Association, nor Maui Tomorrow Foundation, were contacted for their input prior to the preparation of the Environmental Assessment.

Additionally, the Maui Planning Department erroneously issued an SMA Minor permit in January 2004, which has still not been rescinded.

#### PROJECT DESIGN

It is our understanding that recent discussions with County, A&B, and the Paia Postmaster have resulted in a decision to change the connection point at Baldwin Avenue to above the Post Office. While this probably represents a better overall design choice, there are no supporting traffic studies to indicate impacts on this area of Paia traffic. This issue of absence of traffic studies was raised by a number of citizens at the

public hearing that was held. Proceeding without such studies is deemed unwise, and this substantial change to the Draft EA should require a revision, rather than a Final EA/ Finding of No Significant Impact (FONSI).

The most distressing portion of the Paia Mini-Bypass project design is the State D.O.T.'s proposed third travel lane on Hana Highway. Section VII of the Draft EA concedes that this will be an irreversible and irretrievable commitment of resources. We strongly urge reconsideration of this element of the project for these reasons:

- 1) Widening the roadway would bring traffic dangerously close to power poles on mauka side of highway.
- 2) Widening road would jeopardize bicycle and pedestrian safety on both mauka and makai sides.
- 3) Widening road would eliminate parking in front of Small Park, a well used and much needed parking area.
- 4) Vehicle traffic would be four feet closer to corridor for North Shore Greenway/ Bike Path.
- 5) Stacking lane would put traffic side-by-side rather than bumper-to-bumper, with negligible effect in time.
- 6) "Lane-jumping" would lead to safety concerns, including racing.
- 7) Plan says shoulder lane usage would be limited to 3 hours/ day, to be controlled by signage and police, yet the Maui Police Department made no comments to the Draft EA document.
- 8) Third lane concept originated from a single study in 2002, commissioned by A&B. However, this study recommended a third lane separated from the roadway by a barrier (which we also do not recommend).
- 9) Without a separation barrier, it is probable the shoulder lane would be used during all hours, day and night.
- 10) Third lane could lead D.O.T. to revive their plan to put third travel lane through Paia Town, thereby eliminating the diagonal parking spaces that are so depended upon by customers of the local merchants.
- 11) Construction and restriping activities would exacerbate existing traffic conditions.
- 12) Based upon review of the diagrams in the draft EA, it appears at least 8 new road signs would be necessary, cluttering the scenic open space views.
- 13) Any necessary vehicle storage lane should be on A&B portion, not on State Highway.
- 14) Expenditure of over a half million dollars in public funds is not prudent for a "temporary" solution.
- 15) Design representations of D.O.T. portion are insufficient in that they do not sufficiently place themselves in context to local adjacent activities and community uses, nor are they drawn to scale.

NEED FOR INCLUSION OF TRAFFIC IMPACT ANALYSIS REPORT

There is no mention that a feasibility study has ever been done for this proposed project. Neither has a Traffic Impact Analysis Report has been prepared. The justification for the proposed action is that an estimated 30% of cars coming eastbound on the Hana Highway turn right onto Baldwin Avenue. However, there is no data to indicate what percentage of those vehicles proceed above the Post Office, and what percentage eventually return down and proceed either east or west on the Hana Highway.

Furthermore, traffic conditions at the proposed connection point at Baldwin Avenue have not been studied. Volume of traffic and timing of the traffic signal create a traffic queue up Baldwin Avenue, at times beyond the connection point. Therefore, it is questionable whether reconnecting traffic at this point would significantly reduce Hana Highway backup, or if it would merely worsen Baldwin Avenue northbound traffic.

ALTERNATIVES USING KALA/ SUNNYSIDE ROAD

Section VI., "Alternatives to the Proposed Action", contains productive discussion of the usage of existing roadways for both a two-lane and one-lane road. However, some erroneous conclusions were drawn, seemingly based on conjecture rather than factual data. Use of Kala/ Sunnyside Roads for a one-lane road during peak rush hours appears to be a viable option, with minimal investment of time and money for adequate analysis and planning. Has the County asked A & B for a schedule of their frequency of usage of these cane roads? Is their use truly conflicting, or is it just occasional? The connection point is above existing traffic backups on Baldwin Avenue, and ideally could extend on existing roads east to Holomua Road or beyond. This would serve the purpose of a real bypass, rather than merely funneling traffic to another part of Paia Town.

RECOMMENDATIONS

€ Given numerous concerns, including the environmental review process itself, we believe it is most prudent to withdraw the current Draft EA and begin the process anew, with true community-based input. Our experience has taught us that long-range planning processes have cost the County considerable excess time and money because simple principles were not followed. We believe that involving the community from the inception is the key to success. It is not too late to follow this precept.

€ In the new community review meetings, offer revised alternatives to both the proposed "mini-bypass" (with sufficient traffic study data, and without the D.O.T. third lane), and the Kala/ Sunnyside one lane alternative. Both of these offer possibilities for immediate traffic relief, without losing sight of the true alternate bypass solution contained in the Paia-Haiku Community Plan, and without irretrievable, irreversible commitment of resources.

€ View all comments received as a collective effort to improve existing conditions, and

be willing to consider even the most unusual, outside-the-box ideas. We believe our planning efforts are enhanced when the multiple voices of our community are interwoven with the thinking of our landowners and government agencies.

€ Please list us as an interested party, and please respond to our official comments when you are able.

Thank you for taking the time to consider our views. We hope that they will be of assistance to you in furthering this planning process.

Very truly yours,

MAUI TOMORROW FOUNDATION, INC



Ron Sturtz  
President

cc: Rob Parsons,  
Executive Assistant for Environmental Concerns

5-26.001

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Ms. Cissy McDonald  
581 Pili Loko Street  
Paia, Maui, Hawaii 96779

Dear Ms. McDonald:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments from the March 17, 2004 Paia informational meeting, as well as comments received by our Department on March 24, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

We appreciate your comments on the use of Kala/Sunnyside Road. While we understand your opposition to the use of the road as a bypass route, it can be considered as one (1) alternative for a permanent Paia Bypass.

We acknowledge your suggestion that parking stalls in Paia Town along Hana Highway should be deleted. While this could help in providing additional traffic capacity through Paia Town during the afternoon rush hour, it has been proposed in the past and was met with significant community opposition.

We do not believe that the suggestion to turn off the stoplight to a yield and installation of a speed hump would significantly reduce the length of the moving queue on Hana Highway. The reason is that the constraints in processing more traffic through the traffic signal occurs before the signal itself. These constraints are diagonal parking, left-turning vehicles and pedestrians crossing the street.

Other solutions such as car pooling and bus service can help in reducing traffic congestion and can be considered as non-structural solutions which could enhance overall traffic management in the region.

Ms. Cissy McDonald  
March 2, 2005  
Page 2

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-mcdonald

Paia Mini-Bypass

04 MAR 24 P 3:09

COUNTY OF MAUI  
PUBLIC WORKS

Name Cissy McDonald  
Address 581 Pili Loko st  
Paia, HI.  
Phone Number 808-579-6052  
e-mail address N/A

926  RUSH

DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
DEP. DIR	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
<u>AD/DO</u>	<input checked="" type="checkbox"/>	<input type="checkbox"/>				

Return to \_\_\_\_\_ Due \_\_\_\_\_  
Ref. No. \_\_\_\_\_  
By: \_\_\_\_\_ Date: \_\_\_\_\_

3/21  
fined 3/20/04

I have the following comments on the Paia Mini-Bypass Project.

Aloha and thank you. I think the mini-by-pass is sufficient "relief" for now. It should be RIGHT TURN ONLY onto Baldwin Ave. (maybe additional public parking area near post-office so if people want/need to shop at mana, etc., they can walk there. As there's no parking at mana at that time of day anyway!) I'm all FOR TRYING the blinking yellow light at the intersection (it might work - but would need speed bumps to slow people down).

I am completely against a bigger, future by-pass on the Kala/Sunnyside Road. make people carpool

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

or rally  
Support  
for a  
viable  
Bus  
Service!

→ That's a better long-term solution for Maui County!

FROM : LEONARD T. YU, M.D.

FAX NO. : 8088777457

To: Fred Cajigal  
Maui office DOT  
Fax: 873-3544

RE: Kala-Sunnyside Rd. By-Pass Alternative; Paia, Maui  
Alpha. I want it known that not everyone wants this  
sugar cane road as a by-pass route. I attended the  
informational meeting on 3/17/04. I was disturbed by  
the attitudes of many people present. They seemed to  
think that they are entitled to use that road, as if  
the cane industry "owes it" to them. They are essen-  
tially demanding a \$30 million road that will be  
used almost exclusively by people who live in Haiku  
only. The smaller "mini-by-pass" from the public  
parking on Hana Hwy. to the Baldwin Ave. Post office is  
a fair compromise.

I feel pressure needs to be put on those few Paia  
businesses who refuse to give up a dozen parking  
spots on Hana Hwy., as it is for the greater good of  
the community. I heard another suggestion that  
might be worth trying: change the ill-timed stoplight  
at the Paia intersection to a blinking yellow "Caution"  
light and add speed bumps to discourage speeders.

Please support Maui's sugar cane operations.  
Help protect their rights to their roads so that their  
business will thrive and remain on Maui. The sugar  
cane industry is a vital part of Maui County's  
socio-economic, cultural and historical integrity.

The people of Paia and Haiku need to change  
their focus and rally support for carpool stations  
and a viable, affordable public bus system (that  
services the North shore communities). Those are  
the ONLY true long-term solutions to traffic woes.

Thank you very much for your time;  
Sincerely, Cissy McDonald  
Cissy McDonald  
Paia, Maui, HI.

FROM : LEONARD T. YU, M.D.

FAX NO. : 8088777457

141 7147105 - 1A 314  
Mar. 20 2004 01:45PM P1

To: Ronald Tsuzuki  
State office DOT  
Fax: 1-808-587-1787

RE: Kala-Sunnyside Rd. By-Pass Alternative; Paia, Maui  
Aloha. I want it known that not everyone wants this road. I am completely AGAINST the public use of this sugar cane road as a by-pass route. I attended the informational meeting on 3/17/04. I was disturbed by the attitudes of many people present. They seemed to think that they are entitled to use that road, as if the cane industry "owes it" to them. They are essentially demanding a \$30 million road that will be used almost exclusively by people who live in Haiku only. The smaller "mini-by-pass" from the public parking on Hana Hwy. to the Baldwin Ave. Post Office is a fair compromise.

I feel pressure needs to be put on those few Paia businesses who refuse to give up a dozen parking spots on Hana Hwy., as it is for the greater good of the community. I heard another suggestion that might be worth trying: change the ill-timed stoplight at the Paia intersection to a blinking yellow "Caution" light and add speed bumps to discourage speeders.

Please support Maui's sugar cane operations. Help protect their rights to their roads so that their business will thrive and remain on Maui. The sugar cane industry is a vital part of Maui County's socio-economic, cultural and historical integrity.

The people of Paia and Haiku need to change their focus and rally support for carpool stations and a viable, affordable public bus system (that services the North Shore communities). Those are the ONLY true long-term solutions to traffic woes.

Thank you very much for your time;  
Sincerely, Cissy McDonald  
Cissy McDonald  
Paia, Maui, HI.

MAUI COUNTY  
STATE OF HAWAII  
Mar 23 1 54 PM '04  
HIGHWAY  
PLANNING

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Ms. Diane McDonald  
581 Pili Loko Street  
Paia, Maui, Hawaii 96779

Dear Ms. McDonald:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments from the March 17, 2004 Paia informational meeting relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

Regarding your concern that the Paia Mini-Bypass changes the whole look of downtown Paia, we note that all of the work on the shoulder lane will be within the Hana Highway right-of-way. The proposed cross section would involve six (6) feet of additional pavement on the mauka side and four (4) feet on the makai side. The remaining shoulder area would continue to be grassed as it is now. We do not believe that the project adversely affects the scenic and open space qualities of the entrance to Paia.

We acknowledge your suggestion to remove diagonal parking in front of the existing stores on Hana Highway. While this could help in providing additional traffic capacity through Paia Town during the afternoon rush hour, it has been proposed in the past and was met with significant community opposition.

Kala/Sunnyside Road can be considered one (1) alternative for a permanent Paia Bypass. As you have noted, one (1) of the issues which should be considered in any future alignment for a permanent bypass is possible land use changes which could occur as a result.

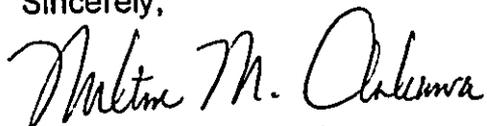
Car pooling and public transportation can be considered separately in addition to proposed roadway improvements to also aid in traffic mitigation. A bike path is being

Ms. Diane McDonald  
March 2, 2005  
Page 2

implemented incrementally from Kahului to Paia by the County. Portions of the bike path already exist near the airport to Spreckelsville. The next phase of the bike path is planned from Paia Beach Park to Baldwin Park on the makai side of the Hana Highway right-of-way.

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-dmcdonald

04 MAR 19 P3:05  
COUNTY OF MAUI  
PUBLIC WORKS  
Address

### Paia Mini-Bypass

Phone Number  
e-mail address

Diane McDonald  
581 Pili Loko St.  
Paia, HI. 96779  
808 579 6052

985  RUSH

DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DEP. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PEERS	<input type="checkbox"/>					
STAFF CE	<input type="checkbox"/>					
LICA	<input type="checkbox"/>					
WW RECL.	<input type="checkbox"/>					
SOLID W.	<input type="checkbox"/>					
ENGR.	<input type="checkbox"/>					
HWYS.	<input type="checkbox"/>					
SECTY	<input checked="" type="checkbox"/>					

Return to \_\_\_\_\_ Date: 3/19  
 Ref. No. \_\_\_\_\_  
 By: \_\_\_\_\_

I SIT IN PAIA TOWN TRAFFIC EVERY NITE AFTER WORKING A 10-12 hr. day. I AM OPPOSE TO THE BYPASS.  
I have the following comments on the Paia Mini-Bypass Project

- ① Mini-bypass changes the whole look of downtown Paia from cute Plantation town to 4 lane strip mall. IT takes away the essence of Paia town that is left.
- ② The simple, sensible; cost effective; least inconvenient, fastest SOLUTION seems to be to remove the parking spots presently located in front of the 1st - 4 or 5 stores Before Baldwin Ave. on Hana Hwy. Maybe loosing 10 parking spots. Protests from shop owners are purely SELFISH.
- ③ Saves county most of \$650,000 since it's mostly repainting vs. re-paving + new construction.

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

- ④ Kala road use prepares the way for development of the area on both sides of that Rd. in the future. I Believe it is Naieve or just plain dishonest to pretend that Paia won't be developed - if they develop Makana

4.) Why wouldn't they develop this rd? I know "A+B's" - Position Now - BUT we are aware of how things change time for implementing is 6-8 yrs. I'm sure of major development there

5.) Why NOT put half or more of the \$650,000. cost of the mini-bypass into - car pooling? Bike lanes? Public transportation? These things will actually take cars off the road. I find it interesting that no one even entertains this idea.

\$650,000. to divert 30% of traffic. Doesn't sound reasonable to me. Removing parking spots is interim solution. We need a real solution - mini-bypass + Kala rd. Bypass ave neither a long term solution.

I suggest solutions of many small positive adjustments is always more successful than a huge sweeping solution whose end result is UNKNOWN. + may be a mistake - expensive mistake. small solutions, inexpensive + fast could be

- ① ADJUST STOP LITE - caution like
- ② eliminate Diagonal parking - for greater good of community
- ③ Put \$ into Public transportation

The Paia mini bypass as presented is expensive & not a solution at all. It will aggravate traffic in this area. It will change the look of down town Paia from plantation town to STRIP MALL.

A SIMPLE, SPEEDY, COST-EFFECTIVE SOLUTION WOULD INCLUDE: ① adjusting the STOP LINE AT Baldwin Ave / Hana Hwy - Blinking orange. OR JUST adjust timing.

② Removing Diagonal parking spaces that impede traffic + are unsafe when people Back out - For the greater good of the community. The road is Public after all - not private property of those particular selfish store owners that I won't patronize any more.

③ The savings earmarked for Both Bypasses (or part of it) diverted to removing some cars, + saving oil consumption by improving safe Bike Lanes; car pooling user friendly improvements + Public transportation -

Kala Rd. only paves the way for future Development - Denying that insults the Public's intelligence.

Sincerely,  
Diane McDonald  
Paia, HI

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. and Mrs. Norman Mezin  
[JoanneMezin@aol.com](mailto:JoanneMezin@aol.com)

Dear Mr. and Mrs. Mezin:

We have received your e-mail transmittal of April 4, 2004 relating to the Paia Mini-Bypass. We would like to provide the following additional information:

First, we would like to emphasize that the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

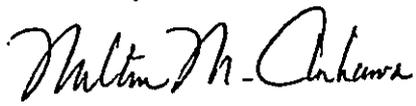
The permanent Paia Bypass is a needed facility. Planning should be expedited in order to facilitate a solution. However, a permanent bypass would require the resolution of a number of outstanding issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one (1) possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be completed in order to comply with necessary planning, public hearings, permits, and design requirements, as well as obtain funding.

Thus, while a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Mr. and Mrs. Norman Mezin  
March 3, 2005  
Page 2

Thank you for your input and support.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\mezin\_paia mini bypass

**From:** <JoanneMezin@aol.com>  
**To:** <Milton.Arakawa@co.maui.hi.us>  
**Date:** 4/4/04 10:23AM  
**Subject:** Bi Pass Road in Paia

Please help us we live in Halku right after Maliko Gulch and it takes us so long to get home Thu Paia. The long line of cars from Sprecks/Baldwin results in a lot of wasted time and fuel. Please help us get home to our Families sooner after a long day of work. Thank you for listening.

Aloha,

Norman & Joanne Mezin

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. Sol Morey  
P. O. Box 791872  
Paia, Maui, Hawaii 96779

Dear Mr. Morey:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments from the March 17, 2004 Paia informational meeting on the Paia Mini-Bypass. We would like to provide the following response:

You have suggested improvements to the pavement in front of the Paia Youth Center. However, Puna Road which provides access to the Paia Youth Center is a private road. Any future improvements in this area could perhaps be considered in the context of a master planned development for lands makai of Hana Highway.

Any extension of the mini-bypass to Hamakuapoko Road or other alternative should likely be considered in the context of a permanent Paia Bypass. Kala/Sunnyside Road could be considered as one (1) possible route for a permanent bypass. However, the Kahului leg of the bypass should be planned in concert with the Haiku leg of the bypass which would connect back to Hana Highway.

The hours of operation for the mini-bypass are currently being proposed for 4:00 p.m. to 6:00 p.m., excluding Saturdays, Sundays and holidays. After the project is implemented, the hours of operation can be adjusted accordingly depending on need and personnel availability.

It should be clarified that bicyclists would be allowed to utilize the Hana Highway shoulder lane. It is 14 feet wide which allows the shoulder lane to be shared by motorists and bicyclists.

Mr. Sol Morey  
March 2, 2005  
Page 2

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While we believe that a permanent Paia Bypass should be expedited, it should be emphasized that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-morey

## Paia Mini-Bypass

Name Sol Morey  
Address 10~~00~~ 791872 Paia 96779  
Phone Number 579-9667  
e-mail address Moreysol@hotmail.com

I have the following comments on the Paia Mini-Bypass Project.

- \* Improvement needs to be made to pavement in front of Paia Youth Center
- \* ~~request~~ Please extend mini-bypass to H. Poko Rd.
- \* Earlier times of having the Paia-Bypass open should be considered. (2pm - 6pm)
- \* Bike traffic safety needs consideration because people will try using the proposed time limited mini-bypass.
- \* Please consider routing traffic up behind the old mill to connect with Kala Rd. & continue through to H. Poko Road

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Ms. Debra Schonewill, Chairperson  
Paia Main Street Association  
P. O. Box 995  
Paia, Maui, Hawaii 96779

Dear Ms. Schonewill:

Thank you for your letters dated April 14, 2004 and July 20, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

First, we would like to note that a permanent Paia Bypass is a needed facility. The Draft EA discusses the use of Kala Road and Sunnyside Road as an alternative action to the mini-bypass. Kala Road is an old government road which extends from Hana Highway to Sunnyside Road which then extends to Baldwin Avenue just mauka of the Paia Mill. Sunnyside Road is a privately owned road which must be purchased if it is desired for public use. There are also a number of other issues which need to be resolved before use of Kala/Sunnyside Road by the public is warranted. These include safety and liability issues relating to the current agricultural use of Sunnyside Road. There are also planning and route selection issues which need to be discussed for the Haiku leg of the bypass. An engineering analysis also needs to be done which discusses drainage, desired cross section of the road, and intersection design. Our analysis showed that it could take approximately six (6) to eight (8) years before the necessary planning, public hearings, environmental review, permits, design, and funding can be obtained in order to address the foregoing issues. Based on other roadways recently constructed on the island of Maui, an estimated roadway cost for the Kala/Sunnyside Road segment (not including a Haiku leg) could range from \$25.9 to \$37 million.

Ms. Debra Schonewill, Chairperson  
March 2, 2005  
Page 2

Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

It is noted that an additional traffic assessment was done in order to address possible traffic impacts on Baldwin Avenue which may result from the Paia Mini-Bypass. In terms of the traffic counts at the Hana Highway-Baldwin Avenue intersection, the assessment confirmed that 30 percent (30%) of the vehicles on eastbound Hana Highway turn right on Baldwin Avenue. The assessment further notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

It is noted that the timing for the traffic signal at Hana Highway and Baldwin Avenue has been adjusted to take into consideration varying traffic counts during different times of the day. For example, the Baldwin Avenue green phase is longer after school to accommodate school-related traffic. After that, the green phase on Hana Highway is lengthened in order to address afternoon commuter traffic. It is noted, however, that traffic-signal timing should again be adjusted with the completion of the Paia Mini-Bypass.

While the hours of the mini-bypass are proposed as 4:00 p.m. to 6:00 p.m., excluding Saturdays, Sundays and holidays, we are open to discussing the hours of operation for the mini-bypass. It is noted, however, that the one-lane portion of the mini-bypass is not proposed with streetlights. This may pose a safety issue if the operating hours include nighttime hours.

There have been a number of concerns raised that the Baldwin Avenue terminus of the mini-bypass makai of the Paia Post Office may conflict with traffic from the Paia Post Office driveway. Thus, we will be revising our plans to move the Baldwin Avenue terminus above or mauka of the Paia Post Office. The Final Environmental Assessment will reflect the change.

Ms. Debra Schonewill, Chairperson  
March 2, 2005  
Page 3

The length of the Hana Highway shoulder lane is slightly more than one-half ( $\frac{1}{2}$ ) mile outside of Paia Town. The traffic study done for A&B Properties back in 2002 mentioned that a single-lane roadway extending from the County parking lot to Baldwin Avenue, will not significantly reduce the length of the moving traffic queue because the access is located too close to the source of congestion within Paia Town itself. While the traffic study recommended a parallel access road extending one (1) mile outside of Paia Town, the proposed one-half ( $\frac{1}{2}$ ) mile shoulder lane still represents a significant increase in storage capacity compared to existing conditions.

Should the shoulder lane be decreased to one-fourth ( $\frac{1}{4}$ ) mile, this would decrease the effectiveness of the project in terms of diverting traffic from the main Hana Highway queue. The right shoulder lane on Hana Highway would be restricted from use as a passing lane during closed hours by use of signage. Should there be noncompliance, then enforcement by the Police Department can be requested.

We would be open to discussing signage in terms of reducing the total number of signs entering Paia Town.

With regard to parking along the Paia Beach Park fence, we note that striping for the travel lanes will move approximately four (4) feet makai from the existing line in accordance with the typical section. Vehicles currently use the paved shoulder for angle parking. Even with the new striping, there is sufficient room for angle vehicular parking although the parking angles may become more acute.

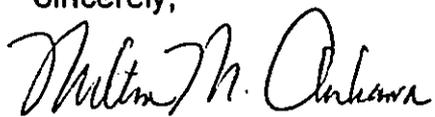
We would like to clarify that State funds are being proposed for Hana Highway improvements. Improvements for the single-lane roadway on A&B's property will be paid for by A&B and will remain the property of A&B. No public funding is being proposed to be utilized for construction of those portions of the mini-bypass located on private property. County funds are being proposed for improvements within Baldwin Avenue. When the permanent Paia Bypass is being analyzed, we believe that the mini-bypass should also be included to assess what role, if any, the mini-bypass would play in the regional transportation system.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Ms. Debra Schonewill, Chairperson  
March 2, 2005  
Page 4

Thank you for your input.

Sincerely,



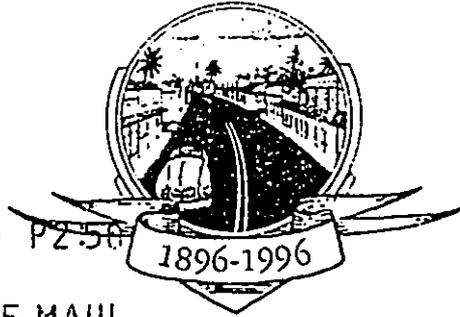
MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Jocelyn Perreira, Executive Director, Wailuku Main Street Association  
Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-paia main st

# PĀĪA



# MAIN STREET ASSOCIATION

'04 APR 20 P2:58

April 14, 2004

COUNTY OF MAUI  
PUBLIC WORKS

TO: COUNTY OF MAUI  
Robin Loudermilk  
Department of Planning  
250 S. High St.  
Wailuku, HI. 96793  
270-7735

RE: SMA Application review and comment for TMK (2) 2-5-005:018  
PAIA MINI-BYPASS PROJECT ID # SM1 2004/0004  
Applicant: Gilbert Coloma-Agaran

\* Gilbert Coloma-Agaran, Director  
Milton Arakawa, Deputy Director  
Department of Public Works  
200 South High St.  
Wailuku, HI. 96793  
270-7845

\* RE: DRAFT Environmental Assessment PAIA MINI- BYPASS

The Paia Main Street Board of Directors and Officers reviewed the SMA document and Draft Environmental Assessment regarding the PAIA MINI-BYPASS at our April 13, 2004 Board meeting. We concluded the following.

Paia Main Street would like to make it clear that we unanimously support the Paia By-Pass as the only real solution to our traffic problems.

Hana Highway traffic bottlenecks at historical Paia town due to the exponential growth of traffic headed towards Haiku/Hana after 3:00pm or all day on big wave days. The morning traffic coming from Haiku headed to Kahului also stacks up to Kuau Store.

We do support traffic calming measures, however we have never expressed that the original 'free' A&B reliever road/Paia Mini-Bypass was a solution to our traffic issues, at best, an awkward band aid, traffic calming measure. No matter how many automobiles come through historical Paia they must slow down before, throughout and after town for life safety issues. In fact we prefer additional crosswalks in town. We will work to keep the unique character and our town's sense of place by retaining a pedestrian oriented walking town, not a highway.

#### Brief history.

The A&B Reliever Road was presented 4 years ago in a Paia Main Street Association Board meeting.

The traffic study presented claimed that 30% of traffic coming to town was turning right at Baldwin and leaving town for Makawao. This resulted in an almost immediate coning change to the intersection of Hana Highway and Haleakala Highway. The permanent widening of that merging lane is complete. Kula, Pukalani and Makawao, upper Haiku residents all benefited as a result of the study. Paia did not benefit.

1231  RUSH

DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR						
DEP. DIR						
PERS.						
STAFF CE						
LUCA						
WW RECL.						
SOLID W.						
ENGR.						
HWYS.						
SECTY.						
ADD/DO						

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_

By: \_\_\_\_\_ Date: \_\_\_\_\_

4/20  
Janked 4/20/04

**p. 2 PAIA MINI-BYPASS**

Our comments four years ago were as follows:

1. We questioned the traffic studies claim regarding Baldwin Ave. right turns. We stated that most of our traffic is headed to Haiku/Hana. We asked; Did the study made sure the cars actually left town towards Makawao and weren't just stopping in Paia town?
2. We questioned the limited use (hours) and the one lane issues of the road. We felt the hours would cause confusion and be a safety issue.
3. We stated the road should connect to Baldwin above the new Paia Post Office not below.
4. We stated the Hana entrance was to close to town to do any good for the right turning cars. Traffic backs up to Kaunoa Senior Center.
5. We stated we need all of our existing parking in Paia town and do not want to encourage cars to speed up through town for obvious life safety issues.
6. We appreciated that the temporary road was free of cost, provided by A&B.

You will note in the resulting design below only item 4 above was addressed.

In both the DRAFT EA and SMA recently submitted by the County of Maui Public Works Department and Planning Department, the once called A&B reliever road, has changed significantly. Specifically; the taxpayer cost, the added State queue design and County right/left at Baldwin Ave. below the Post Office.

**Paia Main Street supports the PAIA MINI-BYPASS (A&B reliever road) with the following conditions:**

1. An updated extensive traffic study with broader parameters to determine if the mini bypass will be of any significant use. We, as Paia residents, observe a much lower number than 30% of right hand turns at Baldwin Ave actually leaving town to commute upcountry. This study should include the timing of the light, a longer time duration and possibilities of upgrading the lights software for more efficient traffic control.
2. Ideally the road would be open 24 hours a day for less confusion and safety issues. At the very least the road should be open for the hours of 3:00pm to 7:30pm.
3. The Baldwin connection that is designed below the Post Office is a safety issue. We would support an above the Post Office connection with a right turn only design.
4. The State designed shoulder lane portion is; much longer than desired. Concerns remain regarding sign pollution. There are 21 signs between Baldwin Beach Park and town. The striping somehow ended up 4 feet makai, cramping the already heavily used parallel parking along Paia park's fence. This is a safety issue due to heavy pedestrians, bikers.
5. Finally, if tax payer funds are to be used for any part of this road should it not permanently belong to the community? When the larger real BY PASS goes in will A&B just lock the gates? Concerns remain about taxpayers left with a scarring shoulder leading to nowhere. The gates should either remain open for traffic or the road should become a bikeway or long term community use.

p. 3 PAIA MINI-BYPASS

These Board comments address the widely supported concerns of our community. These comments should be utilized to determine the best use of tax payer funds. Paia Main Street Association would like to ask for the support of the County of Maui Planning Department and Public Works in our endeavors to expedite the Paia By Pass. We are convinced it is the only solution to assist our historical town from being further congested with traffic of commercial vehicles, commuters, Hana bound tourists, school routes, and local resident's. We would hope that our local communities would be worthy of the same road improvements as the resort communities. We consider our historical towns, besides the weather and the beaches, Maui's most valuable assets. That's why we volunteer to protect them. Investing in roadways that protect and enhance our town is sound planning.

We appreciate the opportunity to review the project.

Sincerely,  
Paia Main Street Association



Debra Schonewill  
Chairperson, Paia Main Street

Cc: Jocelyn Perreira, Executive Director  
Tri-Isle Main Street Program Coordinator

# PĀĪA



# MAIN STREET ASSOCIATION

July 20, 2004

**Gilbert Coloma-Agaran, Director**  
**Milton Arakawa, Deputy Director**  
Department of Public Works  
200 South High St.  
Wailuku, HI. 96793  
270-7845  
270-7955 fax

**RE: DRAFT Environmental Assessment PAIA MINI- BYPASS clarification.**

We are eagerly waiting for the information in the new traffic study. We would like to clarify what Paia Main Street Association stated in our letter dated April 14, 2004 Condition #4, referring to the state portion of the road, noting "much too long". This has lead to some confusion. *We feel that the collector turn off lane should be no longer than 1/4 mile with some kind of barrier for safety.* We note that in the EA on page 3 of Appendix B Traffic Letter Report under Findings and Conclusions in the last paragraph, your consultant traffic engineer, Ted Kawahigashi, P.E. of Austin Tsutsumi & Associates, Inc. stated, "A parallel road could be constructed to the mauka side etc." "Further there should be no opportunity for motorists using this parallel road to re-center Hana Highway closer to Paia Town to prevent "cheaters".

In the event the traffic study reveals less right turn traffic than anticipated then we would like to see an even shorter lane. Please keep us apprised on the progress of this project including any further revisions.

Sincerely,

Debra Schonewill, Chairperson  
Paia Main Street Association

Cc:  
Fred Cajugal, Administrator State DOT  
Councilman Mike Molina  
George Kaya, Governor Lingle's Liaison  
Madge Schaefer, Chairperson Governor Lingle's Advisory Committee  
Jocelyn Perreira, Executive Director  
Tri-Isle Main Street Program Coordinator

2703 *Rec'd 7/20/04*

RUSH

DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
DEP. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CC	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WWES	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DOT/AD	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Return to \_\_\_\_\_ Due \_\_\_\_\_ *7/22*

Ref. No. \_\_\_\_\_ *7/20*

By: \_\_\_\_\_ Date: \_\_\_\_\_ *7/20/04*

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Ms. Ann Pitcaithley  
43 South Laelua Place  
Paia, Maui, Hawaii 96779

Dear Ms. Pitcaithley:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments received by our Department on March 23, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

We appreciate your support of the Paia Mini-Bypass project. We note that the Environmental Assessment and Special Management Area Use Permit are required by law prior to starting construction and cannot be deferred. However, assuming these required permits can be obtained in a timely manner, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

Thank you for your input to the project.

Sincerely,

A handwritten signature in black ink that reads "Milton M. Arakawa".

MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-pitcaithley

04 MAR 23 P3:20

COUNTY OF MAUI  
PUBLIC WORKS

### Paia Mini-Bypass

Address

Ann Pitcaithley  
43 South Laelua Pl  
Paia HI 96725

Phone Number

579-9889

e-mail address

pitcaith@shaka.com

911  PUCH

DEPT. OF PUBLIC WORKS

	INFO	SECTION	SECTION	COUNTY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DEP DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LECA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENCR.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ATTN: DOT	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Return to \_\_\_\_\_ Dup \_\_\_\_\_

Ref. No. \_\_\_\_\_

By: \_\_\_\_\_ Date: \_\_\_\_\_

*3/30*  
*fixed 3/30/04*

I have the following comments on the Paia Mini-Bypass Project.

Mini Bypass now!!

Defer the environmental study

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 2, 2005

Mr. Augustin C. Quinsaas  
Ms. Pauline R. Quinsaas  
P. O. Box 790728  
Paia, Maui, Hawaii 96779

Dear Mr. and Ms. Quinsaas:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your letter of March 25, 2004 providing comments on the Draft Environmental Assessment (EA) for the Paia Mini-Bypass. We would like to provide the following response:

We acknowledge your concern regarding turning off the traffic signals, as well as removing the diagonal parking. These measures are not being proposed as part of the proposed mini-bypass project.

There is no linkage between the Paia Mini-Bypass and possible future projects such as A&B's commercial project. Any future proposal would be required to be considered under rules and regulations in existence at the time of application.

In terms of a permanent Paia Bypass, the Kala/Sunnyside Road may be considered one (1) possible alternative. It should be noted that there are also a number of other issues which need to be resolved before use of Kala/Sunnyside Road by the public is warranted. These include safety and liability issues relating to the current agricultural use of Sunnyside Road. There are also planning and route selection issues which need to be discussed for the Haiku leg of the bypass. An engineering analysis also needs to be done which discusses drainage, desired cross section of the road, and intersection design. Our analysis showed that it could take approximately six (6) to eight (8) years before the necessary planning, public hearings, environmental review, permits, design, and funding can be obtained in order to address the foregoing issues. Based on other roadways recently constructed on the island of Maui, an estimated

Mr. Augustin C. Quinsaas  
Ms. Pauline R. Quinsaas  
March 2, 2005  
Page 2

roadway cost for the Kala/Sunnyside Road segment (not including a Haiku leg) could range from \$25.9 to \$37 million. Without some interim traffic mitigation measures, we acknowledge that future additional development and resulting traffic will only worsen traffic conditions.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-quinsaas

March 25, 2004

04 APR - 1

COUNTY

Mr. Gilbert Coloma-Agaran  
Director of Public Works & Environmental Mgmt.  
200 S. High Street  
Wailuku, Maui, HI 96793

RE: PAIA BYPASS NOW!!

Dear Mr. Agaran:

1050  RUSH

DEPT. OF  
PUBLIC  
WORKS

DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DEP. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
A&B/DOJ	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_

4/5  
fused  
4/2/04

My name is Augustin C. & Pauline Quinsaas and we have been residents in Paia Date: \_\_\_\_\_ for over 49 years now and this by-pass was 1<sup>st</sup> addressed about 38-years ago ( Elmer Carvalho was the mayor at that time). This is when we 1<sup>st</sup> learned about the monies appropriated by the Federal Government for this by-pass and due to lack of community support, the funded monies went to the improvements of Lahaina roadways. Over just a short period of time, Paia, Kuau and Haiku have grown in population which means more people going through Paia to get to their destination, whether it is Kuau or Haiku. Hana Highway from Stable Road through Paia Town and continuing on to Haiku has been overly stressed not only for the people in traffic, but the pedestrians and handicapped on foot. We see all of this daily from our place of business located on 77 Hana Highway (Aloha Island Shave Ice, LLC/formerly Auggies Pool Hall) and we are also commercial property owners of 69 Hana Highway (Rainbow Jo, Lessee) and our place or residence located in back of our commercial rental.

1. We object to turning off the street lights on Baldwin and Hana Highway intersection as this will cause stress on Baldwin Ave. traffic. This is definitely not logical at all. Removing the diagonal parking stalls will create high speed travelers that rush through Paia at 35 mph and this continues daily, again not very logical.
2. We disagree with the proposed mini-bypass as this will allow leverage for A&B to get their Mini Mall built before the Proposed By-Pass gets underway. This bypass should be prioritized before any new developments. Our roads have not been improved and should be looked at with utmost importance for the safety of all automobile travelers (i.e. residents, tourist, workers, etc.) Kala and Sunnyside road would be the ideal site for this bypass and that should be treated as the reliever road and start a split roadway and if the Police Department can do a monitoring of this road during peak hours of 3:00-7:00pm this is where they can be utilized on this Kala Sunnyside road while half the road gets worked on for this bypass. If Haleakala Highway can manage their Contra-Flow while road work is in progress, I cannot see how Kala Sunnyside Road is any different when it comes

Mr. Gilbert Coloma-Agaran  
March 25, 2004  
Page 2

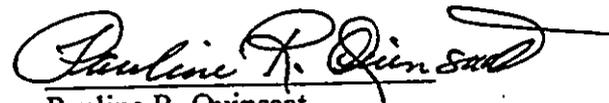
to relieving 30% of the traffic coming through Paia. At the age of 75 and my wife 65, we've seen so much growth and this is of great concern when it comes to Paia Community. County should negotiate with the owners of this Kala Sunnyside Road and buy or do an exchange of County Land if this is deemed fit.

3. Any construction or new developments should be placed on hold until this bypass gets moving because as the years go on the traffic will increase and the roads will no longer be safe. Sometimes it takes a traffic fatality to occur before any changes or improvements are done.
4. We also have a cleaning service business (A.S.K. Cleaning Service) and we take care of several vacation rental units on the beach front and this traffic back-up has affected our visitors who stay in vacation rentals in Paia, Kuau and Haiku and comments are made about the traffic and maybe reconsidering their next stay elsewhere if they have to put up with this delay in traffic while on vacation.

We've heard a lot over the years of this By-Pass and to think it's been over 38 years and still No By-Pass. Paia needs relief and NOW, so it's up to you to put your pencil to work and see that this gets done now and not another 38 years later.

Sincerely,

  
Augustin C. Quinsaat  
69 Hana Highway  
P. O. Box 790728  
Paia, HI 96779

  
Pauline R. Quinsaat  
69 Hana Highway  
P. O. Box 790728  
Paia, HI 96779

cc: Congressman Ed Case  
Governor Linda Lingle (George Kaya)  
Mayor Alan Arakawa  
Council member Mike Molina  
County council Services  
State Office DOT, Ronald F. Tsuzuki  
Maui office DOT, Fred Cajigal  
Representative Bob Nakasone  
Representative Sol Kahoohalahala  
Senator Shan Tsutsui  
Senator Kalani English

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. Hans Riecke, FAIA  
77 Apalapani Lane  
Haiku, Maui, Hawaii 96708

Dear Mr. Riecke:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments received on March 24, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

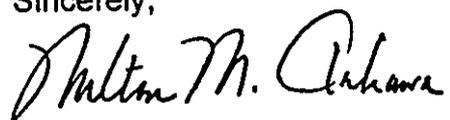
We acknowledge your suggestion to block off the diagonal parking on the Kahului side of the Baldwin Avenue intersection between the hours of 4:00 p.m. and 6:00 p.m. While this could help in providing additional traffic capacity through Paia Town during the afternoon rush hour, it has been proposed in the past and was met with significant community opposition. Your proposed restriping of the Hana Highway-Baldwin Avenue intersection would also affect existing on-street parking and/or necessitate land acquisition.

In terms of your suggestion for a secondary street through Paia Town, this would not significantly reduce the existing traffic queue on Hana Highway since it is too close to the source of congestion within Paia Town itself. However, a secondary street should likely be considered in the context of how future land use is envisioned. An appropriate venue could be during the update of the Paia-Haiku Community Plan which is scheduled to take place over the next few years.

Mr. Hans Riecke, FAIA  
March 3, 2005  
Page 2

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-riecke

LIR 04/1083

### Paia Mini-Bypass

'04 MAR 24 11:12

Name HANS RIECKE FAIA  
 Address 77 APALANI LANE  
HAIKU MAUI HI 96708  
 Phone Number 575-2520  
 e-mail address FAX 575-2077

DEPT OF PLANNING  
COUNTY OF MAUI  
RECEIVED

I have the following comments on the Paia Mini-Bypass Project.

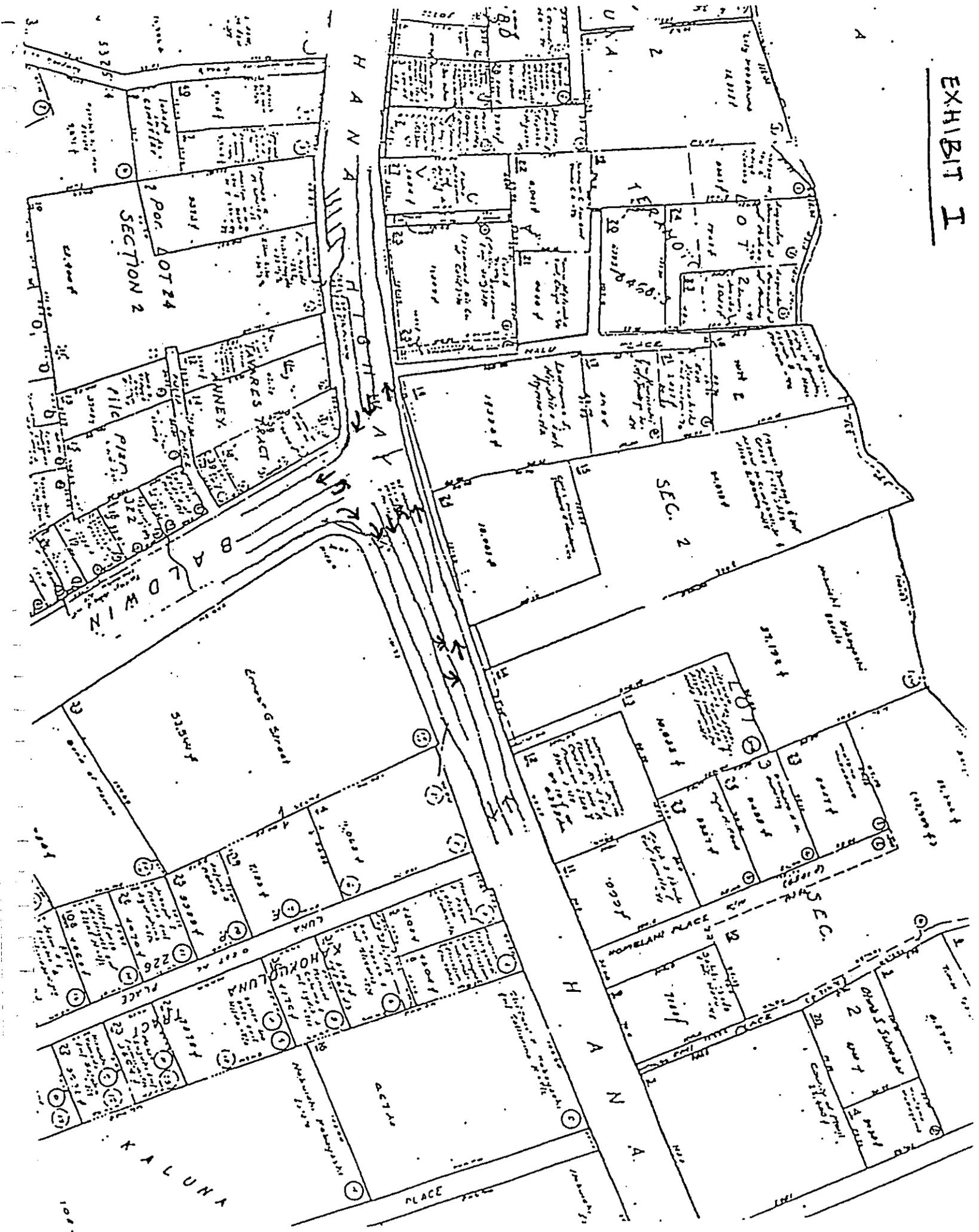
IT IS DOUBTFULL THAT THE MINI-BYPASS WILL SUBSTANTIALLY REDUCE THE  
DAILY CONGESTION. AS A FIRST STEP THE DIAGONAL PARKING ON THE  
KAHULUI SIDE OF THE BALDWIN AVE. INTERSECTION SHOULD BE BLOCKED OFF  
BETWEEN THE HOURS OF 4:00 PM TO 6:00 PM. THE MERCHANTS WOULD NOT  
LIKE IT BUT THEY DO NOT OWN THE STREET AND A PARKING LOT WAS  
BUILT FOR THEM AT TAX PAYERS EXPENSE. FOR A MORE PERMANENT  
SOLUTION I AM ENCLOSEING EXHIBIT I TO SHOW HOW THE INTERSECTION  
COULD BE RESTRIPE TO MAKE THE TRAFFIC FLOW BETTER IN BOTH DIRECTION  
THE ENCLOSED EXHIBIT II SHOWS A SUGGESTION FOR A SECONDARY STREET THROUGH  
PAIA TOWN MUCH LIKE WELLS & VINEYARD STREETS IN WAILUKU. PAIA DOES NOT

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
 Director  
 Department of Public Works and Environmental Management  
 200 South High Street  
 Wailuku, Hawaii 96793

HAVE TO  
 BE A ONE  
 INTERSECTION TOWN  
 FOR EVER.

EXHIBIT I





ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. Cary Ritt  
360 Papa Place, Suite 205  
Kahului, Maui, Hawaii 96732

Dear Mr. Ritt:

Thank you for your e-mail transmittal of April 5, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

The Draft EA discusses the use of Kaia Road and Sunnyside Road as an alternative action to the mini-bypass. Kala Road is an old government road which extends from Hana Highway to Sunnyside Road which then extends to Baldwin Avenue just mauka of the Paia Mill. Sunnyside Road is a privately owned road which must be purchased if it is desired for public use. There are also a number of other issues which need to be resolved before use of Kala/Sunnyside Road by the public is warranted. These include safety and liability issues relating to the current agricultural use of Sunnyside Road. There are also planning and route selection issues which need to be discussed for the Haiku leg of the bypass. An engineering analysis also needs to be done which discusses drainage, desired cross section of the road, and intersection design. Our analysis showed that it could take approximately six (6) to eight (8) years before the necessary planning, public hearings, permits, design, and funding can be obtained in order to address the foregoing issues. Based on other roadways recently constructed on the island of Maui, an estimated roadway cost for the Kala/Sunnyside Road segment (not including a Haiku leg) could range from \$25.9 to \$37 million.

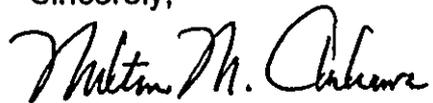
The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

Mr. Cary Ritt  
March 3, 2005  
Page 2

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-ritt

**From:** "Cary Ritt" <caryritt@windsurfari.com>  
**To:** <Milton.Arakawa@co.maui.hi.us>  
**Date:** 4/5/04 12:18PM  
**Subject:** Paia bypass

Please, open Kala Road since the money spent doing that would at least contribute to a "long-term" solution once the road can be extended thru to Hookipa vs. the mini-bypass "short-term" solution. But please do something asap.

Cary Ritt - 871-7766

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration  
TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division  
CARY YAMASHITA, P.E.  
Engineering Division  
BRIAN HASHIRO, P.E.  
Highways Division  
Solid Waste Division

March 3, 2005

Mr. Nick Rusnak  
581 Pili Loko Street  
Paia, Maui, Hawaii 96779

Dear Mr. Rusnak:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments from the March 17, 2004 Paia informational meeting, as well as comments received by our Department on March 24, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

We appreciate your comments on the use of Kala/Sunnyside Road. Certainly, this can be considered one (1) alternative for a permanent Paia Bypass. However, there are number of unresolved issues which need to be addressed including safety and liability, planning, permitting, engineering and land acquisition. We estimate that this will take at least six (6) to eight (8) years. Based on other similar new roadway projects within the island of Maui, an estimate of the project cost ranges from \$25.9 million to \$37 million.

We do not believe that the suggestion to turn off the stoplight to a yield and installation of a speed hump would significantly reduce the length of the moving queue on Hana Highway. The reason is that the constraints in processing more traffic through the traffic signal occurs before the signal itself. These constraints are diagonal parking, left-turning vehicles and pedestrians crossing the street.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from

Mr. Nick Rusnak  
March 3, 2005  
Page 2

Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-rusnak

Paia Mini-Bypass

04 MAR 24 P3:09

Name **COUNTY OF MAUI PUBLIC WORKS** Nick Rusnak  
 Address 581 Pili Loko St.  
Paia  
 Phone Number 579-6052  
 e-mail address \_\_\_\_\_

**RUSH**

DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
DEP. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
VW RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
<b>AB/DOT</b>	<input checked="" type="checkbox"/>	<input type="checkbox"/>				

Return to \_\_\_\_\_ Due \_\_\_\_\_  
 Ref. No. \_\_\_\_\_  
 By: \_\_\_\_\_

*Handed 3/24/04*

I have the following comments on the Paia Mini-Bypass Project.

The use of Kala Road as a permanent by-pass is ill-considered, expensive, and irresponsible. It does not even consider this road's use as a main artery for the cane industry. At best it is the whim of a novice politico looking for a soapbox, which explains the lynch-mob mentality of its supporters. Best Idea! Turn off the stop light to a yield... To aid in slowing traffic at the intersection, install speed bumps one-car-length away from the intersection. This allows the car which has already negotiated the bump the time necessary to cross the road, and staggers traffic evenly along the highway instead of in massive clumps. Thanks!

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
 Director  
 Department of Public Works and Environmental Management  
 200 South High Street  
 Wailuku, Hawaii 96793

MAR-24-04 15:37 FROM: D.O.T. HIGHWAYS

ID: 808 873 3344

FROM: LEONARD L. ... N.H.

TO: ...

...

Kudos to Mr. Agaran for handling the lynch-mob at the Paia Community Center on 3/17/4. Use of the cane road as a permanent by-pass is an expensive use of money better suited to the far worse traffic situation on Lahaina side. It seems more an ill-conceived and irresponsible whim of a novice politician seeking popular acclaim from a soapbox.

The residents and business owners most vehemently favoring wresting the road from the cane companies are the same ones threatening legal action over a few diagonal parking slots at the Hana Hwy / Baldwin Ave intersection. One even claimed it is safer for cars to back into the crowded intersection than to remove the slots for a right turn lane to relieve congestion at this intersection.

Instead of thanking the cane industry for offering land and footing much of the bill for the approved by-pass, those selfish individuals vilified the cane industry and demanded more.

The best original idea of the evening came from an ex-Okinawa MP. Turn the stop light into a blinking yellow yield. It is easily tested and very inexpensive.

To aid in slowing traffic at the intersection, install speed bumps (they really work!) one-car-length from the intersection. This allows the waiting car which has already negotiated its speed bump the time necessary to cross the road. It also staggers traffic along the highway evenly instead of in massive clumps.

Thank You

Nick Rusnak  
Paia

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. Kent Smith  
President and CEO  
Smith Development  
1043 Makawao Avenue, Suite 208  
Makawao, Maui, Hawaii 96768

Dear Mr. Smith:

We have received your letter of April 2, 2004 pertaining to the Paia Mini-Bypass. We would like to provide the following additional information:

We would like to emphasize that the permanent Paia Bypass is a needed facility. Planning should be expedited in order to facilitate a solution. However, a permanent bypass would require the resolution of a number of outstanding issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one (1) possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be completed in order to comply with necessary planning, public hearings, permits, and design requirements, as well as obtain funding.

Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion three (3) months after start of completion.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Mr. Kent Smith  
March 3, 2005  
Page 2

Thank you for your input and support.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-smith

**SMITH DEVELOPMENT** Real Estate Development & Consulting

1043 Makawao Avenue, Suite 208, Makawao, HI 96768  
 Phone: (808) 572-3011 Fax: (808) 572-8378  
 www.SDHawaii.com email: KRS@SDHawaii.com

APR -8 P 3:59  
 COUNTY OF MAUI  
 PUBLIC WORKS  
 APR 2, 2004

**RUSH**

DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
ASST. DIR.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
PLANS	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
SERVICE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
FINA.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
MANAGEMENT	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
INSPECTION	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
SAFETY	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		

Return to ABB/POI Due 4/8/04

Ref. No. \_\_\_\_\_ Date: \_\_\_\_\_

*4/8*  
*forwarded 4/8/04*

Mr. Gilbert S. Coloma-Agaran, Director  
 Department of Public Works and  
 Environmental Management  
 County of Maui  
 200 South High Street  
 Wailuku, Hawai'i 96793

Dear Director Coloma-Agaran,

RE: Draft Environmental Assessment for the Proposed Pa'ia Mini Bypass

Thank you for this opportunity to offer comments on the draft Environmental Assessment for the proposed Mini Bypass road around Pa'ia Town. I fully support this effort.

As a member of the Pa'ia-Ha'iku Community Plan Citizens' Advisory Committee (CAC), I have heard many hours of discussion and many points of view on the issue of the Pa'ia bypass. When the CAC conducted our deliberations back in 1992 – more than ten years ago! – the traffic through Pa'ia warranted an alternate roadway.

Back then, many people (myself included) wanted a real bypass that could accommodate two-way traffic and that would begin and terminate far from the core of Pa'ia Town. The State Department of Transportation appeared to support and plan for this idea, but the town merchants objected out of concern that such a roadway would siphon too much traffic (and, therefore, business) from Pa'ia. In large part due to these concerns, the CAC majority recommended only generalized objectives, policies and implementing actions relating to an alternate roadway, rather than definitive statements about the location and design of a real bypass; but some of us on the CAC endorsed a minority report that advocated stronger language. Ultimately, the generalized statements were approved by the County Council and thus became part of the Community Plan that guides us today.

I still support a real bypass around all of Pa'ia Town, but after more than ten years with no other realistic options in site, a Mini Bypass makes a great deal of sense as an immediate short-term solution to Pa'ia's traffic congestion. A real bypass will take years,

Mr. Gilbert S. Coloma-Agaran  
Director  
April 2, 2004  
Page Two

if not decades, to design, plan, fund and construct. The Mini Bypass might be a band-aid solution when stitches are needed, but it only makes sense to use the band-aid until the stitches are put in place.

The concerns of the Pa'ia community and small businesses should not be overlooked or dismissed. However, a small but vocal group guided this issue in the mid-1990's, resulting in one of the island's most congested roadways. Clear thinking, sound planning, proper engineering and an accurate understanding of the history of this issue should guide us today and in the future, and will result in a better quality of life for the community as a whole.

Again, thank you for this opportunity to comment on this issue. If you have any questions in this regard, please feel free to contact me.

Sincerely,



Kent R. Smith  
President and CEO

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Ms. Lisa Starr  
P. O. Box 790008  
Paia, Maui, Hawaii 96779

Dear Ms. Starr:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments dated March 19, 2004 relating to the Draft Environmental Assessment of the Paia Mini-Bypass. We would like to provide the following response:

An additional traffic assessment was done in order to address traffic impacts on Baldwin Avenue. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

Thank you for your input to the project.

Sincerely,

A handwritten signature in black ink that reads "Milton M. Arakawa".

MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-starr

Lisa Starr  
P.O. Box 790008 Paia, HI 96779  
Ph: (808) 579-8322 Fx: (808) 579-9725  
[ecs@maui.net](mailto:ecs@maui.net)

'04 MAR 22 P3:33

COUNTY OF MAUI  
PUBLIC WORKS

March 19, 2004

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Protection  
200 S. High St.  
Wailuku, HI 96793

Re: Paia Mini-Bypass

Dear Mr. Coloma-Agaran,

I was present at the presentation given about the so called "Paia Mini-Bypass" on Tuesday, March 17<sup>th</sup> at the Paia Comm. Center. We were told to submit our concerns and that we could expect answers.

I would like to ask why a Traffic Flow Analysis has not been conducted. When will it be conducted? It seems from the comments at the meeting people from this region have serious concerns about the impact to Baldwin Avenue especially. Please listen to the people who know the area and please have a comprehensive traffic study completed before proceeding any further.

Sincerely,



Lisa Starr

897  RUSH

DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
DEP. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
FERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY	<input checked="" type="checkbox"/>	<input type="checkbox"/>				

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_ Date: \_\_\_\_\_

By \_\_\_\_\_

3/30  
Lisa Starr

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration  
TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division  
CARY YAMASHITA, P.E.  
Engineering Division  
BRIAN HASHIRO, P.E.  
Highways Division  
Solid Waste Division

March 3, 2005

Mr. Randall M. Hashimoto  
State Land Surveyor  
Department of Accounting and  
General Services  
Survey Division  
P. O. Box 119  
Honolulu, Hawaii 96810-0119

Dear Mr. Hashimoto:

Thank you for your memorandum of March 18, 2004 indicating that you have no objections on the Draft Environmental Assessment (EA) for the Paia Mini-Bypass project.

We appreciate your interest and participation in the review phase of the Draft EA. Your letter will be included as part of the Final EA.

Sincerely,

A handwritten signature in cursive script, appearing to read "Milton M. Arakawa".

MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-dags survey

LINDA LINGLE  
GOVERNOR



RUSS K. SAITO  
Comptroller  
KATHERINE H. THOMASON  
Deputy Comptroller

'04 APR 29 P 4:34  
COUNTY OF MAUI  
PUBLIC WORKS

STATE OF HAWAII  
DEPARTMENT OF ACCOUNTING  
AND GENERAL SERVICES  
SURVEY DIVISION  
P.O. BOX 119  
HONOLULU, HAWAII 96810-0119  
RECEIVED

MAR 22 12:24  
DEPT OF PLANNING  
COUNTY OF MAUI  
RECEIVED

March 18, 2004

MEMORANDUM

TO: Michael W. Foley, Planning Director  
Maui County Planning Department

ATTN: Robyn L. Loudermilk, Staff Planner

FROM: Randall M. Hashimoto, State Land Surveyor *mm*  
DAGS, Survey Division *fr*

SUBJECT: I.D.: SM1 2004/0004  
TMK: 2-5-005:018  
Project Name: Paia Mini-Bypass Project  
Applicant: Gilbert Coloma-Agaran

The subject proposal has been reviewed and confirmed that no Government Survey Triangulation Stations or Benchmarks are affected. Survey has no objections to the proposed project.

**1357**  RUSH

DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
REP. DIR	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PLA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PLANNING	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
INSPECTION	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LEGAL	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ADMIN	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECURITY	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>ADD/OUT</b>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

4/30

*initials*

Revised to \_\_\_\_\_ Due \_\_\_\_\_

By \_\_\_\_\_

Date: \_\_\_\_\_

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Ms. P. Holly McEldowney, Administrator  
State Historic Preservation Division  
Department of Land and Natural Resources  
Kakuhihewa Building, Room 555  
601 Kamokila Boulevard  
Kapolei, Hawaii 96707

Dear Ms. McEldowney:

We have received your letter of April 26, 2004 pertaining to the Paia Mini-Bypass project. As you have recommended, an archaeological inventory survey was conducted of the portion of the project corridor which extends across TMK 2-5-5:18 to determine whether historic sites are present.

An archaeological assessment report was done by Xamanek Researches and has been forwarded to your office under separate cover. The study area covered that portion of the project area consisting of the single-lane roadway extending from Hana Highway through former sugar-cane lands to Baldwin Avenue. This area has been impacted by previous field clearing, planting and harvesting activities.

The pedestrian portion of the archaeological inventory/assessment did not locate any surface structural remains. Subsurface investigation consisting of 11 backhoe trenches were utilized to sample the study area. Four (4) of the 11 backhoe trenches revealed relatively shallow marine sand deposits in the makai portions of the study area. However, no precontact or post contact cultural layers were found during the survey.

The report concludes that while no significant material culture remains were found, the presence of intact marine sand deposits between Hana Highway and a point approximately 450 feet mauka of the highway is of interest. The study notes that human burials have been located in the general area, often associated with sand deposits. Thus, precautionary monitoring is recommended during construction activities

Ms. P. Holly McEldowney, Administrator  
March 3, 2005  
Page 2

in the makai portions of the project area extending from Hana Highway to a point approximately 450 feet mauka of the highway.

In accordance with these recommendations, the applicant will implement these precautionary measures during construction.

If you have any questions, please feel free to call me at 270-7845. Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-shpd

LINDA LINGLE  
GOVERNOR OF HAWAII



213 Keda 1/19/04 RUSH KL

DEPT. OF PUBLIC WORKS  
PETER T. YOUNG  
CHAIRPERSON  
BOARD OF LAND AND NATURAL RESOURCES  
COMMISSION ON WATER RESOURCE MANAGEMENT  
DAN DAVIDSON  
DEPUTY DIRECTOR - LAND  
ERNEST Y.W. LAU  
DEPUTY DIRECTOR - WATER

'04 MAY -3 P1:20

STATE OF HAWAII  
DEPARTMENT OF LAND AND NATURAL RESOURCES  
HISTORIC PRESERVATION DIVISION  
KAKUHIHEWA BUILDING, ROOM 555W.  
601 KAMOKILA BOULEVARD  
KAPOLEI, HAWAII 96707  
RECEIVED

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input type="checkbox"/>					
DEP. DIR	<input type="checkbox"/>					
PERS.	<input type="checkbox"/>					
STAFF	<input type="checkbox"/>					
CE	<input type="checkbox"/>					
AD. RECL.	<input type="checkbox"/>					
AD. ENGR.	<input type="checkbox"/>					
AD. PLAN.	<input type="checkbox"/>					
AD. RECL. & ENGR.	<input type="checkbox"/>					
AD. STATE PARKS	<input type="checkbox"/>					

April 26, 2004

Mr. Michael Foley, Planning Director  
Department of Planning – Maui  
250 South High Street  
Wailuku, Hawaii 96793

LOG NO: 2004.1298  
DOC NO: 0404CD54

Dear Mr. Foley,

**SUBJECT: Chapter 6E-8 Historic Preservation Review – Special Management Area Permit Application for the Proposed Paia Mini-Bypass (SUBJECT I.D.: SM1 2004/0004) [County/Planning] Hamakuapoko Ahupua`a, Makawao District, Island of Maui TMK: (2) 2-5-005:018**

Thank you for the opportunity to review and comment on the Special Management Area Application (SMA) for the Proposed Paia Mini-Bypass, which was received by our staff on March 18, 2004. Our review is based on reports, maps, and aerial photographs maintained at the State Historic Preservation Division; no field inspection was conducted of the subject property.

Based on the submitted SMA, we understand the proposed undertaking consists of the construction of a one-way, single-lane roadway extending from the vicinity of Baldwin Park to the new Paia Post Office. From Baldwin Park, a shoulder lane is proposed within the Hana Highway right-of-way extending to the vicinity of the Municipal Parking Lot. The Hana Highway shoulders will be widened. A twelve-foot wide shoulder lane will be created on the mauka side of the travel lanes along with four-foot wide paved shoulders. All work within this segment will be conducted in the previously disturbed deposits of the existing Hana Highway right-of-way and base course. The shoulder lane will then turn mauka to a single-lane roadway, near the Municipal Parking Lot, skirting the existing portions of Paia Town, then connecting with Baldwin Avenue just makai of the Post Office. A twelve-foot-wide roadway with two-foot wide shoulders is proposed. At the Baldwin Avenue connection, separate left and right turn lanes are proposed. The portion of the project which extends beyond the Hana right-of-way to Baldwin Avenue is located in TMK: 2-5-005:018, which is currently vacant and undeveloped land.

A search of our records indicates an archaeological inventory survey has not been conducted of the subject property. The subject property is located just inland from the coast, in an area where pre-Contact habitations and burials have been found. As recent archaeological inventory surveys in similar geographic areas have identified historic sites beneath the till zone, we believe it is possible that habitation and burial sites may be present in the subsurface deposits of some portions of the proposed project area.

Mr. Michael Foley, Planning Director  
Page 2

Therefore, we recommend that no action be taken on the subject SMA until an archaeological inventory survey has been conducted of the portion of the project corridor which extends across TMK: 2-5-005:018 to determine whether significant historic sites are present. An acceptable report documenting the findings of the survey will need to be submitted to this office for review. If significant historic sites are identified, a mitigation plan may need to be developed, in consultation with this office, and executed.

If you have any questions, please call Cathleen A. Dagher at 692-8023.

Aloha,

*P. Holly McEldowney*

P. Holly McEldowney, Administrator  
State Historic Preservation Division

CD:jen

c: Maui Cultural Resources Commission, Dept of Plng, 250 S. High St, Wailuku, HI 96793  
Chair, Maui/Lana'i Islands Burial Council  
Kana'i Kapeliela, Burial Sites Program

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration  
TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division  
CARY YAMASHITA, P.E.  
Engineering Division  
BRIAN HASHIRO, P.E.  
Highways Division  
Solid Waste Division

March 3, 2005

Ms. Dierdre S. Mamiya, Administrator  
Department of Land and Natural Resources  
Land Division  
P. O. Box 621  
Honolulu, Hawaii 96809

Dear Ms. Mamiya:

Thank you for your letter of April 6, 2004 to Mr. Michael Foley on the Paia Mini-Bypass project.

As the agency formulating the Environmental Assessment (EA) for the project, we appreciate your interest and participation in the review phase of the EA as well. With regard to your comments, we have the following response:

The use of Kala/Sunnyside Road was discussed as part of the alternatives analysis of the EA. However, any public use and future improvements to Kala/Sunnyside Road will require a separate environmental analysis. Should this occur, the Department of Land and Natural Resources (DLNR) should be consulted as part of the environmental review process.

We acknowledge that the project must comply with applicable rules and regulations of the National Flood Insurance Program.

Thank you for your input.

Sincerely,

A handwritten signature in cursive script that reads "Milton M. Arakawa".

MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-dlnr

LINDA LINGLE  
GOVERNOR OF HAWAII



PETER T. YOUNG  
CHAIRPERSON  
BOARD OF LAND AND NATURAL RESOURCES  
COMMISSION ON WATER RESOURCE MANAGEMENT

DAN DAVIDSON  
DEPUTY DIRECTOR - LAND

ERNEST Y.W. LAU  
DEPUTY DIRECTOR - WATER

AQUATIC RESOURCES  
BOATING AND OCEAN RECREATION  
BUREAU OF CONVEYANCES  
COMMISSION ON WATER RESOURCE MANAGEMENT  
CONSERVATION AND COASTAL LANDS  
CONSERVATION AND RESOURCES ENFORCEMENT  
ENGINEERING  
FORESTRY AND WILDLIFE  
HISTORIC PRESERVATION  
KAHOOLAWE ISLAND RESERVE COMMISSION  
LAND

'04 APR 29 P4:34

'04 APR -8 P12:28 STATE OF HAWAII  
DEPARTMENT OF LAND AND NATURAL RESOURCES UI  
LAND DIVISION PUBLIC WORKS  
DEPT OF PLANNING  
COUNTY OF MAUI  
POST OFFICE BOX 621  
HONOLULU, HAWAII 96809  
April 6, 2004

1352 TRUSH

DEPT. OF PUBLIC WORKS	LD-NAV	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
-----------------------	--------	------	--------	--------	----------	------	------

SM12004-0004.RCM

Honorable Michael W. Foley  
Planning Director  
County of Maui  
Planning Department  
250 S. High Street  
Wailuku, Hawaii 96793

Dear Mr. Foley:

Subject: I.D. No.: SM1 2004/0004  
Applicant: Gilbert Coloma-Agaran  
Authority: County of Maui Department of Planning  
Project: Pala Mini-Bypass Project  
TMK: (2) 2-5-005: 018

DIRECTOR	<input type="checkbox"/>						
DEP. DIR	<input type="checkbox"/>						
PERS.	<input type="checkbox"/>						
STAFF CE	<input type="checkbox"/>						
LUCA	<input type="checkbox"/>						
WW RECL.	<input type="checkbox"/>						
SOLID W.	<input type="checkbox"/>						
ENGR.	<input type="checkbox"/>						
PLANN.	<input type="checkbox"/>						
SECTY.	<input type="checkbox"/>						

ADD/DOT

Thank you for the opportunity to review and comment on this subject. Due matter. Ref. No. \_\_\_\_\_

The Department of Land and Natural Resources' (DLNR) Land Division made available or distributed a copy of the document pertaining to the subject matter to the following DLNR Divisions for their review and comment:

- Division of Aquatic Resources
- Division of Forestry and Wildlife
- Na Ala Hele Trails
- Division of State Parks
- Engineering Division
- Commission on Water Resource Management
- Office of Conservation and Coastal Lands
- Land-Maui District Land Office
- Land-Planning and Development

Enclosed please find a copy of the Engineering Division and Land Division Maui District Land Office comment.

Based on the attached responses, the Department of Land and Natural Resources has no other comment to offer at this time.

If you have any questions, please feel free to contact Nicholas A. Vaccaro of the Land Division Support Services Branch at 1-808-587-0384.

Very truly yours,

DIERDRE S. MAMIYA  
Administrator

C: MDLO

LINDA LINGLE  
GOVERNOR OF HAWAII

RECEIVED  
LAND DIVISION



2004 APR -1 A 8:14

MAR 23 PM 1:02



STATE OF HAWAII  
DEPARTMENT OF LAND AND NATURAL RESOURCES  
LAND DIVISION  
POST OFFICE BOX 621  
HONOLULU, HAWAII 96809

PETER T. YOUNG  
CHAIRPERSON  
BOARD OF LAND AND NATURAL RESOURCES  
COMMISSION ON WATER RESOURCE MANAGEMENT

DAN DAVIDSON  
DEPUTY DIRECTOR - LAND

ERNEST Y.W. LAU  
DEPUTY DIRECTOR - WATER

AQUATIC RESOURCES  
BOATING AND OCEAN RECREATION  
BUREAU OF CONVEYANCES  
COMMISSION ON WATER RESOURCE MANAGEMENT  
CONSERVATION AND COASTAL LANDS  
CONSERVATION AND RESOURCES ENFORCEMENT  
ENGINEERING  
FORESTRY AND WILDLIFE  
HISTORIC PRESERVATION  
KAHOOLAWE ISLAND RESERVE COMMISSION

LAND  
STATE PARKS  
LD/NAV

March 19, 2004  
SM12004-0004.CMT

Suspense Date: 4/2/04

MEMORANDUM:

- TO:
- \*XXX Division of Aquatic Resources
  - \*XXX Division of Forestry & Wildlife
  - \*XXX Na Ala Hele Trails
  - \*XXX Engineering Division
  - \*XXX Division of State Parks
  - Division of Boating and Ocean Recreation
  - XXX Commission on Water Resource Management (DD)
  - \*XXX Office of Conservation and Coastal Lands
  - ~~\*XXX Land-Maui District Land Office (RD)~~
  - \*XXX Land-Planning and Development

FROM: Dierdre S. Mamiya, Administrator  
Land Division

SUBJECT: I. D. No.: SM1 2004/0004 (February 2004)  
Applicant: Gilbert Coloma-Agaran  
Project: Pala Mini-Bypass Project  
TMK: 2<sup>nd</sup>/ 2-5-005: 018  
Authority: County of Maui Department of Planning

Please review the document pertaining to the subject matter and submit your comment (if any) on Division letterhead signed and dated by the suspense date.

**\*Note: One copy of the document is available for your review in the Land Office, Room 220.**

Should you have any questions, please contact Nicholas A. Vaccaro at ext.: 7-0384. If this office does not receive your comments by the suspense date, we will assume there are no comments.

( ) We have no comments.

(X) Comments attached.

Division: MDLO

Signed: Jason K. Koga

Date: 3-30-04

Print Name: Jason K. Koga

LINDA LINGLE  
GOVERNOR OF HAWAII



STATE OF HAWAII  
DEPARTMENT OF LAND AND NATURAL RESOURCES  
LAND DIVISION

54 High Street, Room 101  
Wailuku, Hawaii 96793  
PHONE: (808) 984-8103  
FAX: (808) 984-8111

March 30, 2004

PETER T. YOUNG  
CHAIRPERSON  
BOARD OF LAND AND NATURAL RESOURCES  
COMMISSION ON WATER RESOURCE MANAGEMENT

DAN DAVIDSON  
DEPUTY DIRECTOR - LAND

ERNEST Y.W. LAU  
DEPUTY DIRECTOR - WATER

AQUATIC RESOURCES  
BOATING AND OCEAN RECREATION  
BUREAU OF CONVEYANCES  
COMMISSION ON WATER RESOURCE MANAGEMENT  
CONSERVATION AND COASTAL LANDS  
CONSERVATION AND RESOURCES ENFORCEMENT  
ENGINEERING  
FORESTRY AND WILDLIFE  
HISTORIC PRESERVATION  
KAHOOLAWE ISLAND RESERVE COMMISSION  
LAND  
STATE PARKS

Ref: SM12004-0004.CMT

**MEMORANDUM**

TO: Dierdre S. Mamiya, Administrator  
Land Division

FROM: Jason K. Koga, District Land Agent *J. Koga*  
Maui District Land Office

SUBJECT: Paia Mini-ByPass Project, SM1 2004/0004 (February 2004), County of  
Maui, TMK: (2) 2-5-005:018

We have no comments on the proposed action. However, we respectfully  
request the opportunity to review and comment on the use of Kala/Sunnyside Road  
should that alternative be pursued.

c: N. Vaccaro  
District Files

LINDA LINGLE  
GOVERNOR OF HAWAII



STATE OF HAWAII  
DEPARTMENT OF LAND AND NATURAL RESOURCES  
LAND DIVISION

POST OFFICE BOX 621  
HONOLULU, HAWAII 96809

March 19, 2004  
SM12004-0004.CMT

PETER T. YOUNG  
CHAIRPERSON  
BOARD OF LAND AND NATURAL RESOURCES  
COMMISSION ON WATER RESOURCE MANAGEMENT

DAN DAVIDSON  
DEPUTY DIRECTOR - LAND

ERNEST Y.W. LAU  
DEPUTY DIRECTOR - WATER

AQUATIC RESOURCES  
BOATING AND OCEAN RECREATION  
BUREAU OF CONVEYANCES  
COMMISSION ON WATER RESOURCE MANAGEMENT  
CONSERVATION AND COASTAL LANDS  
CONSERVATION AND RESOURCES ENFORCEMENT  
ENGINEERING  
FORESTRY AND WILDLIFE  
HISTORIC PRESERVATION  
KAHOOLAWE ISLAND RESERVE COMMISSION  
LAND

STATE PARKS  
LD/NAV

Suspense Date: 4/2/04

MEMORANDUM:

- TO:*
- \*XXX Division of Aquatic Resources
  - \*XXX Division of Forestry & Wildlife
  - \*XXX Na Ala Hele Trails
  - \*XXX Engineering Division
  - \*XXX Division of State Parks
  - Division of Boating and Ocean Recreation
  - ~~\*XXX Commission on Water Resource Management~~
  - \*XXX Office of Conservation and Coastal Lands
  - XXX Land-Maui District Land Office (RD)
  - \*XXX Land-Planning and Development

*FDV:*

*TO:*

*FROM:* Dierdre S. Mamiya, Administrator  
Land Division

SUBJECT: I. D. No.: SM1 2004/0004 (February 2004)  
 Applicant: Gilbert Coloma-Agaran  
 Project: Pala Mini-Bypass Project  
 TMK: 2<sup>nd</sup>/ 2-5-005: 018  
 Authority: County of Maui Department of Planning

Please review the document pertaining to the subject matter and submit your comment (if any) on Division letterhead signed and dated by the suspense date.

\*Note: One copy of the document is available for your review in the Land Office, Room 220.

Should you have any questions, please contact Nicholas A. Vaccaro at ext.: 7-0384. If this office does not receive your comments by the suspense date, we will assume there are no comments.

We have no comments.

Comments attached.

Division: CWRM

Signed: Edwin T. Sakoda

Date: 3/23/04

Print Name: Edwin T. Sakoda

COMMISSION ON WATER RESOURCE MANAGEMENT

04 MAR 22 4:40

RECEIVED

2004 MAR 25

RECEIVED

APR 13

LINDA LINGLE  
GOVERNOR OF HAWAII



2004 MAR 29 P 3:18

STATE OF HAWAII  
DEPARTMENT OF LAND AND NATURAL RESOURCES  
LAND DIVISION

POST OFFICE BOX 621  
HONOLULU, HAWAII 96809

PETER T. YOUNG  
CHAIRPERSON  
BOARD OF LAND AND NATURAL RESOURCES  
COMMISSION ON WATER RESOURCE MANAGEMENT

DAN DAVIDSON  
DEPUTY DIRECTOR - LAND

ERNEST Y.W. LAU  
DEPUTY DIRECTOR - WATER

AQUATIC RESOURCES  
BOATING AND OCEAN RECREATION  
BUREAU OF CONVEYANCES  
COMMISSION ON WATER RESOURCE MANAGEMENT  
CONSERVATION AND COASTAL LANDS  
CONSERVATION AND RESOURCES ENFORCEMENT  
ENGINEERING  
FORESTRY AND WILDLIFE  
HISTORIC PRESERVATION  
KAHOOLAWE ISLAND RESERVE COMMISSION  
LAND  
STATE PARKS  
LD/NAV

March 19, 2004  
SM12004-0004.CMT

Suspense Date: 4/2/04

MEMORANDUM:

- TO:
- \*XXX Division of Aquatic Resources
  - \*XXX Division of Forestry & Wildlife
  - \*XXX Na Ala Hele Trails
  - ~~\*XXX Engineering Division~~
  - \*XXX Division of State Parks
  - Division of Boating and Ocean Recreation
  - XXX Commission on Water Resource Management (DD)
  - \*XXX Office of Conservation and Coastal Lands
  - XXX Land-Maui District Land Office (RD)
  - \*XXX Land-Planning and Development

FROM: Dierdre S. Mamiya, Administrator  
Land Division

SUBJECT: I. D. No.: SM1 2004/0004 (February 2004)  
Applicant: Gilbert Coloma-Agaran  
Project: Pala Mini-Bypass Project  
TMK: 2<sup>nd</sup>/ 2-5-005: 018  
Authority: County of Maui Department of Planning

Please review the document pertaining to the subject matter and submit your comment (if any) on Division letterhead signed and dated by the suspense date.

\*Note: One copy of the document is available for your review in the Land Office, Room 220.

Should you have any questions, please contact Nicholas A. Vaccaro at ext.: 7-0384. If this office does not receive your comments by the suspense date, we will assume there are no comments.

( ) We have no comments.

Comments attached.

Division: Engineering

Signed: \_\_\_\_\_

Date: MAR 29 2004

Print Name: ERIC T. HIRANO, CHIEF ENGINEER

04 MAR 22 PM 01:08 ENGINEERING

DEPARTMENT OF LAND AND NATURAL RESOURCES  
ENGINEERING DIVISION

LA/NAV

Ref.: *SM1 2004-0004-017*

COMMENTS

- We confirm that the project site, according to the Flood Insurance Rate Map (FIRM), is located in Flood Zones *C and A4*.
- Please take note that the project site, according to the Flood Insurance Rate Map (FIRM), is located in Zone \_\_\_\_\_.
- Please note that the correct Flood Zone Designation for the project site according to the Flood Insurance Rate Map (FIRM) is \_\_\_\_\_.
- Please note that the project must comply with the rules and regulations of the National Flood Insurance Program (NFIP) presented in Title 44 of the Code of Federal Regulations (44CFR), whenever development within a Special Flood Hazard Area is undertaken. If there are any questions, please contact the State NFIP Coordinator, Ms. Carol Tyau-Beam, of the Department of Land and Natural Resources, Engineering Division at (808) 587-0267.

Please be advised that 44CFR indicates the minimum standards set forth by the NFIP. Your Community's local flood ordinance may prove to be more restrictive and thus take precedence over the minimum NFIP standards. If there are questions regarding the local flood ordinances, please contact the applicable County NFIP Coordinators below:

- Mr. Robert Sumimoto at (808) 523-4254 or Mr. Mario Siu Li at (808) 523-4247 of the City and County of Honolulu, Department of Planning and Permitting.
  - Mr. Kelly Gomes at (808) 961-8327 (Hilo) or Mr. Kiran Emler at (808) 327-3530 (Kona) of the County of Hawaii, Department of Public Works.
  - Mr. Francis Cerizo at (808) 270-7771 of the County of Maui, Department of Planning.
  - Mr. Mario Antonio at (808) 241-6620 of the County of Kauai, Department of Public Works.
- The applicant should include project water demands and infrastructure required to meet water demands. Please note that the implementation of any State-sponsored projects requiring water service from the Honolulu Board of Water Supply system must first obtain water allocation credits from the Engineering Division before it can receive a building permit and/or water meter.
  - The applicant should provide the water demands and calculations to the Engineering Division so it can be included in the State Water Projects Plan Update.

- Additional Comments: \_\_\_\_\_  
\_\_\_\_\_
- Other: \_\_\_\_\_  
\_\_\_\_\_

Should you have any questions, please call Mr. Andrew Monden of the Planning Branch at 587-0229.

Signed: *Andrew M. Monden*  
for ERIC T. HIRANO, CHIEF ENGINEER  
Date: *3/29/04*

LINDA LINGLE  
GOVERNOR OF HAWAII



STATE OF HAWAII  
DEPARTMENT OF LAND AND NATURAL RESOURCES  
LAND DIVISION

POST OFFICE BOX 621  
HONOLULU, HAWAII 96809

PETER T. YOUNG  
CHAIRPERSON  
BOARD OF LAND AND NATURAL RESOURCES  
COMMISSION ON WATER RESOURCE MANAGEMENT

DAN DAVIDSON  
DEPUTY DIRECTOR - LAND

ERNEST Y.W. LAU  
DEPUTY DIRECTOR - WATER

AQUATIC RESOURCES  
BOATING AND OCEAN RECREATION  
BUREAU OF CONVEYANCES  
COMMISSION ON WATER RESOURCE MANAGEMENT  
CONSERVATION AND COASTAL LANDS  
CONSERVATION AND RESOURCES ENFORCEMENT  
ENGINEERING  
FORESTRY AND WILDLIFE  
HISTORIC PRESERVATION  
KAHOOLAWE ISLAND RESERVE COMMISSION  
LAND  
STATE PARKS

LD/NAV

March 19, 2004  
SM12004-0004.CMT

Suspense Date: 4/2/04

MEMORANDUM:

TO: \*XXX Division of Aquatic Resources  
~~\*XXX Division of Forestry & Wildlife~~  
\*XXX Na Ala Hele Trails  
\*XXX Engineering Division  
\*XXX Division of State Parks  
Division of Boating and Ocean Recreation  
XXX Commission on Water Resource Management (DD)  
\*XXX Office of Conservation and Coastal Lands:  
XXX Land-Maui District Land Office (RD)  
\*XXX Land-Planning and Development

FROM: Dierdre S. Mamiya, Administrator  
Land Division

SUBJECT: I. D. No.: SM1 2004/0004 (February 2004)  
Applicant: Gilbert Coloma-Agaran  
Project: Pala Mini-Bypass Project  
TMK: 2<sup>nd</sup>/ 2-5-005: 018  
Authority: County of Maui Department of Planning

2004 MAR 24 A 9:32  
L. DIVISION  
P. DIVISION

Please review the document pertaining to the subject matter and submit your comment (if any) on Division letterhead signed and dated by the suspense date.

\*Note: One copy of the document is available for your review in the Land Office, Room 220.

Should you have any questions, please contact Nicholas A. Vaccaro at ext.: 7-0384. If this office does not receive your comments by the suspense date, we will assume there are no comments.

We have no comments.

Comments attached.

Division: \_\_\_\_\_

Signed: Michael G. Buck

Date: MAR 23 2004

Print Name: **MICHAEL G. BUCK, ADMINISTRATOR**  
**DIVISION OF FORESTRY AND WILDLIFE**

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Ms. Genevieve Salmonson, Director  
State of Hawaii  
Office of Environmental Quality Control  
235 South Beretania Street, Suite 702  
Honolulu, Hawaii 96813

Dear Ms. Salmonson:

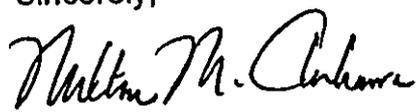
We have received your letter of April 2, 2004 pertaining to the Paia Mini-Bypass. We would like to provide the following information:

1. With regard to your comment relating to economic impact to Paia's businesses, it should be noted that the mini-bypass is planned to be operational only during the weekday afternoon peak hours, excluding holidays. Through traffic and traffic desiring to patronize Paia's businesses still must utilize Hana Highway through Paia Town. The mini-bypass only diverts traffic heading mauka on Baldwin Avenue. Should these motorists desire to patronize Paia's businesses, they may still utilize Hana Highway to shop in Paia Town. We do not believe that an economic impact study is warranted. A community informational meeting was held at the Paia Community Center on March 17, 2004 to receive feedback from Paia's businesses, residents and other interested parties.
2. The project will comply with applicable State statutes relating to the use of recycled glass.

Ms. Genevieve Salmonson, Director  
March 3, 2005  
Page 2

Thank you for your comments.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-oeqc

LINDA LINGLE  
GOVERNOR OF HAWAII



GENEVIEVE SALMONSON  
DIRECTOR

'04 APR -8 P3:59  
COUNTY OF MAUI  
PUBLIC WORKS

STATE OF HAWAII  
OFFICE OF ENVIRONMENTAL QUALITY CONTROL  
235 SOUTH BERETANIA STREET  
SUITE 702  
HONOLULU, HAWAII 96813  
TELEPHONE (808) 586-4185  
FACSIMILE (808) 586-4186  
E-mail: ooqc@health.state.hi.us

117

	INFO	ACTION	SEARCH	COMMENTS	COPY	FILE
DIRECTOR						
DEP. DIR						
PERM.						
STAFF CE						
LIDA						
MANAGER						
SOL. DIV.						
ENGR.						
INSTR.						
SOCY.						

Return to \_\_\_\_\_ Due \_\_\_\_\_  
Ref. No. \_\_\_\_\_  
By: \_\_\_\_\_ Date: \_\_\_\_\_

*4/8*  
*forced 4/8/04*

April 6, 2004

Mr. Gilbert Coloma-Agaran, Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawai'i 96809

Dear Mr. Coloma-Agaran:

Subject: Draft Environmental Assessment for the Paia Mini-Bypass

Thank you for the opportunity to review the subject document. We have the following comments.

1. Please describe how the Department arrived at the conclusion that the proposed project should not result in any adverse economic impact to businesses within Paia Town. Did the Department prepare an economic impact study? Did the Department consult with businesses in Paia Town?
2. This project should comply with sections 103D-408 of Hawaii Revised Statutes concerning the use of recycled glass.

Should you have any questions, please call Jeyan Thirugnanam at 586-4185.

Sincerely,

*Genevieve Salmonson*  
Genevieve Salmonson  
Director

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Ms. Gayln Sweet  
P. O. Box 791872  
Paia, Maui, Hawaii 96779

Dear Ms. Sweet:

Thank you for your comments from the March 17, 2004 informational meeting relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

We appreciate your comments that a long-range plan needs to be done in a timely manner to address Paia's traffic needs. We would like to emphasize that the permanent Paia Bypass is a needed facility. Planning should be expedited in order to facilitate a solution. However, a permanent bypass would require the resolution of a number of outstanding issues before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one (1) possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be completed in order to comply with necessary planning, public hearings, permits, and design requirements, as well as obtain funding.

Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

The hours of operation for the mini-bypass are proposed to be 4:00 p.m. to 6:00 p.m., excluding Saturdays, Sundays and holidays. Extension and/or adjustment of the hours of operation can be considered depending on need and personnel availability.

Ms. Gayln Sweet  
March 3, 2005  
Page 2

While we appreciate the suggestion to have a police officer direct traffic during the afternoon peak traffic hours, the causes for the traffic backup actually occur before the intersection itself. These include diagonal parking, left-turning vehicles, and pedestrians crossing the street.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input and support.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

GSCA:MA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-sweet

## Paia Mini-Bypass

Name Cayln Sweet  
Address 36 B Kulani Pl. Paia HI 96779  
Mailing - P.O. Box 791872  
Phone Number 579-9667  
e-mail address hulamaka.maui@msn.com

I have the following comments on the Paia Mini-Bypass Project.

- ① We need a "long range" over all plan in a timely manner that accomodates Paia's needs
- ② We Need our Paia Mini Bypass for "close in" solution =
- ③ A police officer directing traffic at post office between the hours of 4<sup>00</sup> 6:00

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Ms. Christine Thevenard  
24 Alenui Road  
Paia, Maui, Hawaii 96779

Dear Ms. Thevenard:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments which we received on April 2, 2004 on the Draft Environmental Assessment (EA) for the Paia Mini-Bypass. We would like to provide the following response:

There is no linkage between the Paia Mini-Bypass and possible future projects such as A&B's commercial project. Any future proposal would be required to be considered under rules and regulations in existence at the time of application.

In terms of a permanent Paia Bypass, the Kala/Sunnyside Road may be considered one (1) possible alternative. It should be noted that there are also a number of other issues which need to be resolved before use of Kala/Sunnyside Road by the public is warranted. These include safety and liability issues relating to the current agricultural use of Sunnyside Road. There are also planning and route selection issues which need to be discussed for the Haiku leg of the bypass. An engineering analysis also needs to be done which discusses drainage, desired cross section of the road, and intersection design. Our analysis showed that it could take approximately six (6) to eight (8) years before the necessary planning, public hearings, environmental review, permits, design, and funding can be obtained in order to address the foregoing issues. Based on other roadways recently constructed on the island of Maui, an estimated roadway cost for the Kala/Sunnyside Road segment (not including a Haiku leg) could range from \$25.9 to \$37 million.

The intent of the Paia Mini-Bypass project is to divert traffic intending to turn right on Baldwin Avenue. The proposed Hana Highway shoulder lane would be built entirely

Ms. Christine Thevenard  
March 3, 2005  
Page 2

within the existing right-of-way. It is needed in order to divert traffic from the slowly moving queue on Hana Highway and thereby reduce overall congestion in the afternoon peak hours.

With the proposed project, a prohibition on left turns at the Baldwin Avenue connection may result in less vehicles being able to utilize the mini-bypass. It is noted there are a number of residences and businesses which are located makai of the Baldwin Avenue connection. Thus, left turns at Baldwin Avenue are needed in order to maximize the amount of vehicles deleted from the Hana Highway queue.

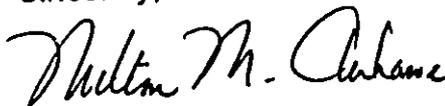
Moreover, we have done an additional traffic assessment in order to address traffic impacts on Baldwin Avenue. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-thevenard

Paia Mini-Bypass

'04 APR -2 P3:10

COUNTY OF MAUI  
Name PUBLIC WORKS KRISTINE THEVENARD

Address 24 ALENUI RD

PAIA, HI 96779

Phone Number 579-9222

e-mail address \_\_\_\_\_

1063  RUSH

DEPT OF PUBLIC WORKS	INFO	ACTION	SCOPE	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>					
DEP DIR	<input checked="" type="checkbox"/>					
PERMS.	<input type="checkbox"/>					
STAFF CE	<input type="checkbox"/>					
LUDA	<input type="checkbox"/>					
WW RECL.	<input type="checkbox"/>					
SOLID W.	<input type="checkbox"/>					
ENGR.	<input checked="" type="checkbox"/>					
HWYS.	<input checked="" type="checkbox"/>					
SECTY.	<input checked="" type="checkbox"/>					
APPLICANT	<input checked="" type="checkbox"/>					

Return to \_\_\_\_\_ Due \_\_\_\_\_  
Ref. No. \_\_\_\_\_  
By: \_\_\_\_\_ Date: \_\_\_\_\_

HS  
Kated H/c

I have the following comments on the Paia Mini-Bypass Project.

THIS SHOULD NOT COST THE TAXPAYERS A DIME!!!  
THIS ONLY SERVES AEB'S FUTURE DEVELOPMENT OF  
A SHOPPING MALL ON BALDWIN AVE. THIS WILL SERVE  
AS NO RELIEF AT ALL TO THE MOUNTING TRAFFIC PROBLEM  
WE MUST SPEND \$ ON AN EFFECTIVE PLAN THAT WILL  
TRULY BE A BYPASS, TO BYPASS THE TOWN AS  
A WHOLE! THREE LANES ENTERING SMALL CUTS,  
PLANTATION TOWN PAIA WOULD JUST RUIN THE FEEL  
AND ESSENCE OF WHAT PAIA IS ALL ABOUT. THIS →

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
 Director  
 Department of Public Works and Environmental Management  
 200 South High Street  
 Wailuku, Hawaii 96793

MINI BYPASS WAS NEVER MEANT TO BE THIS!  
IT WAS AN A&B RELIEF ROAD AND NOTHING  
MORE. LETS PUT \$ INTO A SERIOUS SOLUTION  
NOT A TEMPORARY BAND-AID. AFTER WAITING  
TWENTY YEARS FOR A BYPASS ROAD FOR PAIA,  
I CAN WAIT ANOTHER TEN TO SEE IT DONE  
THE RIGHT WAY! THIS "MINI BYPASS" IS  
THE SILLIEST SOLUTION I'VE EVER HEARD OF!  
NEWER TRAFFIC STUDIES SHOULD BE DONE,  
AND NOT BY A&B. WHAT % OF THE 30%  
OF RIGHT HAND TURNS ONTO BALDWIN AVE GO  
BACK ONTO HANA HWY? THE MINI BYPASS  
SHOULD ONLY HAVE A RIGHT TURN EGRESS ONTO  
BALDWIN. SO BASICALLY THE ONLY PEOPLE  
USING THIS ROAD LIVE IN <sup>UPPER PAIA</sup>  
<sub>W HAWAII</sub>  
ALL THE A&B SUBDIVISIONS ARE WHATS MAKING  
THIS TRAFFIC BACKED UP. I CAN ONLY SAY THAT  
I HOPE YOU LET THE PAIA COMMUNITY DECIDE OUR  
OWN TRAFFIC FATE. PLEASE LISTEN TO OUR  
COLLECTIVE INPUT. THIS "MINI BYPASS" IS A  
WASTE OF \$!

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. Jack Thompson  
204 Kealakai Place  
Paia, Maui, Hawaii 96779

Dear Mr. Thompson:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your e-mail transmittal of March 28, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. You are correct in noting that this project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

The comment in the Draft EA on community consensus pertained to the previous effort to implement the Paia Bypass.

There is no linkage between the mini-bypass project and possible future projects by A&B. Any future proposal would be required to be considered under rules and regulations in existence at the time of application.

We note that the State Department of Transportation intends to start the planning and design process for the permanent Paia Bypass. One of the issues which needs to be addressed is obtaining a consensus on the permanent alignment. We agree that the permanent Paia Bypass should be expedited. However, the Paia Mini-Bypass can provide immediate relief to traffic congestion in the afternoon peak hours.

Mr. Jack Thompson  
March 3, 2005  
Page 2

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-thompson

Milton Arakawa - Mini-bypass

**From:** Thompson/Maui <jjmaui@maui.net>  
**To:** <milton.arakawa@co.maui.hi.us>  
**Date:** 3/29/04 9:08AM  
**Subject:** Mini-bypass

March 28, 2004  
To: Milton Arakawa  
From: Jack Thompson  
Re: Paia mini-bypass

Milton,

Allow me to begin with an observation or two. The above project, in my opinion, has become a fiasco. First, it does a minimal job of relieving Paia and Hana Highway traffic congestion. By State guess, it will remove only thirty per cent of the existing traffic. Seventy per cent will continue today's frustration. Second, a statement is made in the Environmental Assessment booklet that suggests there is opposition to ANY Paia by-pass and that is why the project has taken so long. That opposition took place about twenty years ago when the Paia Business Association was fearful of a bypass taking away business. Since that time the total business community has become one of the strongest proponents for a bypass. Third, about five years ago A&B presented their plans for commercial developments behind the existing Baldwin Ave. business community. They planned a roadway off of Hana Highway, through the planned development, to Baldwin Ave. That project had such strong opposition that it was not approved at the County level. Under the existing mini-bypass plan that roadway would be built and, once in, will bring about pressure from A&B to continue with their commercial development plans. Such a plan would bring MORE people to Paia and add to current traffic problems. Also, when a permanent bypass is completed the proposed turning lane on Hana Highway will direct people to the new shopping area. A long-ranged A&B plan?

Though I can see small advantages to completing this mini-bypass, I think the resources required are too great. Let us put politics and special interest groups out of the decision-making and replace them with reason and what's good for the community. My suggestion is that we put the mini-bypass aside and concentrate all our efforts and resources towards developing a permanent by-pass as soon as possible. Mike Molina was assured, by the State DOT representative at the last Paia meeting on this subject, that a permanent by-pass could be completed much earlier than the 8 to 10 years usually quoted. Let's gather all our forces from the Communities, County, State and Feds and cooperatively work towards an early completion of the permanent by-pass.

Aloha,  
Jack Thompson

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. James Pennaz, P.E.  
Chief, Civil Works  
Technical Branch  
Department of the Army  
U.S. Army Engineer District, Honolulu  
Fort Shafter, Hawaii 96858-5440

Dear Mr. Pennaz:

Thank you for your letter of March 23, 2004 providing clarification on flood hazard information and Department of the Army permits.

We appreciate your provision of this information, as well as your interest and participation in the review phase of the Draft Environmental Assessment (EA). Your letter will be included as part of the Final EA.

Sincerely,

MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia\_jpennaz



DEPARTMENT OF THE ARMY  
U.S. ARMY ENGINEER DISTRICT, HONOLULU  
FORT SHAFTER, HAWAII 96858-5440

REPLY TO  
ATTENTION OF: CEPOH-EC-T

04 APR 29 P 4:34

COUNTY OF MAUI  
PUBLIC WORKS

Civil Works Technical Branch

Ms. Robyn L. Loudermilk, Staff Planner  
County of Maui  
Department of Planning  
250 South High Street  
Wailuku, Maui, Hawaii 96793

Dear Ms. Loudermilk:

Thank you for the opportunity to review and comment on the Special Management Area Application and Draft Environmental Assessment (DEA) for the Paia Mini Bypass Project, Paia, Maui (TMK 2-5-5: 18). The following comments are provided in accordance with Corps of Engineers authorities to provide flood hazard information and to issue Department of the Army (DA) permits.

- a. Based on the information provided, a DA permit is not required for the project.
- b. The flood hazard information provided in the DEA is correct.

Should you require additional information, please contact Ms. Jessie Dobinchick of my staff at (808) 438-8876.

Sincerely,

*for Thomas D. Smith, P.E.*  
James Pennaz, P.E.  
Chief, Civil Works  
Technical Branch

SMT CA/0004  
RL

1354

RUSH

DEPT. OF  
PUBLIC  
WORKS

INFO ACTION SEE ME COMMENTS COPY FILE

March 23, 2004

'04 MAR 24 12:53

DEPT OF PLANNING  
COUNTY OF MAUI  
RECEIVED

DIRECTOR	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DEP. DIR	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PLANNING	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENVIRONMENTAL	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
HWY'S.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ACT/DOT	<input checked="" type="checkbox"/>	<input type="checkbox"/>				

Return to \_\_\_\_\_ Due \_\_\_\_\_

Ref. No. \_\_\_\_\_

By: \_\_\_\_\_ Date: \_\_\_\_\_

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Ms. Verna M. U'u  
P. O. Box 790417  
Paia, Maui, Hawaii 96779

Dear Ms. U'u:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments from the March 17, 2004 Paia informational meeting on the Paia Mini-Bypass. We would like to provide the following response:

You have suggested additional crosswalks to Baldwin and Hana Highways or start issuing citations. Mid-block crosswalks in Paia would raise a safety issue especially where angle parking currently exists. Cars parked at an angle would tend to shield pedestrians from the view of oncoming motorists. Enforcement issues regarding jaywalking would be under the jurisdiction of the Police Department.

You have suggested that Sunnyside Road be deleted and have suggested another alternative which extends Stable Road in a mauka direction. These are possible alternatives which could be considered in the planning for a permanent Paia Bypass.

The Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While we believe that a permanent Paia Bypass should be expedited, it should be emphasized that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Ms. Verna U'u  
March 3, 2005  
Page 2

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.

s:\milton\paia-u'u

## Paia Mini-Bypass

Name VERNA M. U'u  
Address P.O. Box 790417  
Paia  
Phone Number 579-9477 / 244-5828  
e-mail address \_\_\_\_\_

I have the following comments on the Paia Mini-Bypass Project.

- 1- Add more crosswalks to Baldwin & Anna Highway  
or start issuing citations.
2. Scrap the sunnyside issue. It doesn't serve  
any purpose.
- 3- From Spreckleville to Kaha Stable Rd. Mauka  
Take by pass below mill to Maliko H'paled  
to Maliko.

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. Jay Van Zwalenburg  
P. O. Box 790367  
Paia, Maui, Hawaii 96779

Dear Mr. Van Zwalenburg:

Thank you for your comments from the March 17, 2004 informational meeting relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response:

In answer to your concern that traffic impact on Baldwin Avenue needs to be examined, an additional traffic assessment was done. The assessment notes that the mini-bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town. In addition, reduction in travel demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue. The traffic assessment will be included in the Final Environmental Assessment.

In response to a number of other comments that the Baldwin Avenue terminus of the mini-bypass makai of the Paia Post Office may conflict with traffic from the Paia Post Office driveway, we will be revising our plans to move the Baldwin Avenue terminus above or mauka of the Paia Post Office. The Final Environmental Assessment will reflect the change.

We would like to emphasize that the permanent Paia Bypass is a needed facility which should be able to address west-bound traffic from Haiku, as well as east-bound traffic. Planning should be expedited in order to facilitate a solution. However, a permanent bypass would require the resolution of a number of outstanding issues

Mr. Jay Van Zwalenburg  
March 3, 2005  
Page 2

before it can become a reality. For example, our analysis of use of the Kala/Sunnyside Road which can be considered one (1) possible route for a permanent bypass, showed that it could take approximately six (6) to eight (8) years before it can be completed in order to comply with necessary planning, public hearings, permits, and design requirements, as well as obtain funding.

Thus, the Paia Mini-Bypass is seen as an interim solution to traffic issues in the region. This project would divert approximately 30 percent (30%) of the traffic from Hana Highway during afternoon peak hours. The project cost is approximately \$650,000 and assuming required permits can be obtained, construction of the mini-bypass can be initiated during the second quarter of 2005 with completion anticipated three (3) months after start of construction.

While a permanent Paia Bypass should be expedited, we believe that the Paia Mini-Bypass project will be able to provide some immediate relief to traffic congestion in the afternoon peak hours.

Thank you for your input.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-van zwalenburg

## Paia Mini-Bypass

Name Jay Van Zwanenburg  
Address Po Box 790367  
Paia 96779  
Phone Number 579-9405  
e-mail address \_\_\_\_\_

I have the following comments on the Paia Mini-Bypass Project.

- ① There is a need to analyze the traffic impact on  
Baldwin next to the P.O. at the intersection.
  - ② The west bound traffic from Haiku is worsening -  
what is planned for future mitigation.
  - ③ Permanent bypass needs to be pushed.
- \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor

MILTON M. ARAKAWA, A.I.C.P.  
Director

MICHAEL M. MIYAMOTO  
Deputy Director

Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. Jason White  
P. O. Box 790999  
Paia, Maui, Hawaii 96779

Dear Mr. White:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your comments which we received on March 29, 2004 relating to the Draft Environmental Assessment (EA) of the Paia Mini-Bypass. We would like to provide the following response.

Regarding the existing traffic signal at Hana Highway and Baldwin Avenue, the timing can be adjusted, as appropriate, to maximize the amount of cars flowing through each approach. However, as the traffic study in the EA notes, there are factors which limit the amount of cars which can be processed through Paia Town. These include diagonal parking, left-turning vehicles and pedestrians crossing the street. These constraints would also apply even if a policeman were present to direct traffic at the Hana Highway-Baldwin Avenue intersection.

With regard to your comment on reduction of available parking along Paia Beach Park and the Paia Youth and Cultural Center, it is noted that informal parking occurs in the general vicinity of Paia Beach Park. This includes the area along the fence on the makai side of the Hana Highway right-of-way near the Paia Beach Park, the mauka side of Hana Highway, along Puna Road, and on the makai side of Hana Highway on the Wailuku side of Puna Road. With the proposed improvements, there would still be space for angle parking along the fence although parallel parking which currently takes place on the mauka side of Hana Highway would be curtailed. However, informal parking space is still available on the makai side of Hana Highway extending closer to Baldwin Park.

Mr. Jason White  
March 3, 2005  
Page 2

Thank you for your input to the project.

Sincerely,



MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso

xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-jwhite

974  RUSH

Paia Mini-Bypass

'04 MAR 29 P3:14

Name JASON WHITE  
 Address PO Box 790999  
Paia, HI 96779  
 Phone Number 264-6161  
 e-mail address Jason@mauihm.com

DEPT. OF PUBLIC WORKS

	INFO	ACTION	SEE ME	COMMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
DEP. DIR	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PERS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LICA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>				

Return to \_\_\_\_\_ Due \_\_\_\_\_  
 Ref. No. \_\_\_\_\_  
 By: \_\_\_\_\_ Date: \_\_\_\_\_

I have the following comments on the Paia Mini-Bypass Project.

1- This is an unnecessary expenditure of tax payer's money - This road will do nothing to alleviate traffic; perhaps will create more. A high percentage of people turning right onto Baldwin Ave - go back down after they shop. Change the timing of the light. Try a traffic cop for a 4 way stop situation before doing this - Also you cannot have it go both ways down Baldwin!

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
 Director  
 Department of Public Works and Environmental Management  
 200 South High Street  
 Wailuku, Hawaii 96793

## Paia Mini-Bypass

Name \_\_\_\_\_

Address \_\_\_\_\_

Phone Number \_\_\_\_\_

e-mail address \_\_\_\_\_

I have the following comments on the Paia Mini-Bypass Project.

*you need a REAL traffic assessment  
study - creating another lane on the  
Hone Highway is ridiculous - and we  
cannot afford to lose one parking  
spot along Baby Park or the  
Paia Youth Cultural Center*

*If this goes through - it is against the  
wishes of the community!*

Please turn in this sheet with your comments to a representative of the County of Maui Department of Public Works and Environmental Management. Alternatively, it may be mailed to:

Mr. Gilbert Coloma-Agaran  
Director  
Department of Public Works and Environmental Management  
200 South High Street  
Wailuku, Hawaii 96793

ALAN M. ARAKAWA  
Mayor  
MILTON M. ARAKAWA, A.I.C.P.  
Director  
MICHAEL M. MIYAMOTO  
Deputy Director  
Telephone: (808) 270-7845  
Fax: (808) 270-7955



COUNTY OF MAUI  
**DEPARTMENT OF PUBLIC WORKS  
AND ENVIRONMENTAL MANAGEMENT**  
200 SOUTH HIGH STREET, ROOM 322  
WAILUKU, MAUI, HAWAII 96793

RALPH NAGAMINE, L.S., P.E.  
Development Services Administration

TRACY TAKAMINE, P.E.  
Wastewater Reclamation Division

CARY YAMASHITA, P.E.  
Engineering Division

BRIAN HASHIRO, P.E.  
Highways Division

Solid Waste Division

March 3, 2005

Mr. Calvin M. Yang, Postmaster  
Paia Post Office  
120 Baldwin Avenue  
Paia, Maui, Hawaii 96779-9998

Dear Mr. Yang:

**SUBJECT: PAIA MINI-BYPASS**

Thank you for your letter of April 1, 2004 providing comments on the Draft Environmental Assessment for the Paia Mini-Bypass. We would like to provide the following response:

The information presented in your letter pertaining to the operation of the post office and its customers has been very helpful. In answer to your comments, we will be revising our plans to move the Baldwin Avenue terminus above or mauka of the Paia Post Office. The Final Environmental Assessment will reflect the change.

Thank you for your input to the project.

Sincerely,

A handwritten signature in cursive script that reads "Milton M. Arakawa".

MILTON M. ARAKAWA, A.I.C.P.  
Director

MMA:jso  
xc: Office of Environmental Quality Control  
Department of Transportation, Highways Division-Maui  
Alexander & Baldwin Properties, Inc.  
s:\milton\paia-yang



'04 APR -6 AM 11:13

120 Baldwin Avenue  
Paia HI 96779-9998

COUNTY OF MAUI  
PUBLIC WORKS

Milton Arakawa  
Maui County Public Works  
200 High Street  
Wailuku HI 96793

Dear Milton,

Thank you once again for allowing my input on the potential traffic hazard that would have been created if the Paia Mini Bypass was left to enter Baldwin Ave makai of the Paia Post Office.

Here are my concerns in writing as requested.

1. The proposed entrance to Baldwin Ave will be much to close to the entrance/exit of the Post Office.
2. Directly across is Melia Street in addition to the crosswalk fronting the Post Office.
3. Cars are frequently parked along side the sidewalk in front of the Post Office as well as across at the Laundry Washerette narrowing the width of Baldwin Ave at this point.
4. Cars also frequently use the entrance way of the postal employee entrance to veer off Baldwin Ave to execute a U-turn to head towards town. The dirt parking area at the Washerette is also used for this purpose.
5. We have a large amount of customers living near the Post Office who walk to do their postal business. Having additional traffic at this point would endanger our elderly community.
6. The traffic heading makai on Baldwin in the afternoon hours is quite heavy and at times is backed up to the post office.

Again, thank for your consideration in this matter. I can be reached at 579-8866 or faxed at 579-9758 if needed.

Sincerely,

*Calvin M Yang*  
Calvin M Yang  
Postmaster Paia

475 L'ENFANT PLAZA SW  
WASHINGTON DC 20260-

FAX:

1083  RUSH

DEPT. OF  
PUBLIC  
WORKS

	INFO	ACTION	STAKE	DOCUMENTS	COPY	FILE
DIRECTOR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
DEP. DIR	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
STAFF CE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
LUCA	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
WW RECL.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SOLID W.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ENGR.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
HWYS.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SECTY.	<input checked="" type="checkbox"/>	<input type="checkbox"/>				
AP/DOT	<input checked="" type="checkbox"/>	<input type="checkbox"/>				

Date: April 1, 2004 S.

Return to \_\_\_\_\_ Due \_\_\_\_\_

By: \_\_\_\_\_ Date: \_\_\_\_\_

*46*  
*faxed 4/6*

### References

Community Resources, Inc., Maui County Community Plan Update Program Socio-Economic Forecast Report, January 1994.

County of Maui, Country Town Design Guidelines - Paia-Haiku, April 1990.

County of Maui, The General Plan of the County of Maui 1990 Update, 1990.

County of Maui, Paia-Haiku Community Plan, May 1995.

County of Maui, Makawao-Pukalani-Kula Community Plan, July 1996.

County of Maui, Office of Economic Development, Maui County Data Book 2001, June 2001.

Land Study Bureau, Detailed Land Classification - Island of Maui, May 1967.

Munekiyo, Arakawa & Hiraga, Inc. Environmental Assessment-Paia Municipal Parking Lot, April 1999.

R. M. Towill Corporation, Baldwin Avenue Bike Facility Feasibility Study, Prepared for the County of Maui, Department of Public Works and Waste Management, April 2000.

University of Hawaii, Department of Geography, Atlas of Hawaii, Third Edition, 1998.

U.S. Department of Agriculture, Soil Conservation Service, Soil Survey of Islands of Kauai, Oahu, Maui, Molokai and Lanai, State of Hawaii, 1972.

APPENDIX A  
ARCHAEOLOGICAL  
ASSESSMENT REPORT

**An Archaeological Assessment Report for a  
Portion of Land in Paia  
Hamakua, Maui, Makawao District  
Island of Maui  
(TMK 2-5-05, Portion of 13)**

**Prepared for:**

**The State Historic Preservation Division**

**On behalf of:**

**The County of Maui  
Department of Public Works &  
Environmental Management,  
Engineering Division  
Wailuku, Maui**

**Prepared by:**

**Xamanek Researches  
Pukalani, Maui**

**Erik Fredericksen**

**14 December 2004**

## ABSTRACT

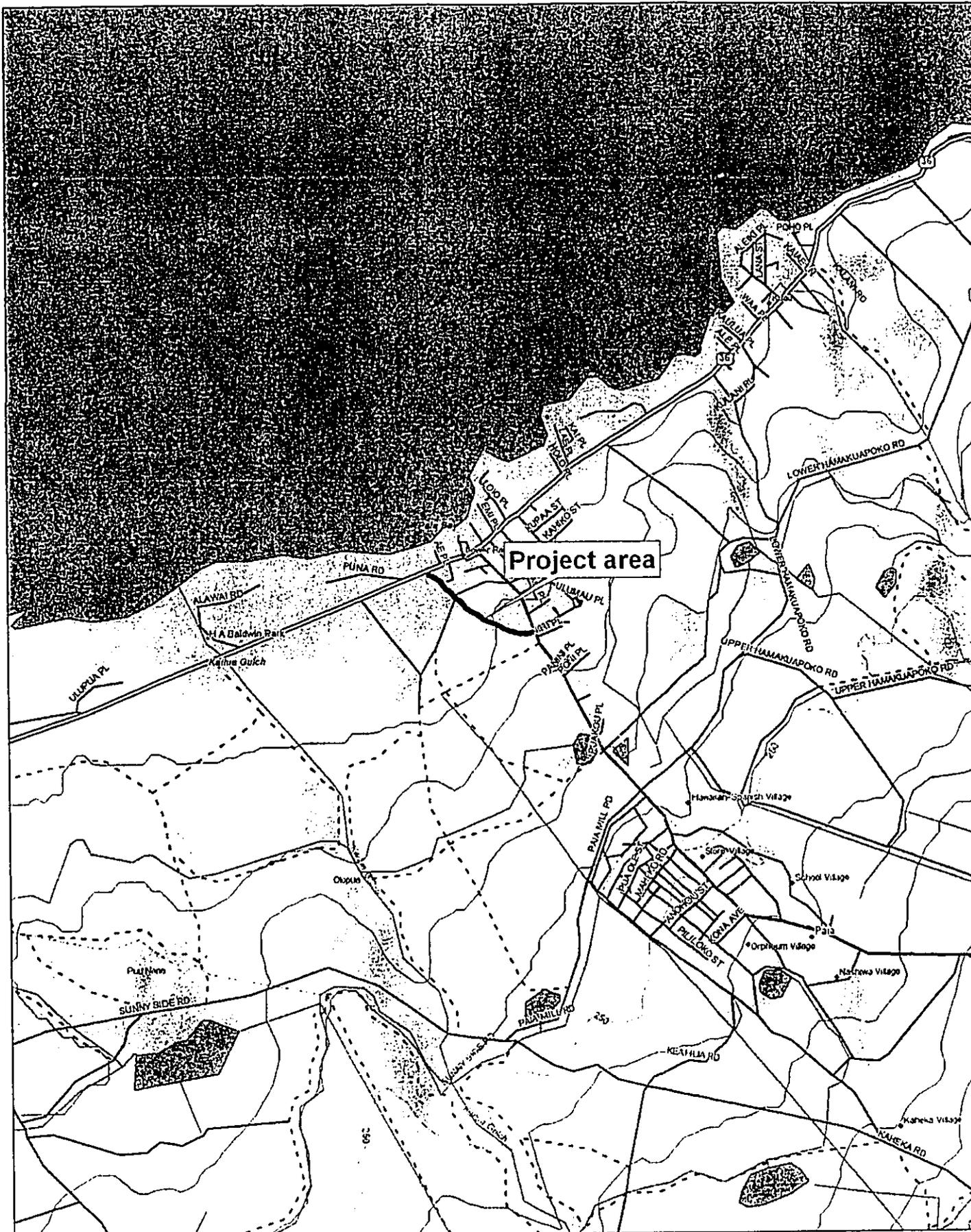
Xamanek Researches conducted an archaeological assessment for a portion of former sugarcane land in Pa`ia, Hamakuapoko *ahupua`a*, Makawao District, Maui, during the late fall of 2004 (TMK: 2-5-05: Portion of 18). The archaeological survey was conducted on behalf of the County of Maui (COM) Department of Public Works and Environmental Management (DPWEM). The State Historic Preservation Division (SHPD) had earlier reviewed COM plans for a proposed Pa`ia Mini Bypass, and indicated that an archaeological inventory/assessment survey was required.

Test results indicate that the upper 40-90 cm of study area was previously impacted by earthmoving activities associated with commercial sugarcane production. There were no significant material culture remains located during subsurface testing, but some scattered modern refuse was found. Four backhoe trenches excavated near Hana Highway yielded intact deposits of beach sand. These sand deposits were generally encountered at 80 cmbs or deeper. Given the proximity to the existing shoreline, it was not unexpected to encounter sand deposits in the general vicinity of Hana Highway. It is interesting to note that evidence of marine sand deposits were encountered as far away as STA 4+50 from the road. Given that burials have been located in the general area in sand deposits, precautionary archaeological monitoring is recommended for the lower (Northwestern ) portion of the proposed Pa`ia Mini Bypass construction project between STA 0+0 and STA 5+00.

**Table of Contents**

Map 1 – Topographic map.....ii  
 Map 2 – Topographic map with corridor location.....iii  
 Map 3 – State Tax Map.....iv

INTRODUCTION.....1  
 THE STUDY AREA.....1  
     Photograph 1 – View to the southwest.....3  
     Photograph 2 – View to the southwest.....3  
     Photograph 3 – View to the northeast.....4  
     Photograph 4 – View to the southeast.....4  
     Photograph 5 – View to the southwest.....5  
     Photograph 6 – View to the northwest.....5  
 BACKGROUND RESEARCH.....6  
     Figure 1 – SHPD map (Pa`ia Quadrangle).....8  
     Table I – Coastal sites in Hamakuapoko ahupua`a.....9  
 PREVIOUS ARCHAEOLOGICAL WORK.....9  
 ARCHAEOLOGICAL METHODS.....11  
 ARCHAEOLOGICAL RESULTS.....11  
     Figure 2 – Map of the project area with backhoe trench locations.....12  
     Figure 3 – Northeast profile of BT 1.....13  
     Figure 4 – Northwest profile of BT 1.....14  
     Figure 5 – Northeast profile of BT 3.....15  
     Figure 6 – Northeast profile of BT 4.....15  
     Figure 7 – Northwest profile of BT 5.....16  
     Figure 8 – North face profile of BT 6.....17  
     Figure 9 – North face profile of BT 7.....18  
     Figure 10 – North face profile of BT 8.....19  
     Figure 11 – South face profile of BT 9.....20  
     Figure 12 – Southeast profile of BT 10.....20  
     Figure 13 – East face profile of BT 11.....21  
     Table 3 – Summary of Backhoe Trench Results.....22-23  
 SUMMARY AND CONCLUSIONS.....23-24  
     Site Significance Evaluations.....24  
     Site Mitigation Recommendations.....24-25  
     References.....25  
 APPENDIX A – Photographs 7-17.....25-31

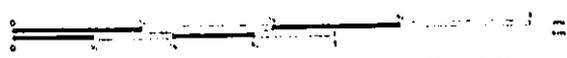


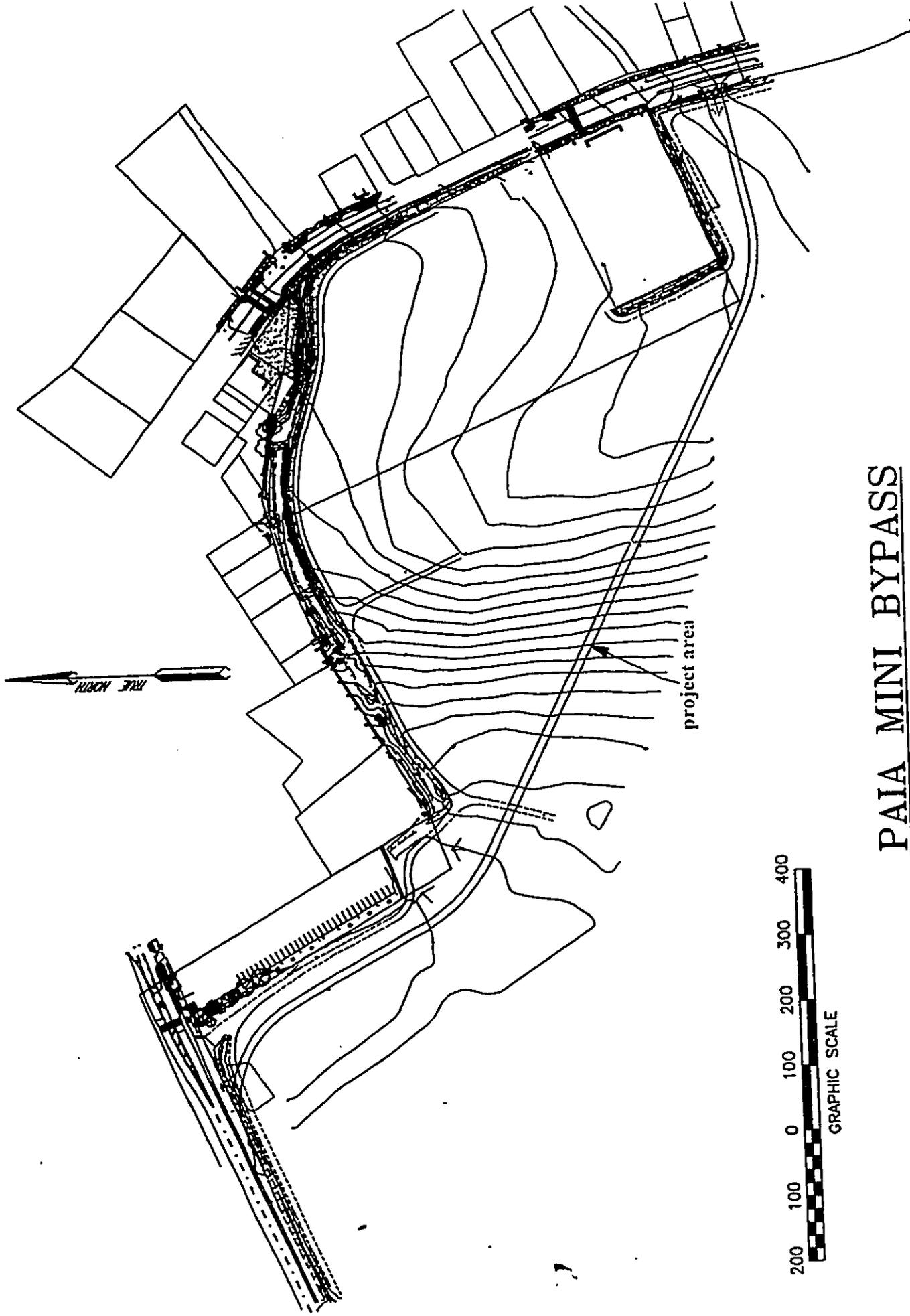
**DELORME**

© 2001 DeLorme, Topo USA 3.0

Zoom Level: 13-1 Datum: WGS84

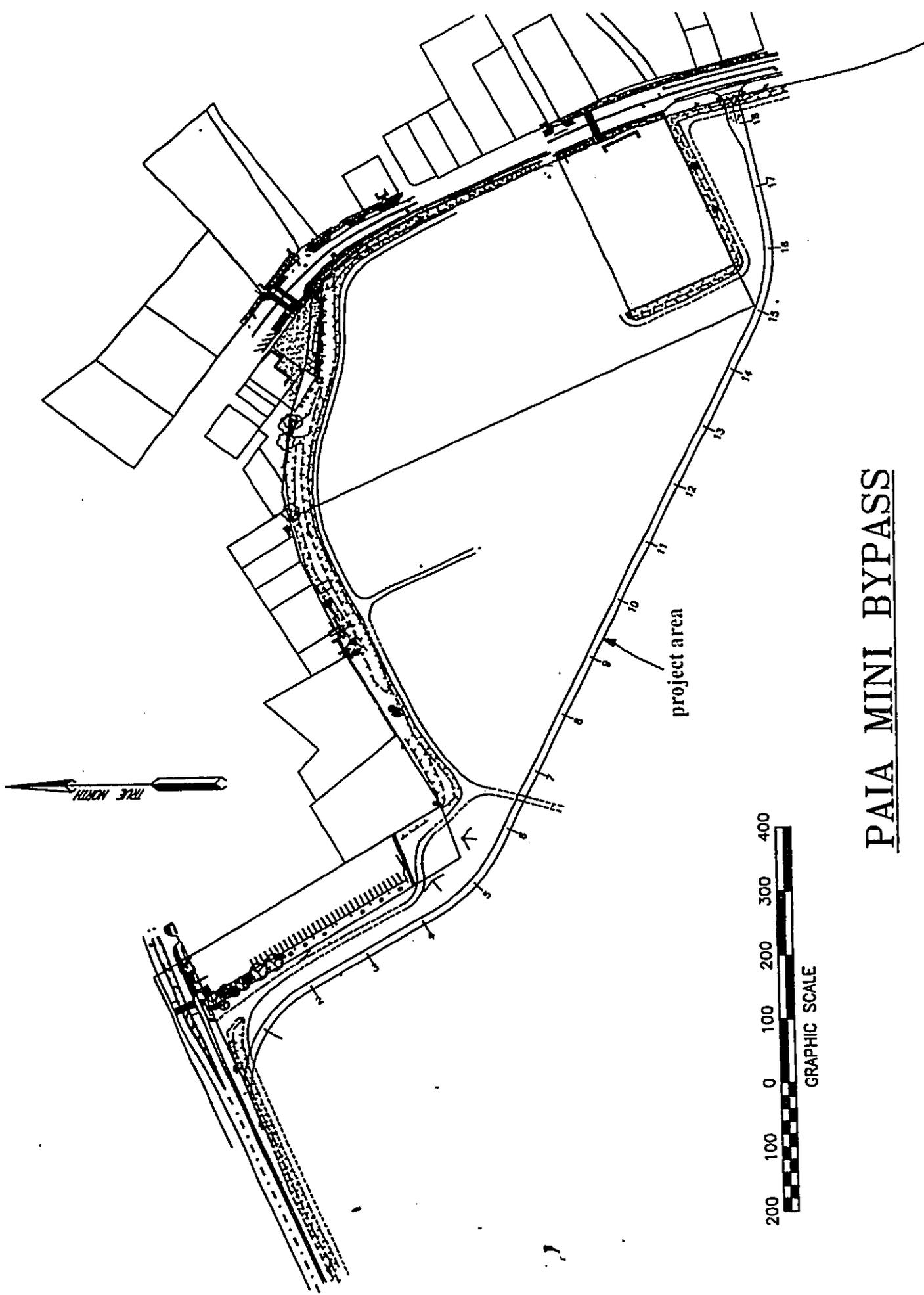
Scale 1 : 24,000  
1" = 2,000.04 ft





# PAIA MINI BYPASS

Map 2—Topographic map with study corridor location.



**PAIA MINI BYPASS**

Map 3—State Tax Map, including the project area, TMK: 2-5-05: Portion of 18.

## INTRODUCTION

Mr. Nolly Yagin of the County of Maui (COM) Department of Public Works and Environmental Management (DPWEM) contacted Xamanek Researches during the July of 2004 about an archaeological survey for the planned Pa'ia Mini Bypass in Pa'ia, Maui (TMK: 2-5-05: Portion of 18). The proposed project was to include the installation of a paved roadway over a c. 1820-foot (550-meter) long by 50-foot (15-meter) wide corridor that would cross former sugarcane land. Once constructed, this roadway would provide a one-way bypass of Pa'ia Town. This new roadway would begin just south of the COM Municipal Parking lot and extend *mauka* (northeast) to meet Baldwin Avenue adjacent to and east of the Pa'ia Post Office. Mr. Yagin requested that we submit a proposal per the State Historic Preservation Division (SHPD) requirements for this project.<sup>1</sup>

Following consultation with the SHPD, we prepared a proposal for the necessary scope of work for this project. We submitted our proposal to the DPWEM and were subsequently contracted to undertake the necessary inventory/assessment survey.

The project area lies in Hamakuapoko *ahupua'a*, Makawao District, Maui (Maps 1, 2 and 3). The field portion of this inventory survey was carried out in mid-November and early December of 2004. The following report presents the results of this project. This report is classed as an archaeological assessment under the new SHPD rules.

## STUDY AREA

As previously noted, the c. ½ km long by 15 meter wide corridor lies in Hamakuapoko *ahupua'a*, Makawao District, along the slopes of Haleakala (TMK: 2-5-05: Portion of 18). The entire project area lies east (*mauka*) of Hana Highway, and all of it was formerly used for commercial agricultural production. Sugarcane has been grown for over a century and continues to be cultivated beyond the DPWEM project area.<sup>2</sup> The

<sup>1</sup> Per discussions with Dr. Melissa Kirkendall, SHPD Maui staff archaeologist, archaeological inventory/assessment survey would be necessary.

<sup>2</sup> Sugarcane field waste, irrigation lines, and remnant cane plants were present on much of the central study area. Relatively mature sugarcane was present on the lower and upper sections of the corridor.

project area has been impacted by previous field clearing, planting and harvesting activities.

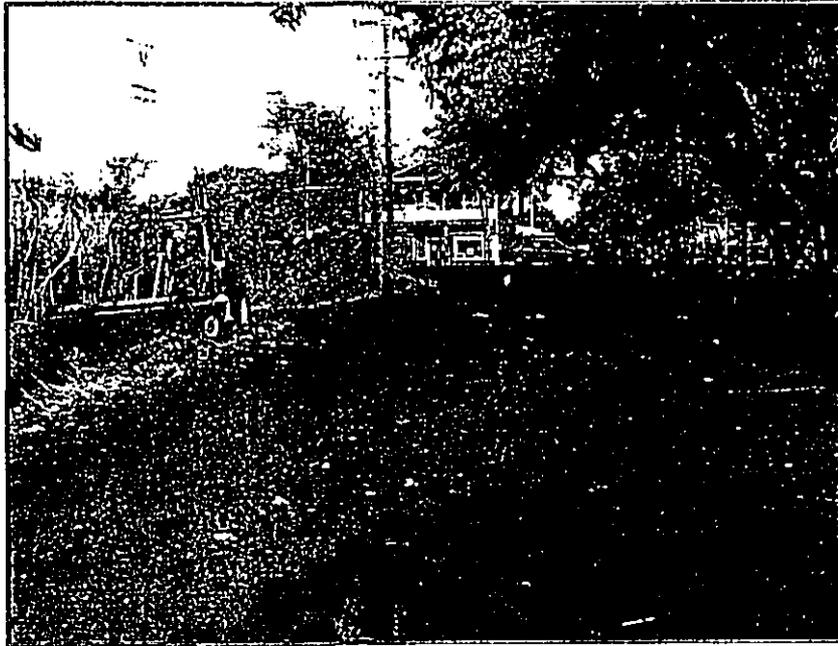
Hana Highway borders a portion of the study area to the west, while Hawaiian and Commercial Sugar, Ltd land lies to the north, east, and south. An existing County of Maui parking lot borders a portion of the project area, which also borders a recently constructed MECO substation and power line right-of-way.

### **Natural History**

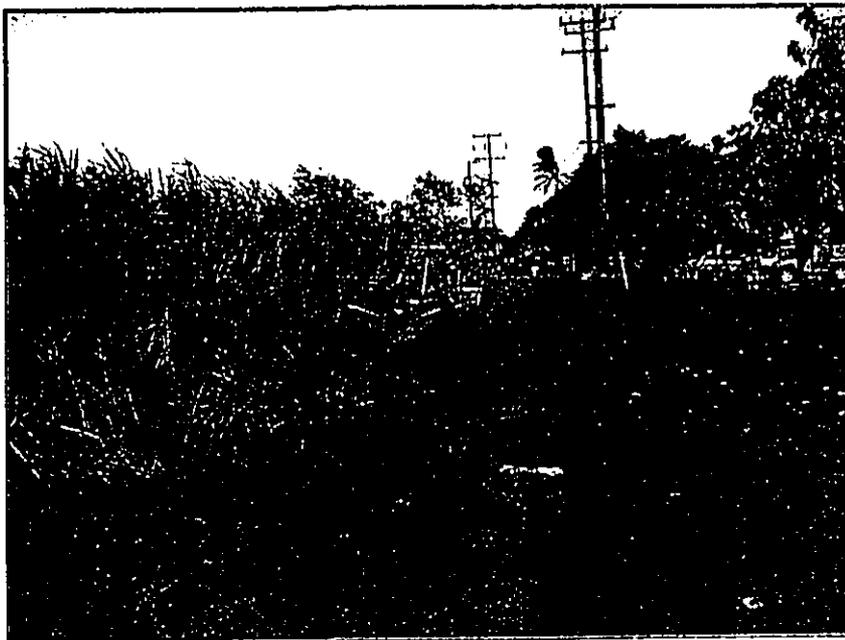
Soil type in the study area is identified as Pulehu silt loam (PpA) which is similar to Pulehu clay loam but with fewer stones. It occurs on nearly flat land, and is ideal for sugarcane production (Foote, et al., p.116).

The gently sloping project area ranges from a low of about 5 feet AMSL near Hana Highway to a high of c. 80 feet AMSL near Baldwin Avenue. Estimated annual precipitation on this part of Maui is about 30-40 inches, and the rains generally occur during the winter months. The average temperature ranges from the mid-seventies to the mid-eighties, and is relatively constant throughout much of the year.

Vegetation noted in the project area at the time of our survey was dominated by non-native plant species. Dominant vegetation present on the parcel consisted of *koa haole* shrubs (*Leucaena leucocephala*), various non-native grasses and succulent weeds, as well as abandoned sugarcane plants (*Saccharum officinarum*). In addition, some mango (*Mangifera indica*) trees and other landscaping plants were noted in the nearby residential areas. There were no native plant species observed during the pedestrian inspection, other than a few isolated *'ilima* (*Sida fallax*) shrubs.



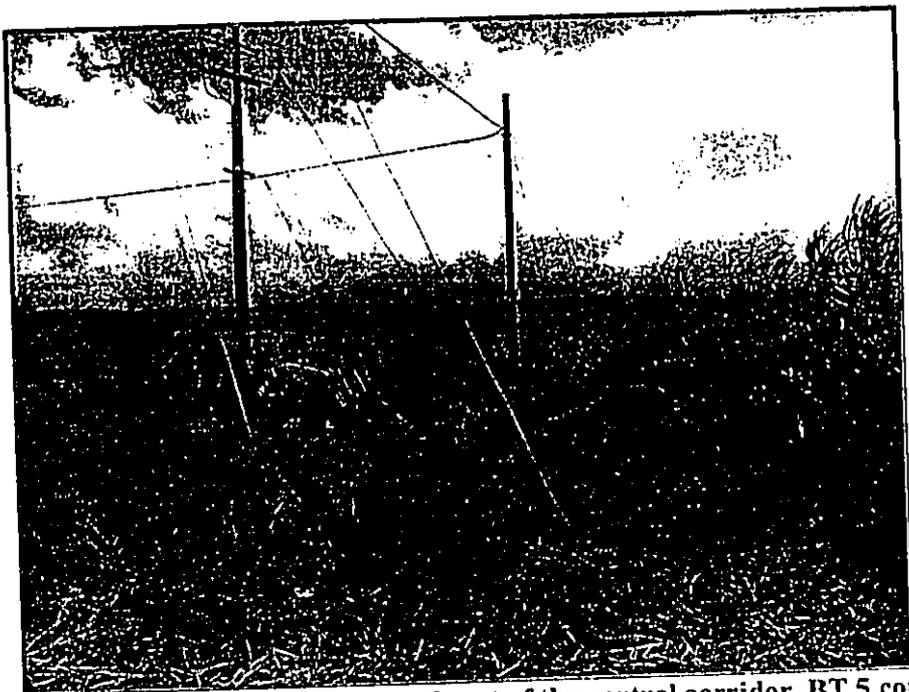
Photograph 1 - View to the southwest, clearing for BT 1 in process.  
Note Hana Highway in background.



Photograph 2 - View to the southwest, BT 4 excavation in progress.



Photograph 3 – View to the northeast, newly constructed MECO substation is at left.



Photograph 4 – View to the southeast of the central corridor, BT 5 completed.



Photograph 5 - View to the southwest across abandoned cane field.



Photograph 6 - View to the northwest, prior to excavation of BT 11.  
Baldwin Avenue is at the right, Pa'ia Post Office is in  
background left of photograph.

## Background Information

Hamakuapoko is noted as a place in which there were numerous landing sites, and a place where many battles were fought in precontact times. Sandy beaches, coral reefs, and sand dunes characterize the western coastline area in Pa'ia. It is mentioned in Hawaiian legends as a place where *ali'i* came to surf and enjoy water activities in peaceful times.

The study area is inland of the coast, on the *mauka* side of Hana Highway (Photograph 1).<sup>3</sup> The tax map on which the study parcel is located is (2) 2-5-05. It is part of Parcel 18, which is a large portion of land. It is located on what was field #212 of Hawaii Commercial Sugar Company up until its development as part of the Pa'ia-Haiku Community Plan (County of Maui, 1995). The lands to the west and south are still designated as agricultural in the 1995 community plan.

In 1853, the Privy Council to the Board of Education resolved "*that in accordance with Section first of the late School Act to provide for the better support and greater efficiency of the Public Schools, the following lands be and are hereby appropriated for the general purpose of Education, to be disposed of as provided in said Act. On the island of Maui, HAMAKUAPOKO, (Whole, with other lands)*" [from a copy of Haiku Sugar Company abstract, A & B Properties archives]. In January 1860, this land was sold by the Board of Education to the Trustees of Oahu College (later known as Punahou) for the sum of \$1.00. The land area was c. 5628 acres. The Trustees of Oahu College sold the land to Haiku Sugar Company in May of 1861 for \$5750. Castle<sup>4</sup> and Cooke<sup>5</sup> invested in a steam-powered mill for Haiku Sugar Company, which had been established in 1858, and the first crop of 260 tons was harvested in 1862.

In 1878 Alexander and Baldwin acquired another sugar company—Pa'ia Plantation—which, along with the Haiku Sugar Company, was incorporated into the A & B partnership in 1879. As time passed, it became unprofitable to run two mills, and a new one was built in Pa'ia in 1905, and was leased to Maui Agricultural Company. This company was incorporated into Hawaiian Commercial and Sugar Company in 1948, as post-war modernization began.

Two railroad spur lines are located near the study property. One led to Well #16, where water for the locomotives and the animals and field workers was taken on.

<sup>3</sup> It is estimated that the Hana Highway side of the project area lies within 80 meters of the shoreline.

<sup>4</sup> Samuel N. Castle was treasurer of Oahu College, and authorized the sale to Haiku Sugar Company.

<sup>5</sup> Amos Cooke had purchased stock in the Haiku Sugar Company in 1858, a company that intended to develop a plantation and mill at Haiku. Richard Armstrong had conceived this notion. Both were missionaries, and took advantage of generous land offerings from Kamehameha III.

Another was the line that ran from the lime plant in lower Pa`ia to the mill in upper Pa`ia. Concerning the railroad, Mr. Henry F. Bonnell wrote (Best, 1977, p. 233):

*"...the Maui Agricultural Company, of Pa`ia, Maui, which maintained one of the best equipped railroad shops in the Islands. The rolling stock on this plantation consisted of 950 cane cars, several box cars, flat cars, portable track cars, water and molasses tank cars, as well as cars for hauling mudpress, from the mill to the fields. The repairs for the five steam locomotives were accomplished in the plantation machine shop.*

*The plantation railway was used for many purposes other than hauling cane. It transported refined sugar from the mill to the plantation store and staple goods, such as rice from the store to the branch camps. Hydrated lime, used in processing raw sugar, was hauled from the kiln, at the beach in lower Pa`ia, along with sand for the locomotives."*

At the time the new mill was built (1905-06), an agreement was made with the Kahului Railroad to extend its main line tracks to a point adjacent to the new mill, thereby allowing the raw sugar to be transported more economically to Kahului Harbor for exportation (Dean, pp. 99-100).

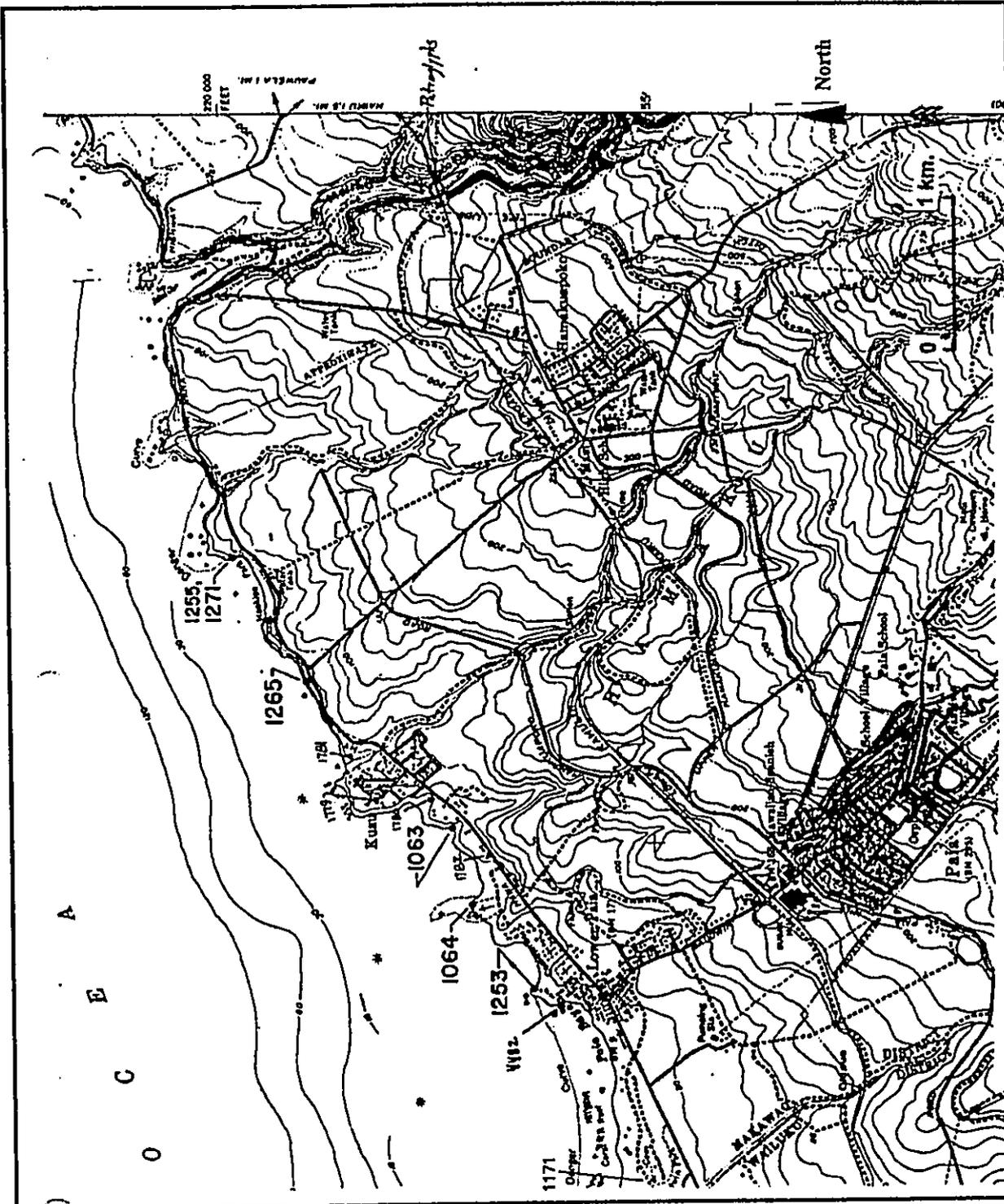


Figure 1 – SHPD map (Pa'ia Quadrangle) showing locations of sites along Hamakuapoko Coastline.

**Table 1**  
**Coastal sites in Hamakuapoko area in vicinity of project area**

Site #	Site type	Remarks
1063	Petroglyphs	Hamakuapoko petroglyphs
1064	Burials	Kalahau Burial Complex
1174	Burials	Baldwin beach burials
1253	Habitation and burial	Pa'ia House and Grave complex. Well-preserved house outline, with rectangular pit. Surface artifacts include grindstone, basalt flakes. Food midden is also present around periphery.
1255	Platform	Ho'okipa platform. Badly eroded terraces (2) without surface artifacts or midden evident.
1271	Burial	Ho'okipa burial. Outline of burial pit in parking area.
1265	Burials	Hamakuapoko Burial Complex
1779-1782	Burials and habitation	Located during monitoring of the Spreckelsville Sewer line project
4482	Historic	World War II Pill Box in Pa'ia

### Previous archaeological work

Previous archaeological work that has been done in the general area has been recorded on the Pa'ia Quadrangle by the SHPD (Figure 1). These sites are listed in Table 1 above. Some sites were identified in connection with the sewer line construction in the 1980s (Clark and Toenjes, 1987) while others were noted during the 1973 statewide inventory of historic sites. In 1988, Xamanek Researches surveyed the c. 14-acre surrounding area for Alexander and Baldwin Properties Group, with no significant findings. The railroad berm, running to the mill was identified along the edge of that surveyed property. Xamanek Researches carried out an archaeological assessment survey for the MECO substation that lies adjacent to and north of a portion of the Pa'ia Mini Bypass project area in 2003. While there were no significant material culture remains encountered during the course of this previous work, beach sand deposits were located during auger coring for four utility poles in the vicinity of Hana Highway (Fredericksen and Fredericksen, 2003)

### Expected Findings

This coastal area, because of its location and precontact importance, is an area with a relatively high potential for cultural deposits, and/or human burials. Significant findings have been documented in this largely disturbed portion of Maui in the past. However, given that the study corridor lies in an area that has been impacted by sugarcane cultivation for well over a century, we did not anticipate the presence of intact cultural deposits. Rather, it was felt that the area could potentially contain remnants of significant subsurface material culture remains, especially in the vicinity of Hana

Highway. In addition, based on previous work carried out in close proximity to the current study area, we expected to encounter intact sand deposits, which could potentially contain human burials.

## ARCHAEOLOGICAL METHODS

This archaeological survey was conducted during November and December of 2004. Erik Fredericksen conducted both the surface walkover and the subsurface testing program. Erik Fredericksen was also the project director. Walter Fredericksen was the senior advisor.

The survey of the study area was conducted in 2 phases—a pedestrian surface inspection, followed by subsurface investigation. The walkover portion of the survey was undertaken using 5-meter wide sweeps. The second portion of the survey utilized a series of 11 backhoe trenches to sample subsurface conditions in the c. 1800-foot (550 meter) long corridor.

As previously noted above, a total of 11 backhoe trenches were utilized to sample the study area (Figure 2). Backhoe trench profiles were recorded, and maps were compiled using metric survey tapes and hand-bearing compasses. Backdirt was visually inspected, and spot-checked with 1/8<sup>th</sup> inch screen. Written notes were kept in the field, and photographs were recorded in a digital format. No material culture remains were transported off-island and standard laboratory procedures and methods were utilized.

## ARCHAEOLOGICAL RESULTS

As previously noted in this report, a total of 11 backhoe trenches were utilized to sample the project area. Four of the trenches tested the area near Hana Highway, and the remaining trenches sampled the rest of the proposed roadway corridor. There were no subsurface cultural layers encountered during the course of our survey. However, intact sand deposits were located in BTs 1-4. Our test results are presented below.

### **Backhoe Trenches 1-4**

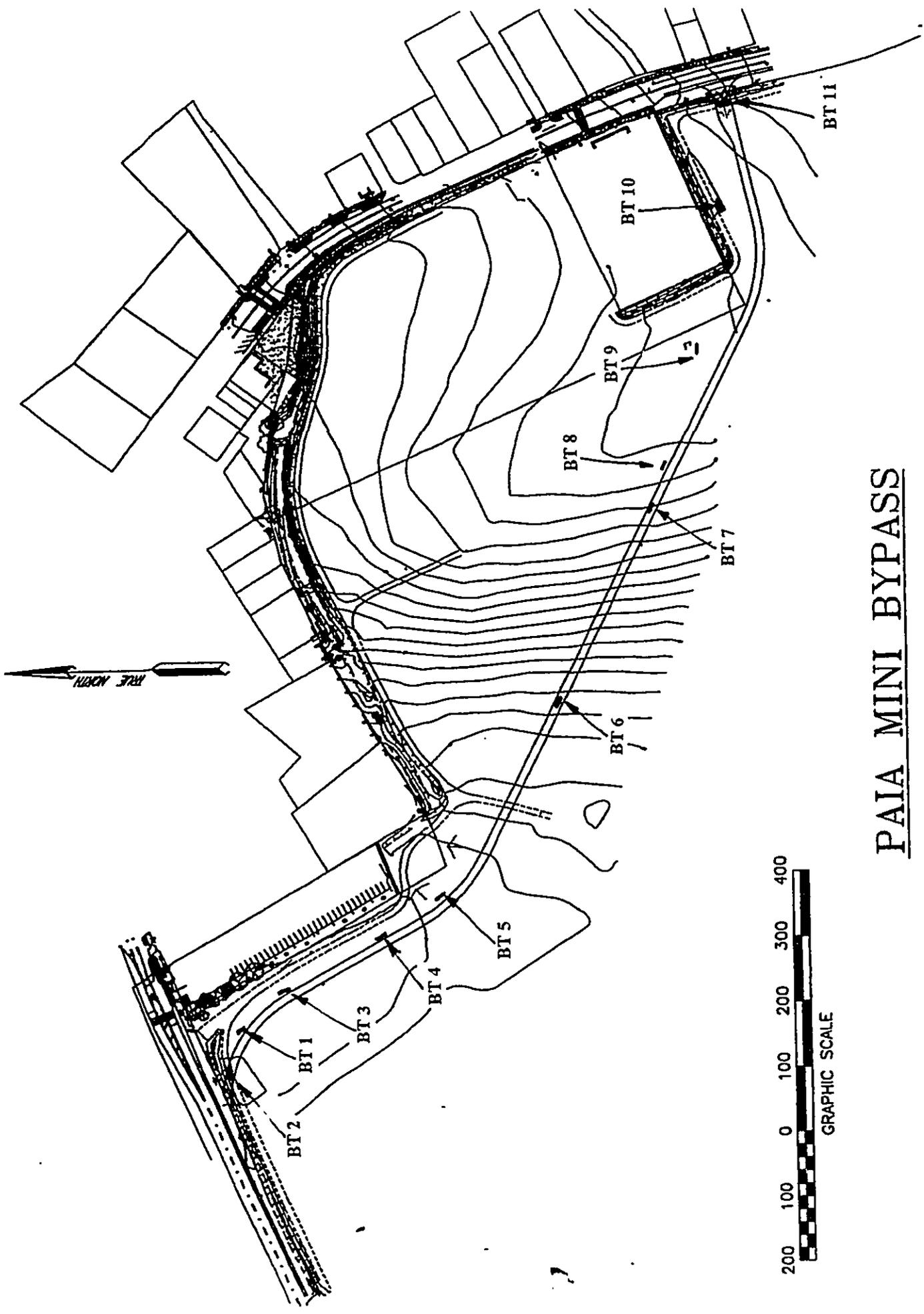
These four backhoe trenches were located near the southwestern boundary of the proposed Pa'ia Mini Bypass corridor. Trench orientations were roughly 70 degrees magnetic and the trenches were c. 5.5 meters in length by up to 1.3 meters in depth. The groundwater table was encountered between 120 and 140 cmbs in these four trenches.

### **Backhoe Trench 1 (Figure 3, Photograph 7)**

This first trench was placed near the southwestern boundary of the of the project area within 25 meters of the paved shoulder of Hana Highway. Backhoe Trench 1 was oriented 310 degrees magnetic and was a maximum of 1.6 meters deep. Two subsurface strata were encountered before excavation was halted because the ground water table was encountered.

Layer I (0-80 cmbs) was composed of brown (7.5 YR 4/3) silty loam. This relatively friable layer is interpreted as the plow zone. Several lengths of black plastic irrigation tubing and pieces of black plastic mulch sheeting were noted in the profile and backdirt pile. In addition, a few pieces of rusted metal, plastic, paper, and several brown bottle glass fragments were observed.

Layer II (c. 80-160 cmbs) consisted of brown (10 YR 4/4) to very pale brown (10 YR 7/4) sand that contained several semi-rounded coral cobbles. This intact beach sand stratum did not yield any significant material culture remains in this test location. The ground water table was located @ 1.4 mbs and excavation was halted at a maximum depth of 1.6 mbs due to unstable subsurface conditions.



**PAIA MINI BYPASS**

Figure 2— Location of the project area and BTs 1-11.

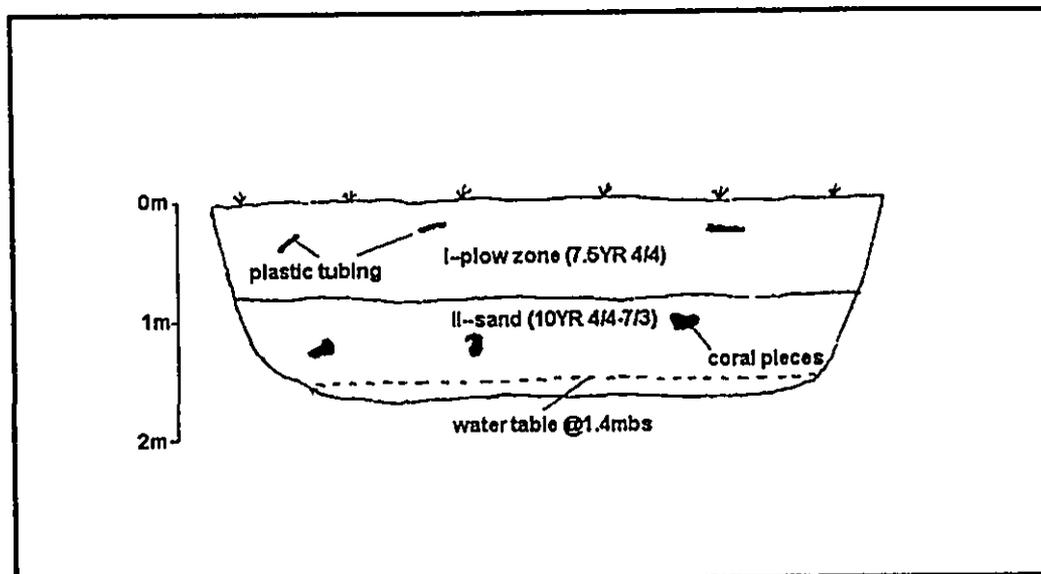


Figure 3 – Northeast profile of BT 1.

#### Backhoe Trench 2 (Figure 4, Photograph 8)

This second trench was placed near the southwestern boundary of the of the project area within 15 meters of the paved shoulder of Hana Highway. This trench was oriented 60 degrees magnetic and was a maximum of 1.4 meters in depth. Two layers were located before excavation was halted just below the ground water table.

Layer I (0-70 cmbs) was composed of brown (7.5 YR 4/3) silty loam, the bottom c. 25 cm of which was mixed with brown (10 YR 4/4) sand. Several lengths of black plastic irrigation tubing and pieces of black plastic mulch sheeting were noted in the profiles of the trench and in the backdirt pile. Additionally, a few pieces of rusted metal, plastic and paper were observed.

Layer II (c. 70-140 cmbs) consisted of brown (10 YR 4/4) to very pale brown (10 YR 7/4) sand that again contained several semi-rounded coral cobbles. This beach sand stratum did not yield any significant material culture remains in this test location. The ground water table was located @ 1.2 mbs and excavation was halted at a maximum depth of 1.4 mbs due to unstable subsurface conditions.

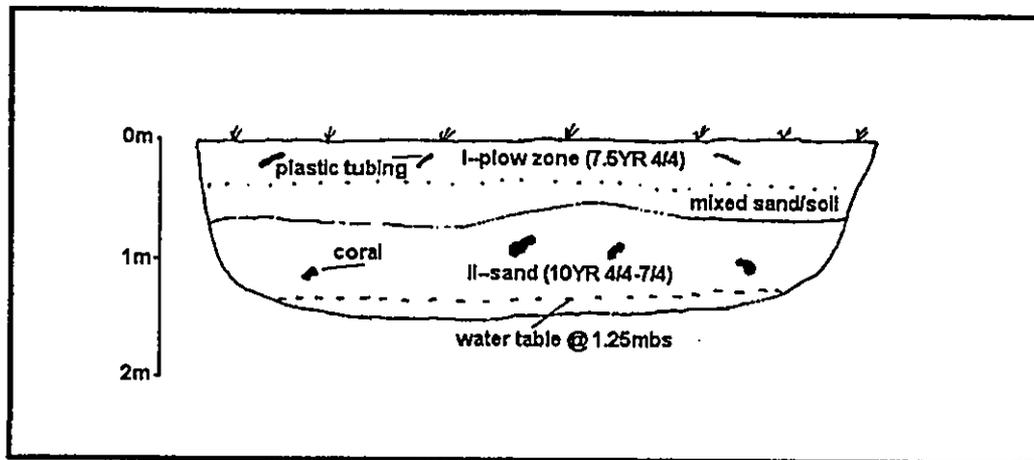


Figure 4 – Northwest face of BT 2.

### Backhoe Trench 3 (Figure 5, Photograph 9)

This third trench was placed near the southwestern boundary of the of the project area within c. 45 meters of the paved shoulder of Hana Highway. This trench was oriented 320 degrees magnetic and was a maximum of 1.6 meters in depth. The two common layers were located before excavation was halted just below the ground water table.

Layer I (0-80 cmbs) was composed of brown (7.5 YR 4/3) silty loam. Again, several lengths of black plastic irrigation tubing and pieces of black plastic mulch sheeting were noted in the profiles of this trench and in the backdirt pile. A few pieces of rusted metal, plastic, modern bottle glass and paper were observed.

Layer II (c. 80-160 cmbs) consisted of brown (10 YR 4/4) to very pale brown (10 YR 7/4) sand that contained several semi-rounded cobbles of coral. There were no significant material culture remains noted in this beach sand stratum. The ground water table was located @ 1.4 mbs and excavation was halted at a maximum depth of 1.65 mbs.

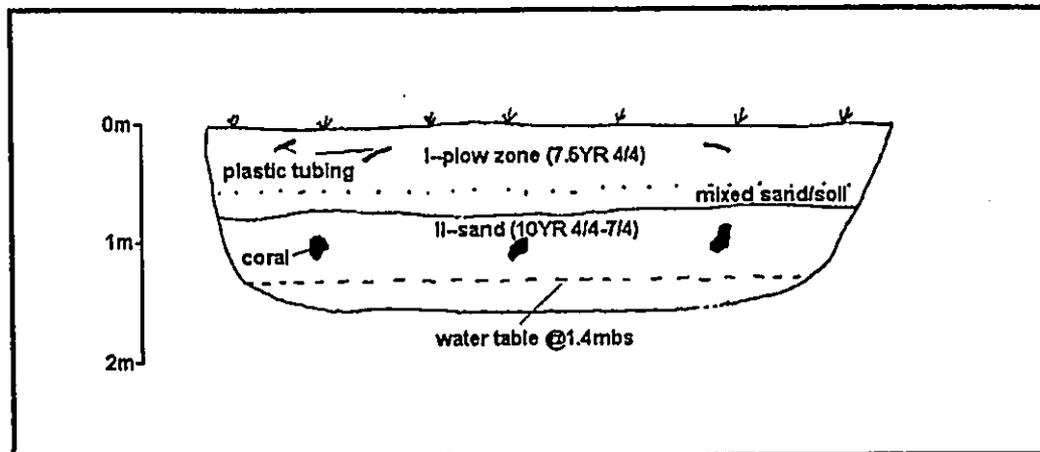


Figure 5 – Northeast profile of BT 3.

#### Backhoe Trench 4 (Figure 6, Photograph 10)

This fourth trench was located 85 meters *mauka* of the paved shoulder of Hana Highway in the southwestern portion of the study area. This trench was oriented 320 degrees magnetic and was a maximum of 2.0 meters in depth. The two common layers were encountered before excavation was halted just below the water table.

Layer I (0-110 cmbs) was composed of brown (7.5 YR 4/3) silty loam. There were several lengths of black plastic irrigation tubing and pieces of black plastic mulch sheeting present in the profiles of this trench and in the backdirt pile. A few pieces of rusted metal, plastic, modern bottle glass and paper were also observed.

Layer II (c. 110-210 cmbs) was made up of brown (10 YR 4/4) to very pale brown (10 YR 7/4) sand that contained scattered semi-rounded cobbles of coral. There were no significant material culture remains observed in this beach sand stratum. The water table was located @ 1.8 mbs and excavation was halted at a maximum depth of 2.1 mbs.

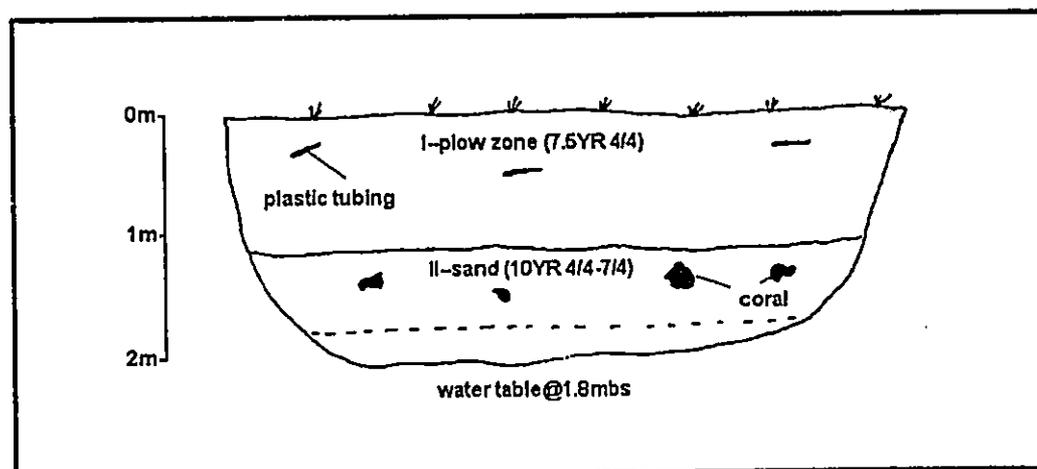


Figure 6 – Northeast profile of BT 4.

## Backhoe Trenches 5-11

This series of trenches explored the *mauka* (northeastern) portion of the project corridor from STA 4 + 50 to the terminus at STA 18 + 20. There were no sand deposits located in these trenches, and only BT 5 encountered the water table. These backhoe trenches ranged from 2.1 to 3.3 meters in depth.

### Backhoe Trench 5 (Figure 7, Photograph 11)

This fifth trench was located in the vicinity of the newly constructed MECO substation, c. 40 meters *mauka* of BT 4. This trench was oriented 330 degrees magnetic and was a maximum of 2.5 meters in depth. Two layers were encountered before excavation was halted below the water table.

Layer I (0-100 cmbs) consisted of dark brown (7.5 YR 3/4) clay loam. There were again a few lengths of black plastic irrigation tubing and pieces of black plastic mulch sheeting present in the profiles of this trench and in the backdirt pile. A few pieces of rusted metal and plastic were also observed. In addition, one coral cobble was located in the profile of the trench.

Layer II (100-250 cmbs) was composed of reddish brown (5 YR 4/3) silty clay with weathered parent material near the bottom portion of the trench. This sterile stratum was relatively compact and did not appear to contain any significant material cultural remains. The water table was located @ 2.2 mbs and excavation was abandoned at a maximum depth of 2.5 mbs.

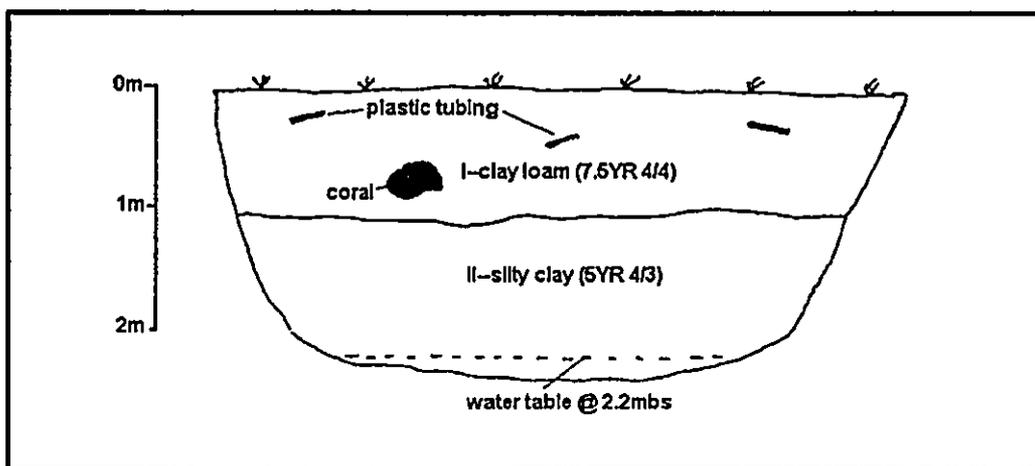


Figure 7 - Northwestern profile of BT 5.

### Backhoe Trench 6 (Figure 8, Photograph 12)

This sixth trench was located near STA 8 + 60, *mauka* of the newly constructed MECO substation. This trench was oriented 280 degrees magnetic and was a maximum of 2.3 meters in depth. Two layers were encountered before excavation was halted below the water table.

Layer I (0-60 cmbs) consisted of dark brown (7.5 YR 3/4) clay loam. This relatively friable stratum contained a few lengths of black plastic irrigation tubing as well as pieces of black plastic mulch sheeting. A few pieces of rusted metal and plastic were also noted.

Layer II (60-230 cmbs) was composed of reddish brown (5 YR 4/3) silty clay with weathered bedrock near the bottom portion of the trench. This sterile layer was relatively compact and did not appear to contain any significant material cultural remains. The water table was not located in this subsurface test, and excavation was halted at a maximum depth of 2.3 mbs.

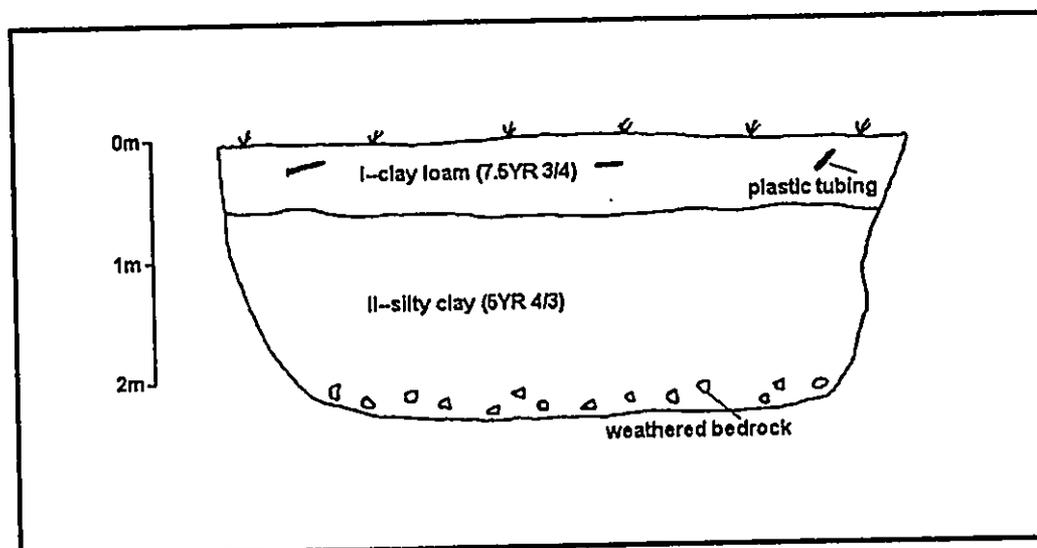


Figure 8 - North face profile of BT 6.

### Backhoe Trench 7 (Figure 9, Photograph 13)

This backhoe trench was located 70 meters *mauka* of BT 6. Trench orientation was 280 degrees magnetic, and BT 7 was a maximum of 1.4 meters in depth. Three strata were encountered before excavation was halted at weathered bedrock.

Layer I (0-50 cmbs) was composed of dark brown (7.5 YR 3/4) clay loam. This relatively friable layer contained a few lengths of black plastic irrigation tubing as well as scattered pieces of black plastic mulch sheeting. A few pieces of rusted metal, and PVC pipe were also noted.

Layer II (50-120 cmbs) was made up of reddish brown (5 YR 4/3) silty clay with scattered pieces of weathered bedrock. This sterile layer was relatively compact and did not appear to contain any significant material cultural remains.

Layer III (80-140 cmbs) consisted of strong brown (7.5 YR 4/6) silty clay and weathered bedrock. This stratum was sterile and excavation was halted at consolidated, weathered bedrock.

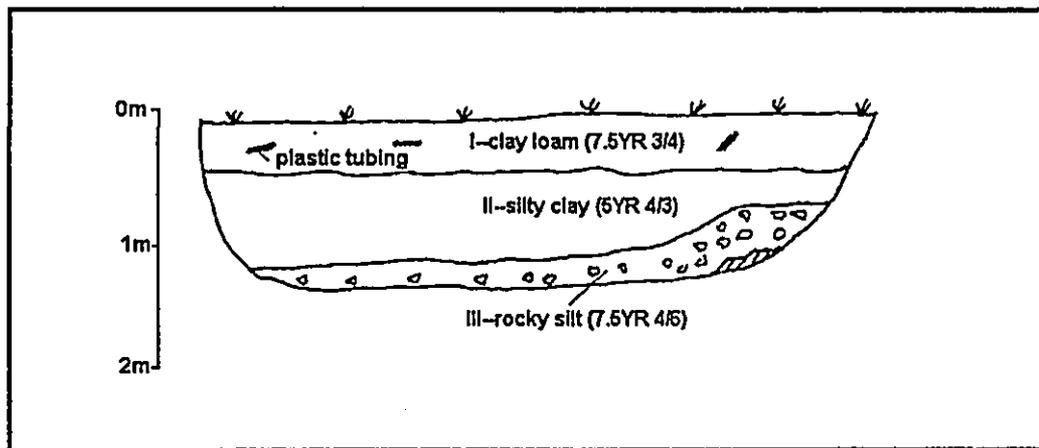


Figure 9 – North face of BT 7.

### Backhoe Trench 8 (Figure 10, Photograph 14)

Backhoe Trench 8 was located an estimated 10 meters to the north of the proposed bypass corridor in an abandoned portion of a sugarcane field. Trench orientation was 300 degrees magnetic, and BT 8 was a maximum of 1.7 meters deep. Three layers were located before excavation was abandoned at weathered bedrock.

Layer I (0-50 cmbs) was made up of dark brown (7.5 YR 3/4) clay loam. This relatively friable layer contained a few lengths of black plastic irrigation tubing along with scattered pieces of black plastic mulch sheeting. A few pieces of rusted metal, some plastic, and PVC pipe were also present.

Layer II (50-100 cmbs) was composed of reddish brown (5 YR 4/3) silty clay with scattered pieces of weathered bedrock. This sterile stratum was compact and did not appear to contain any significant cultural materials.

Layer III (100-170 cmbs) consisted of strong brown (7.5 YR 4/6) silty clay and weathered bedrock. This layer was sterile and excavation was halted at consolidated, weathered bedrock.

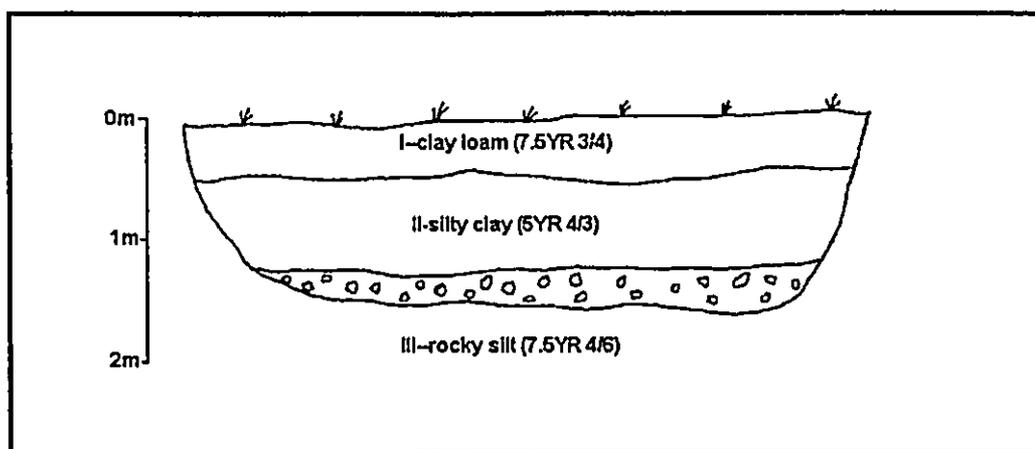


Figure 10 – North face profile of BT 8.

### Backhoe Trench 9 (Figure 11, Photograph 15)

Backhoe Trench 9 was placed an estimated 15 meters north of the proposed bypass corridor in an abandoned portion of a sugarcane field. Trench orientation was 300 degrees magnetic, and BT 9 was up to 2.1 meters in depth. Three strata were located before excavation was abandoned at weathered bedrock.

Layer I (0-80 cmbs) consisted of dark brown (7.5 YR 3/4) clay loam. This relatively friable layer contained a few lengths of black plastic irrigation tubing along with scattered pieces of black plastic mulch sheeting. A few pieces of PVC pipe were also present.

Layer II (80-180 cmbs) was composed of reddish brown (5 YR 4/3) silty clay and scattered pieces of weathered bedrock. This sterile stratum was compact and did not contain any significant cultural materials.

Layer III (180-210 cmbs) was made up of strong brown (7.5 YR 4/6) silty clay and weathered bedrock. This layer was sterile and excavation was halted at consolidated, weathered bedrock.

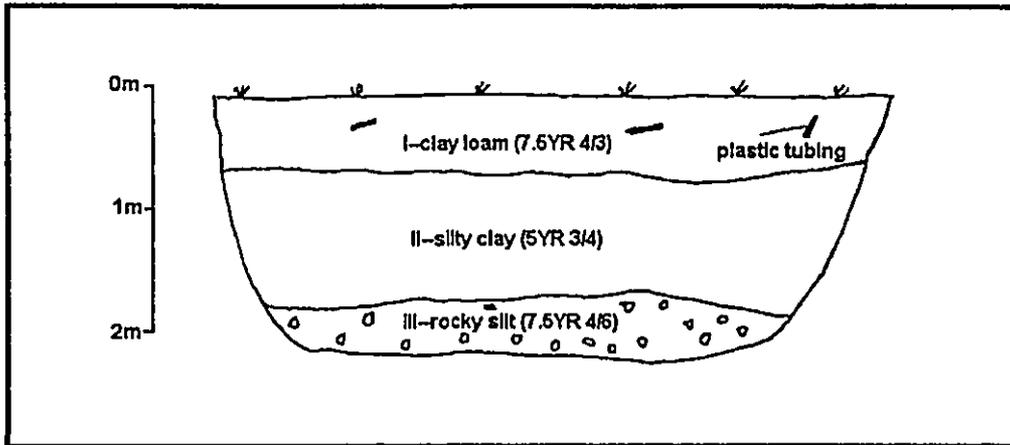


Figure 11 – South face profile of BT 9.

### Backhoe Trench 10 (Figure 12, Photograph 16)

Backhoe Trench 10 was located an estimated 12 meters north of the proposed bypass corridor and adjacent to a sugarcane field access road. Trench orientation was 45 degrees magnetic, and BT 10 was up to 2.3 meters deep. Three layers were encountered before excavation was abandoned.

Layer I (0-45 cmbs) was composed of dark brown (7.5 YR 3/4) clay loam. This friable layer contained a few lengths of black plastic irrigation tubing along with scattered pieces of black plastic mulch sheeting. A few pieces of broken PVC pipe were also present.

Layer II (45-210 cmbs) was made up of reddish brown (5 YR 4/3) silty clay along with scattered pieces of weathered bedrock. This sterile layer was compact and did not contain any significant cultural materials.

Layer III (210-230 cmbs) consisted of strong brown (7.5 YR 4/6) rocky silt. This layer was sterile and excavation was abandoned at 230 cmbs.

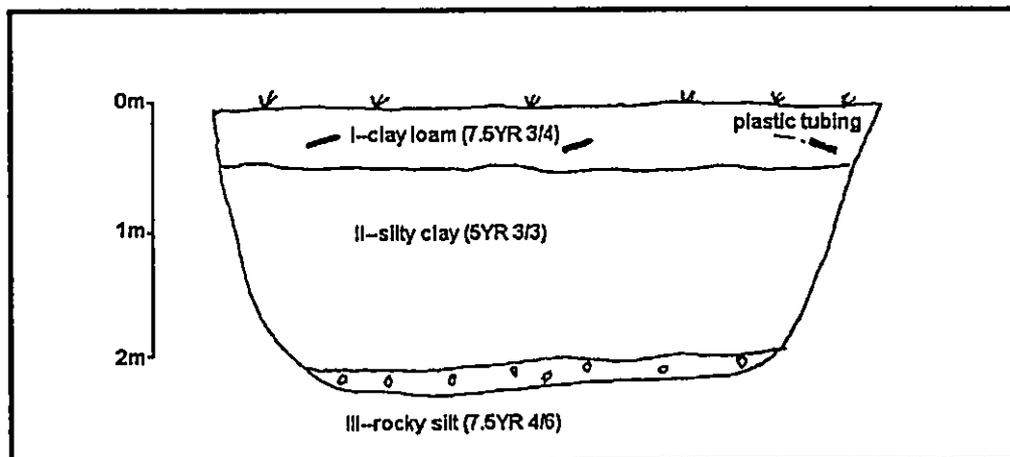


Figure 12 – Southeast face profile of BT 10.

### Backhoe Trench 11 (Figure 13, Photograph 17)

Backhoe Trench 11 was located near the southeastern terminus of the proposed bypass corridor, near Baldwin Avenue. Trench orientation was 340 degrees magnetic, and BT 11 was up to 2.4 meters in depth. Three strata were located before excavation was abandoned at weathered bedrock.

Layer I (0-40 cmbs) was composed of brown (7.5 YR 3/4) clay loam. This relatively friable layer contained a few lengths of black plastic irrigation tubing and scattered pieces of black plastic mulch sheeting. A few pieces of PVC pipe, recent refuse and some lumber were noted in the general area.

Layer II (40-220 cmbs) consisted of reddish brown (5 YR 4/3) silty clay and scattered pieces of weathered bedrock. This sterile stratum was compact and did not contain any significant cultural materials.

Layer III (220-240 cmbs) was made up of strong brown (7.5 YR 4/6) rocky silt and weathered bedrock. This layer was sterile and excavation was abandoned at consolidated, weathered bedrock.

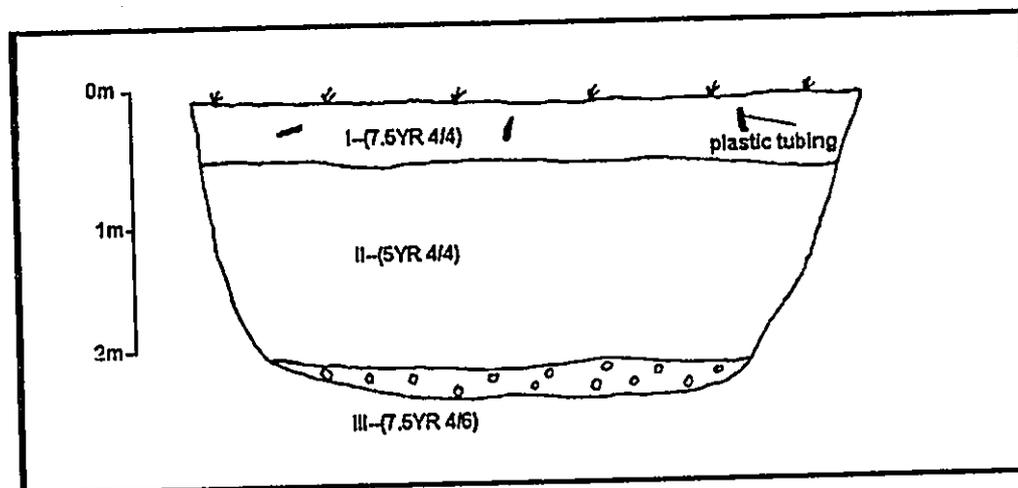


Figure 13 - East face profile of BT 11.

### Discussion of backhoe trench results

The eleven backhoe trenches that were excavated in the project corridor did not yield any evidence of significant material culture remains. However, BTs 1-4 did reveal that intact beach sand deposits underlie the proposed bypass corridor. The ground water table was located in all of the above trenches as well as BT 5.

**TABLE 3**  
**Summary of Backhoe Trench Results**  
**Pa'ia Mini Bypass Project**

BT#	Dimensions (L x W)	Soil Profile	cmbs	Remarks
1	5.0 x 1.6	Layer I: brown (7.5YR 4/3) clay loam—plow zone Layer II: brown (10 YR 4/4) to very pale brown (10 YR 7/4) sand Water table @ 140 cmbs	0 - 80 80 - 160	L I: plow zone with irrigation tubing, plastic, metal LII: undisturbed, sterile, contains coral chunks and blocks
2	5.0 x 1.4	Layer I: brown (7.5YR 4/3) clay loam—plow zone Layer II: brown (10 YR 4/4) to very pale brown (10 YR 7/4) sand Water table @ 120 cmbs	0 - 70 70 - 140	L I: plow zone with irrigation tubing, plastic, metal LII: undisturbed, sterile, contains coral chunks and blocks
3	5.0 x 1.7	Layer I: brown (7.5YR 4/3) clay loam—plow zone Layer II: brown (10 YR 4/4) to very pale brown (10 YR 7/4) sand Water table @ 140 cmbs	0 - 80 80 - 170	L I: plow zone with irrigation tubing, plastic, metal LII: undisturbed, sterile, contains coral chunks and blocks
4	5.0 x 2.0	Layer I: dark brown (7.5YR 3/3) clay loam—plow zone Layer II: brown (10 YR 4/4) to very pale brown (10 YR 7/4) sand Water table @ 180 cmbs	0 - 110 110 - 200	L I: plow zone with irrigation tubing, plastic, metal LII: undisturbed, sterile, contains coral chunks and blocks
5	5.0 x 2.5	Layer I: dark brown (7.5YR 3/4), clay loam—plow zone Layer II: reddish brown (5 YR 4/3) silty clay Water table @ 220 cmbs	0 - 100 100 - 250+	L I: plow zone with irrigation tubing, plastic, one coral cobble LII: sterile, some weathered bedrock
6	5.0 x 2.3	Layer I: dark brown (7.5YR 3/4), clay loam—plow zone Layer II: reddish brown (5 YR 4/3) silty clay Water table not located in BTs 6-11	0 - 60 60 - 230+	L I: plow zone with irrigation tubing, plastic LII: sterile, some weathered bedrock
7	5.0 x 1.4	Layer I: dark brown (7.5YR 3/4), clay loam—plow zone Layer II: reddish brown (5 YR 4/3) silty clay Layer III: strong brown (7.5 YR 5/8) silty clay with weathered bedrock	0 - 50 50 - 120 80 - 140+	L I: plow zone with irrigation tubing, plastic LII: undisturbed, sterile LIII: sterile, some weathered bedrock
8	5.0 x 1.7	Layer I: dark brown (7.5YR 3/4), clay loam—plow zone Layer II: reddish brown (5 YR 4/3) silty clay Layer III: strong brown (7.5 YR 5/8) silty clay with weathered bedrock	0 - 50 50 - 100 100 - 170+	L I: plow zone with irrigation tubing, PVC, plastic LII: undisturbed, sterile LIII: sterile, some weathered bedrock
9	5.0 x 2.1	Layer I: dark brown (7.5YR 3/4), clay loam—plow zone Layer II: reddish brown (5 YR 4/3) silty clay Layer III: strong brown (7.5 YR 5/8) silty clay with weathered bedrock	0 - 80 80 - 180 180 - 210+	L I: plow zone with irrigation tubing, PVC LII: undisturbed, sterile LIII: sterile, some weathered bedrock

<sup>6</sup> All lengths are in meters.

<sup>7</sup> All backhoe trenches were c. 70 cm. in width.

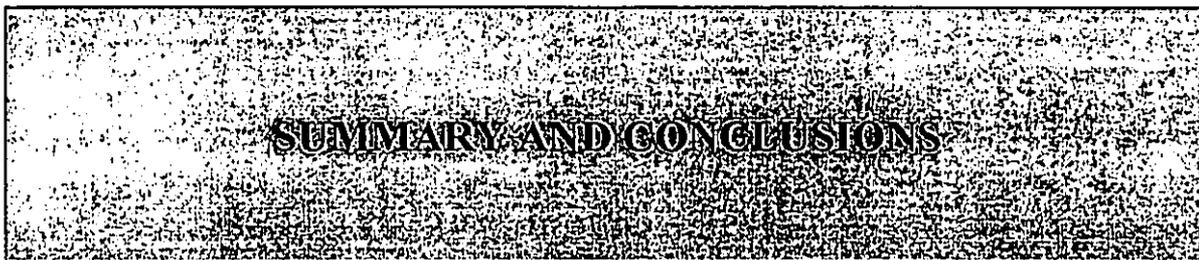
<sup>8</sup> cmbs = centimeters below surface

**Table 3 cont.**

10	5.0 x 2.3	Layer I: dark brown (7.5YR 3/4), clay loam— plow zone Layer II: reddish brown (5 YR 4/3) silty clay Layer III: strong brown (7.5 YR 5/8) silty clay with weathered bedrock	0 - 45 45 - 210 210 - 230+	L I: plow zone with irrigation tubing, PVC, plastic LII: undisturbed, sterile LIII: sterile, some weathered bedrock
11	5.0 x 2.4	Layer I: dark brown (7.5YR 3/4), clay loam— plow zone Layer II: reddish brown (5 YR 4/3) silty clay Layer III: strong brown (7.5 YR 5/8) silty clay with weathered bedrock	0 - 40 4 - 220 220 - 240+	L I: plow zone with irrigation tubing, refuse LII: undisturbed, sterile LIII: sterile, some weathered bedrock

### Discussion of Results

There was no evidence of an intact cultural layer encountered during subsurface testing excavation on any portion of the MECO substation project area. It appears that much of the study area has been heavily impacted by earth moving activities associated with commercial sugarcane cultivation. The plow zone in the vicinity of the substation appeared to be 50 cm to 70 cm in depth.



The pedestrian portion of the archaeological inventory/assessment survey did not locate any surface structural remains. Four of the 11 backhoe trenches revealed relatively shallow marine sand deposits as shallow as 70 cmbs (BT 2) to as deep as 120 cmbs (BT 4). The ground water table was encountered in BTs 1-5 between 120 cmbs and 220 cmbs. Subsurface results indicate that the upper portions of the sampled project area have been heavily impacted by land altering activities associated with commercial sugarcane cultivation. The plow zone varied in depth from 60-90 cm in sampled portions of the study area. As mentioned earlier in this report, there were no precontact or post-contact cultural layers found during our archaeological survey.

One plantation-era site lies in the general vicinity of the project area—a former railroad spur of the Pa'ia Sugar Mill railway system, which was located in our earlier 1988 survey. The lack of any intact or remnant cultural layer in sampled areas is not surprising, since post-contact commercial cultivation and associated field clearing activities have extensively altered both the surface and subsurface nature of the study

area. However, it is noteworthy that beach sand deposits were encountered adjacent to Hana Highway and extended just beyond STA 4 + 50.<sup>9</sup>

### Site Significance Evaluations

The following significance evaluations are based on the Rules Governing Procedures for Historic Preservation Review (DLNR 1996; Chapter 275). According to these rules, a site must possess integrity of location, design, setting, materials, workmanship, feeling and association and shall meet one or more of the following criteria:

**Criterion "a"**—Be associated with events that have made an important contribution to the broad patterns of our history;

**Criterion "b"**—Be associated with the lives of persons important in our past;

**Criterion "c"**—Embody the distinctive characteristics of a type, period, or method of construction; represent the work of a master; or possess high artistic value;

**Criterion "d"**—Have yielded, or is likely to yield, important information for research on prehistory or history;

**Criterion "e"**—Have an important traditional cultural value to the native Hawaiian people or to another ethnic group of the state due to associations with traditional cultural practices once carried out, or still carried out, at the property or due to associations with traditional beliefs, events or oral accounts.

Sites can be considered no longer significant when they qualify only under Criterion "d" and sufficient information has been collected from them during inventory/assessment survey level investigation. As previously noted, there were no significant material culture remains noted during this assessment level project.

### Mitigation Recommendations

While there were there were no significant material culture remains found during our survey, the presence of intact marine sand deposits between Hana Highway and STA 4 + 50 is of interest. Given the relative proximity to the coast, the presence of subsurface marine and dune sand remnants is not surprising. It is important to note that human

<sup>9</sup> It was not possible to test between BT 4 and BT 5 due to the presence of a power pole, anchor cables, mature sugarcane and a previously constructed vehicle barrier/ditch.

burials have been located in the general area, often associated with sand deposits. Consequently, precautionary monitoring is recommended during construction activities in the northwestern portion of the project area (i.e. Hana Highway to STA 4 + 50).

## REFERENCES

- Best, Gerald M.  
1978 Railroads of Hawaii: Narrow and Standard Gauge Common Carriers, Golden West Books, San Marino, CA.
- Clark, Stephan D., and James Toenjes  
1987 Archaeological Monitoring of Sewer Line Construction from Spreckelsville to Ku'au, Maui, State of Hawaii. Prepared for Department of Public Works, County of Maui, by Department of Anthropology, Bishop Museum, Honolulu.
- Dean, Arthur L.  
1950 Alexander and Baldwin, Ltd. and the Predecessor Partnerships, Alexander and Baldwin, Ltd., Honolulu.
- Foote, D.E., E.L. Hill, S. Nakamura, and F. Stephens  
1972 Soil Survey of the Islands of Kauai, Oahu, Maui, Molokai and Lanai, State of Hawaii, U.S. Department of Agriculture, Government Printing Office, Washington, D.C.
- Fredericksen, Demaris L., and Erik M.  
2003 An Archaeological Assessment Survey Report for a Parcel of Land in Pa'ia, Maui, Hawaii. Prepared for Maui Electric Company, Ltd, Kahului, Maui. Prepared by Xamanek Researches, Pukalani, Maui.
- Fredericksen, Walter M., Demaris L., and Erik M.  
1988 Archaeological Inventory Survey Preliminary Report on a Parcel of Land in Pa'ia, Maui, Hawaii. Prepared for Alexander and Baldwin Properties Group, Kahului, Maui, by Xamanek Researches, Pukalani.

**APPENDIX A – PHOTOGRAPHS 7-17**

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26  
27  
28  
29  
30  
31  
32  
33  
34  
35  
36  
37  
38  
39  
40  
41  
42  
43  
44  
45  
46  
47  
48  
49  
50  
51  
52  
53  
54  
55  
56  
57  
58  
59  
60  
61  
62  
63  
64  
65  
66  
67  
68  
69  
70  
71  
72  
73  
74  
75  
76  
77  
78  
79  
80  
81  
82  
83  
84  
85  
86  
87  
88  
89  
90  
91  
92  
93  
94  
95  
96  
97  
98  
99  
100



Photograph 7 - View to the east of BT 1.



Photograph 8 - View to the northeast of BT 2.



Photograph 9 – View to the east of BT 3.



Photograph 10 – View to the west of BT 4.



Photograph 11 – View to the southwest of BT 5.



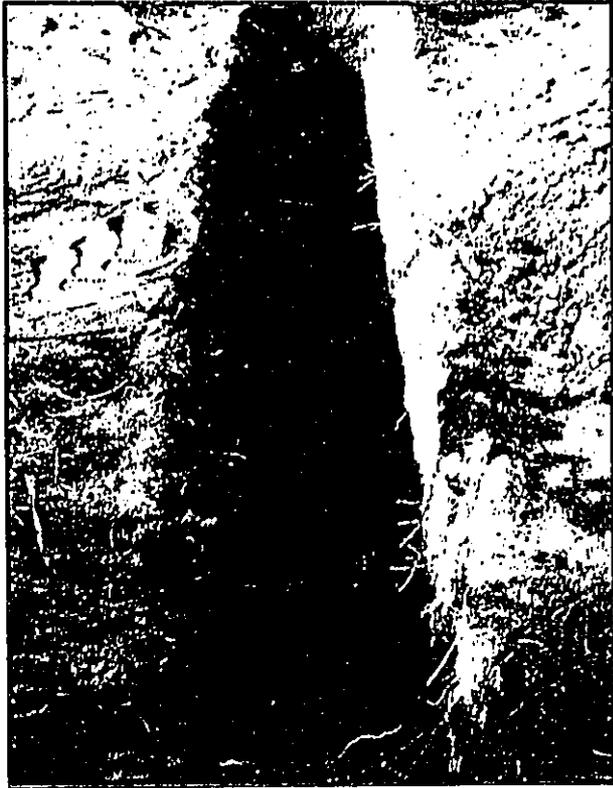
Photograph 12 – View to the northeast of BT 6.



Photograph 13 – View to the northeast of BT 7.



Photograph 14 – View to the southwest of BT 8.



Photograph 15 – View to the northwest of BT 9.



Photograph 16 – View to the southeast of BT 10.

RECEIVED AS FOLLOWS



Photograph 17 - View to the southwest of BT 11.



**A Native Hawaiian Traditional Cultural Practices  
Assessment of Paia**

**Commercial, Paia Post Office Island of Maui.  
(TMK: (2)2-5-05:18(Portion), an area of approximately  
5 acres which is presently in sugar cane.**

**FINAL**

**By**

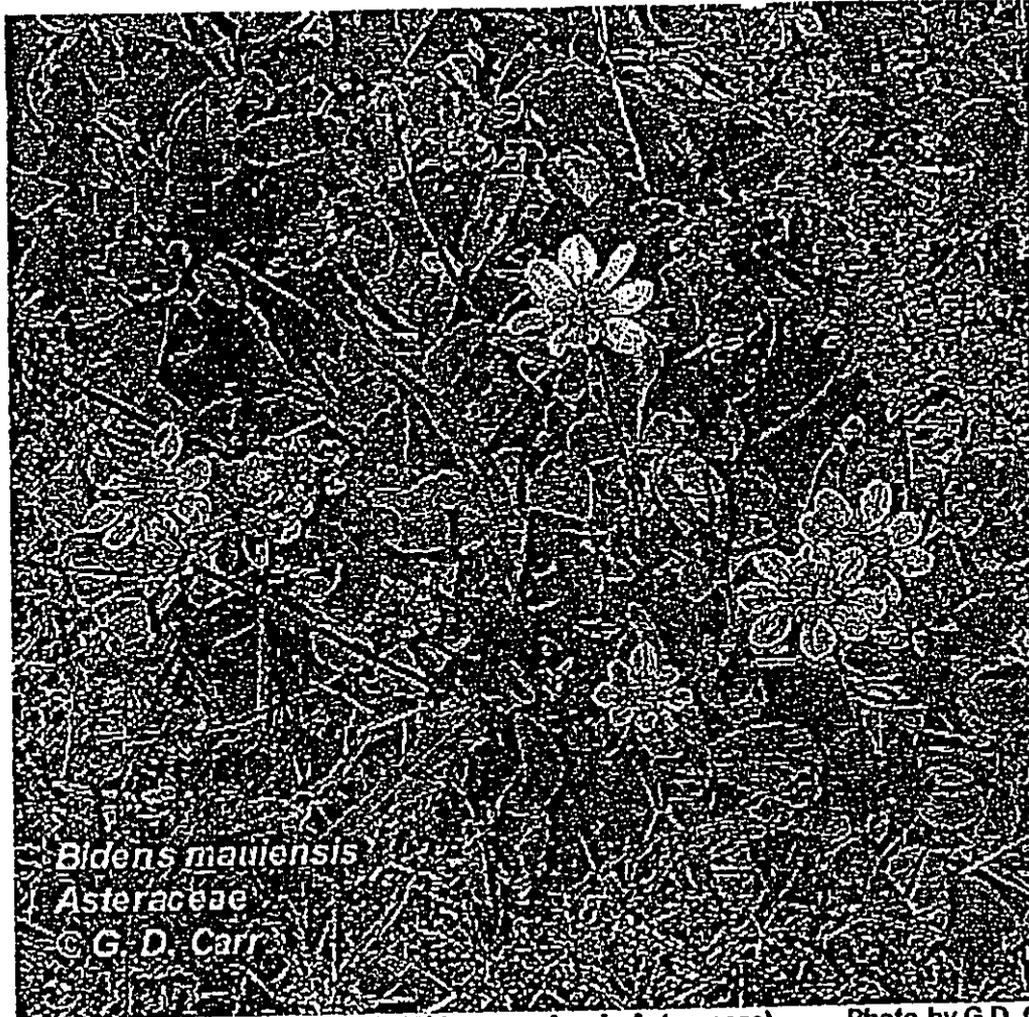
**Charles Kauluwehi Maxwell Sr.,  
Hawaiian Cultural Specialist**

**Prepared for**

**A&B PROPERTIES INC.  
P.O. Box 156 Kahului Maui 96733**

**CKM CULTURAL RESOURCES  
July, 2002**

A Native Hawaiian Traditional Cultural Practices Assessment  
Of Paia Commercial, Paia Post Office, Paia Maui.  
Island Of Maui  
(TMK: (2)2-5-05:18 – Approximately 5 Acres



Ko'oko'olau (Hawaiian tea plant) (*Bidens mauiensis* Asteraceae) Photo by G.D. Carr

By  
Kahu (Rev.) Charles Kauluwehi Maxwell Sr.  
Cultural Practitioner

Prepared for  
A&B Properties Inc., Subsidiary of Alexander & Baldwin Inc.  
PO Box 156, Kahului Hi, 96733-6656

CKM Cultural Resources  
July 2000

**TABLE OF CONTENTS**

**COVER.....II**

**TABLE OF CONTENTS.....III**

**ABSTRACT.....IV**

**INTRODUCTION.....V**

**Sections: I. The District (Ahupua'a)**

        a. What 'ili (land section) are contained  
           in Paia.

        b. The topographic change

        c. The topographic details

**II. Hawaiian Fauna (Lā'au Hawai'i)**

        a. Various/Native fauna

        b. Sugar Cane

**III. Lifestyle (Ke Ola Nei)**

        a. Paradigm of living

        b. Today's generation

**Sections 1, 11 and 111, pages 1 to 7**

**INTERVIEW INFORMANTS.....VI**

    a. The interview Process

**ARCHEOLOGICAL ASSESSMENT OF SITE.....VII**

**ATTACHEMENTS MAP.....VIII**

**CONCLUSION (Ua Pau).....VIII**

**BIBLIOGRAPHY.....X**

## ABSTRACT

CKM Cultural Resources conducted a native Hawaiian traditional cultural practices assessment of the A Native Hawaiian Traditional Cultural Practices Assessment Of Paia Commercial, Paia Post Office, Paia Maui. Island Of Maui (TMK: (2)2-5-05:18(Portion))

The Cultural assessment is for an area of approximately 5 acres and presently it is in sugar cane. Historical documentation is sparse for this exact area because of the long use of sugar cane planting. A major focus of this study was historical research, and limited interviews because of the fact that this area is small and most long time residence have moved away or passed on. Several interviews were conducted and the memory of the persons interviewed only remembers cane growing in this area. The types of usage on this area were highlighted in the Introduction of this report.

Briefly in ancient times the people that used this area did so for farming only, however the habitation studies which will be done by the Archeologist will most likely show that this is true. Until the Archeological report is complete, and the presence or lack of iwi (human remains) are found, no assumptions can be made

NOTE: As much as possible, throughout this report, the spelling of Hawaiian vocabulary and place names had been standardized to present orthography.

## INTRODUCTION

V

### History of Pā'ia, Maui

#### Sections:

- I. The District (Ahupua'a)
  - a. What 'ili (land sections) are contained in Pā'ia
  - b. The topographic change
  - c. Topographic details
- II. Hawaiian Fauna (Lā'au Hawai'i)
  - a. Various/Native fauna
  - b. Sugar Cane
- III. Lifestyle (Ke Ola Nei)
  - a. Paradigm of living
  - b. Today's generation

#### Section I. The District (Ahupua'a)

Pā'ia is a large land district or ahupua'a in the northern shores of Maui. The shoreline is named Kapukaulua, meaning the ulua (certain species of jack or crevalle fish) pit. It was named this because of the abundance of ulua during the certain fishing season.

The Pā'ia ahupua'a is bordered by three other ahupua'a, Wailuku, Ha'ikū, and Pu'u O Kali, respectively. From these three ahupua'a, Pā'ia's ahupua'a extends to more portions of the ocean than it's bordering ahupua'a.

In this particular ahupua'a there are many 'ili. The main east end border for this ahupua'a is Kū'au. Nearing the ocean front, Kū'au sets one line for the ahupua'a. Another point in the border system is Hālī'imaile, from here the border extends to Pukalani (traditionally Pu'ukalani) on the slopes of Maui's eastern mountain, Haleakalā. From Pukalani the border extends back down to the sea to where currently the Kahului airport resides. In the traditional ahupua'a system, this wahi (area) is known as Pā'ia.

These are the different 'ili contained within Pā'ia. Ho'okipa, Kū'au, Kāheka, Kailua, Hālī'imaile, Pukalani (traditionally Pu'ukalani), Keāhua, Spreckelsville, and Kapukaulua - shoreline.

The 'ili that encompasses the old sugar mill and the area around it (for at least a radius of 1.5 miles/2.3 kilometers) is a mix of two separate 'ili - Kāheka and Kailua<sup>1</sup>. This area has an elevation between 560 ft. to 1,269 ft. - Kailua's highest peak in the region.

In lieu of the U.S. Census, the change of land zoning for these tracts of lands may appear differently as compared to the Pā'ia- Kāheka and Kailua

traditional ahupua'a system. The topography of Pā'ia had changed when the areas were rezoned due to reapportionment. Therefore, this report has combined both traditional land uses of this area (i.e. traditional names) and the current topography that this area is currently zoned as.

The land in this area is dry, yet fertile. One 'ōlelo no'eau (Hawaiian proverb) says, "Ka makani hāpala lepo o Pā'ia." This literally means, "Dust smearing wind of Pā'ia." The land is also at a bit of an incline, as mentioned earlier. This incline, the slightly arid temperatures, and the dry plains, made the area perfect for growing 'uala (Ipomea batatas) or sweet potato.

## II. Hawaiian Fauna (Lā'au Hawai'i)

This area rarely made perfect lō'i kalo (taro patches) because of the dry conditions. Therefore, sweet potato may have been a large source of carbohydrate sufficiency because of the lack of water in the area. Although the fact is recognized that Hawaiians would traditionally trade, swap, and share various foods, kalo or taro (*Colocasia esculenta*) was not commonly available, as if one were to have a lō'i kalo in the immediate vicinity.

Another plant that may have grown in this area, to supplement the need of kalo, is 'ulu (*Artocarpus incisus*) or breadfruit. According to a book titled, "Native Planters In Old Hawai'i: Their life, lore, and environment," written by E.S. Handy et al. explicates, "...early voyagers noted extensive planting of breadfruit along the southern and leeward coast..." Although this statement singles out the Southern and leeward coasts, more of the dryer areas on the island, Pā'ia still made a perfect place for 'ulu to grow because of its dry dusty plains. 'Ulu also grew in many of the bordering districts that were near the Kāheka - Kailua area.

Hala (*Pandanus odoratissimus*) or Pandanus may have also been plants

that made a comfortable home in Pā'ia, more specifically Pā'ia- Kāheka and Kailua

the Kāheka and Kailua areas. Hala was known to grow vibrantly in the bordering ahupua'a mentioned earlier. This would be useful in the process to create needed objects in the home, farm, and family settings.

Pili (*Heterogon contortus*) grass was also quite common in these areas because of the climate conditions. Pili liked to grow in arid and dusty conditions. This grass was useful to Hawaiians in that the dried grass would be made into bunches and used to thatch the roofs of homes in the area.

On of the ground covers used to keep some of the dirt from blowing in the wind was Pā'ū o Hi'iaka (*Jacquemontia ovalifolia*). This was a ground covering vine with abundant tubular flowers that range in color from light blue, purple, to white. This plant did not need much water, which in turn would make Pā'ia perfect areas of growth for the Pā'ū o Hi'iaka.

While Hawaiians of the past used Pā'ū o Hi'iaka for curing keiki (children) of ea (thrush, a mouth disease), this plant is better known for the mo'oleo (story) that explains its name. Long ago, Pele, the volcano goddess, took her youngest sister, Hi'iaka, to the ocean. As Pele was out amongst the waves fishing, or some say surfing, the sun climbed higher and hotter in the sky. Meanwhile, Hi'iaka waited patiently on the shoreline for her sister. A plant near Hi'iaka, seeing that the keiki's tender young skin was being burned by the sun's merciless rays, took pity upon Hi'iaka and extended its viney branches to shield her. When Pele returned from the ocean, she discovered Hi'iaka covered, and protected, by the plant. In gratitude, Pele gave the plant its name, Pā'ū (skirt) O Hi'iaka (of Hi'iaka), my baby sister.

Another blossoming plant that may have resided in this area is the 'a'ali'i (*Dodonaea viscosa*) bush. This hard wood native shrub is indigenous to the islands. This plant also grows well in dryer climates. Ranging in heights of one to thirty feet, this shrub to tree is found growing at elevations up to 8,000 feet and wind-swept open country. In today's day and age, 'a'ali'i is being used to reforest the island of Kaho'olawe. This island's water plate is cracked in half from missile testing by the U.S. Another plant which grew profusely in this area was Ko'oko'olau (Hawaiian tea plant) or (*Bidens mauiensis* Asteraceae). It was used by the ancients and is still used today as tea and for medicinal purposes. A picture of this plant is being used as the cover of this report.

Pā'ia- Kāheka and Kailua government in the late 1960's and '70's. Kaho'olawe is not able to retain water because of the cracked water plate, yet the 'a'ali'i is doing

well in growing and flourishing on the island.

One plant that has proved itself worth the while is, Kō (Saccharum officinarum) or sugar cane. Kō is an extremely low maintenance plant that is easy to deal with when water is not as readily available. Alexander and Baldwin found this to be true and later built an empire with this cultural knowledge. The sugar cane, up until the late 1980's, put Hawai'i at the forefront of the sugar cane industry. Today, that industry struggles to survive among top competitors. However, this industry has left many marks (both good and bad) in the history of Hawai'i and the lives of many families, native and non-native.

### III. The Lifestyle (Ke Ola Nei)

Upon the introduction of the sugar cane industry there had been a shift in the treatment of land and other resources contained in these areas. Land divisions were plowed, unearthed, and made into fields to plant the profitable crop of sugar cane. No doubt, this industry employed generations of people. In a matter of years the planting and cropping system of plants in the area of Kāheka and Kailua went from culturally based farms to crops of mass production.

Land titles were lost to quid pro quo deals and nepotistic actions taken by officials who were friends with sugar cane tycoons. At the behest of all of this, was the traditional lifestyle of native Hawaiians, their native wildlife, and the native and indigenous fauna. A lifestyle of its own accelerated the lives of the natives of the area to quickly assimilate to a system unfamiliar to their own.

In the wake of this paradigm, native Hawaiians fared poorly, if they chose to live a cultural lifestyle in this ahupua'a of Pā'ia. As more continental Americans migrated to the islands in search of employment, either as military personnel or to fill vacancies in other industries, they brought with them a new way of life to Pā'ia- Kāheka and Kailua.

## VI

## INTERVIEW INFORMANTS

Based on the recommendations of present residents of Paia, several knowledgeable individuals were identified on (1) knowledgeable individuals with cultural expertise and knowledge of the project area and surrounding vicinity, and (2) identify cultural concerns and potential negative impacts relative to the project. An effort was made to identify *Kupuna* (elders) who either grew up in Paia close to the project area or lived in the area that could relate to the project area prior to the growing of the sugar cane.

5 people were identified as having potential knowledge of the project area, however when contacted, they related that 2 of the people grew up in Kū'au and stated that they only knew the area as having sugar cane growing at the project site and have no knowledge prior to the planting of the cane. The other person, who was born above the project site, did not want to be interviewed and indicated that his recollection was that cane was growing there since he was a "little boy".

It is only common sense to surmise that the planting of sugar cane in the project area probably started in the middle 1800's, and the average age of the *Kupuna* was 75 years old, thus the history of sugar in the project area was firmly established more than 60 years before any of these *kupuna* were born. Therefore it would be difficult to locate *kupuna* with first-hand cultural information of the project area since the integrity of the landscape had been compromised long before they were born.

The two interviews were done by formal taped interviews.

INTERVIEW OF: AARON KALANI BROWN 8/1/02 - 9:00AM.

Aaron Brown was interviewed in my vehicle on the project. He stated that he was born in Hilo on April 3, 1918. That he moved to Paia in 1930, when he was 11 years old and lived close to this project. His family rented a home for \$12 a month. He now lives on Hana Highway, in Lower Paia. He can recollect that across the project site was housing and people from all over Maui lived in Paia. They did not necessarily work for the Sugar Plantation. That he resents the calling of Paia as a "Plantation Town", because a lot of people back then and now does not work for the plantation especially now that Paia Mill has shut down.

As far as he can remember there was sugar cane growing at the project site and does not know of anyone living today that could recall the area prior to planting of the sugar cane. He further related that he can recall when "Upper Paia" and "Lower Paia" were large communities. Upper Paia had its own bakery, and all the different camps (Hawaiian Camp, Nashiwa Camp, Russian Camp, Paia School, the Orphanage, the Catholic Church and Japanese Church).

The ocean below Paia town was always accessible to the residents and everyone used the ocean for sustenance.

He has no recollection of cultural or historical sites in the project area.

He was not happy with the proposed project as he feels that it will open up the use of agricultural land for commercial purposes and would very detrimental and would add to the traffic congestion that is happening now.

INTERVIEW OF CHARLOTTE A. MAXWELL 8/20/02 – 12:45PM.

Charlotte A. Maxwell was interviewed at her home at 157 Alea Place, Pukalani. She related that she lived in Upper Paia, below the Gym and above what is now Doris Todd School. She is 64 years old born on Feb. 10, 1938. She lived there until she was about 5 or 6 years old and can remember that the proposed project was across residential housing, planted in sugar cane. Her grandfather and mother, in fact all her family worked for the Paia Mill. Also that there was hardly any traffic on the highway and to get from one place to the other they would use the bus service. There were different Camps located in Upper Paia and the Nashiwa Bakery where they would walk to early in the morning to buy pastries. Her favorite place was "Camp Store" where they sold all kinds of "goodies" like crack seed, pine nuts etc. Most of her aunts and uncles did not go to High School because they had to help support a large family of 11 children.

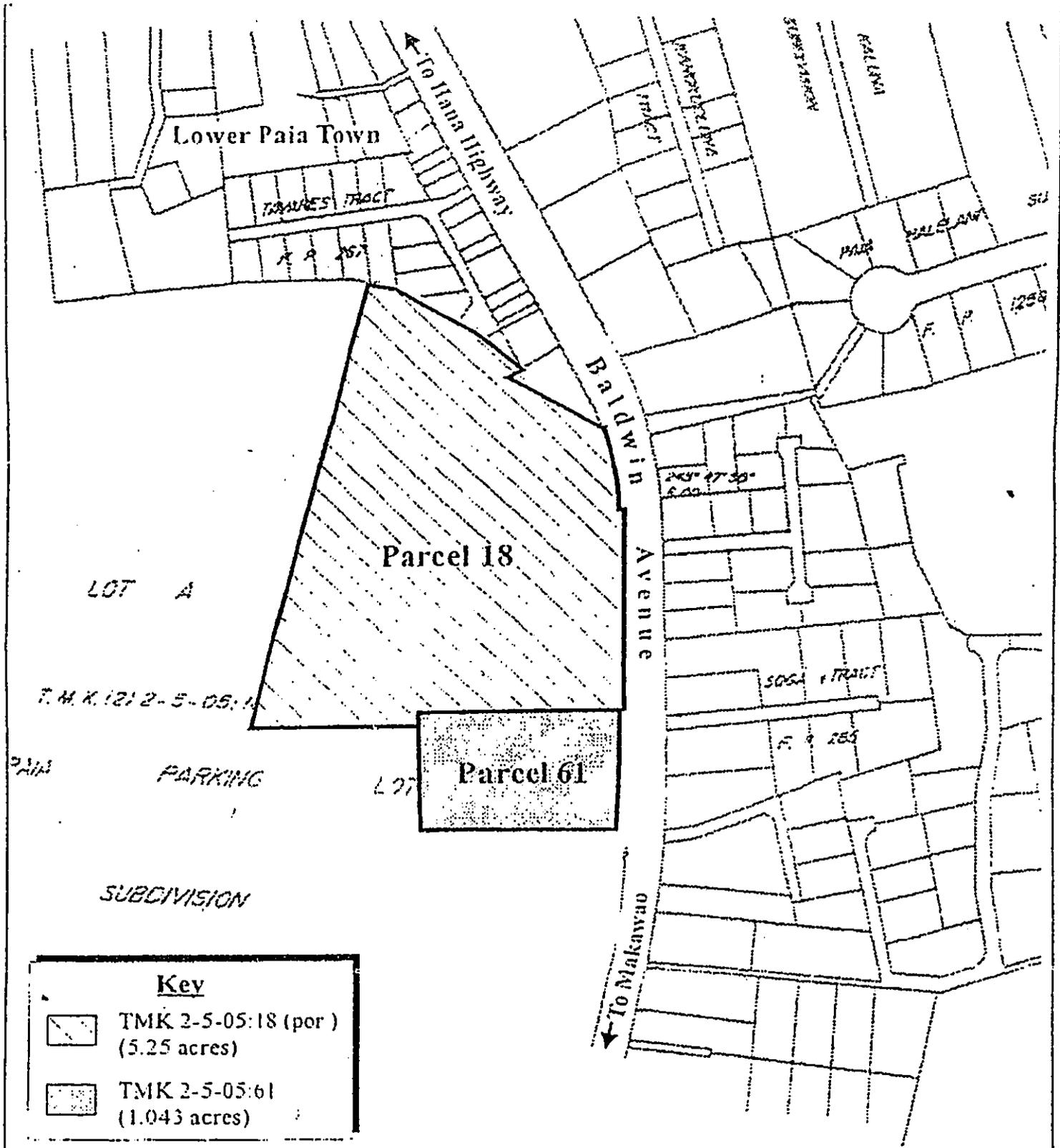
Almost all the people that lived in the camps worked either in the cane fields or at Paia Mill. Across the Mill there was the A&B Store. Although she moved away at an early age, she would visit her Uncle who lived across the Punawai, below the mill.

As far as she can remember the entire Paia area was heavily grown in sugar cane and if anything of Hawaiian cultural significance was present, it would have been impacted by planting of the sugar cane a long time ago.

## ARCHEOLOGICAL ASSESSMENT OF SITE

The archeological Assessment of the site cannot be completed until the sugar cane planted on the project is harvested. Because of this fact, the Cultural Assessment is being finalized and when the Archeological report is completed and should there be significant features, artifacts or human remains, it would be noted in the report from the archeologist.

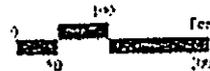
Because of the long history of sugar cane planting, it would be highly improbable that anything on the surface would be found for a cultural analysis. However there have been incidents where human remains have been found in Olowalu under sugar cane fields.



Source: A&B Properties, Inc.

## Entitlements Requests for Paia Post Office and Adjacent Country Town Business/Parking Area

### Site Location Map



Prepared for: A&B Properties, Inc.

MUNEKITO & HIRAGA, INC

## CONCLUSION

### Conclusion (Ua Pau)

Kāheka and Kailua were full of life with different plants and people to mālama (care) the 'āina (land). Today, many generations of families have resided near Kāheka and Kailua, because majority of this area is laden with sugar cane crops, the history beneath it is hidden.

Various species of native fauna inhabited the area, quite possibly some plants that grew there no long exist in Hawai'i. It is important to stress the sanctity of these areas, Kāheka and Kailua, respectively. *Pā'ia is an extremely diverse ahupua'a, from Kū'au to Pukalani and back down to Sprecklesville, the ahupua'a covered vast lands and different landscapes.* Today, majority of Pā'ia's land is now covered by sugar cane crops. Pā'ia, once home to traditional farmers and fishermen, now home to generations of others who come for the sport of windsurfing at Ho'okipa Park in Kū'au. And then there are others who have built homes and work in other parts of Maui and people that have chose to build retirement homes on the outskirts of Paia.

## BIBLIOGRAPHY

Buck, Peter H. Arts and Crafts of Hawai'i. Honolulu: Bishop Museum Special Publication No. 45, 1957.

Emerson, Nathaniel B. Pele and Hi'iaka. A myth from Hawai'i. Rutland, VT and Japan: C.E. Tuttle, 1978.

Handy, E.S. Craighill, et al. Native Planters in Old Hawai'i: Their Life, Lore, and Environment. Honolulu, Hawai'i: 1991.

Kahumoku III, Ph D., Walter. Doctoral Dissertation: University of Hawai'i: 2000.

Malo, David. Chs. 35-37 in Hawaiian Antiquities. 1898. Honolulu: Bishop Museum Special Publication No. 2, 1976.

Mitchell Ph.D., D. Kilolani. Resource Units in Hawaiian Culture: Revised Edition. Honolulu, Hawai'i: Kamehameha Schools Press, 2001.

Puku'i, Mary Kawena, et al. Place Names of Hawai'i. Honolulu, Hawai'i: University of Hawai'i Press, 1976.

Puku'i, Mary Kawena, et al. 'Ólelo No'eau: Hawaiian Proverbs and Poetical Sayings. Honolulu, Hawai'i: Bishop Museum Press, 1983.

United States. United States Map of Census 2000. U.S. Census 2000. Washington: GPO, 2000.

Wise, John H. "Food and Its Preparation." Ch. 8 in Ancient Hawaiian Civilization. A series of lectures delivered at the Kamehameha Schools by E.S. Handy et al. 1933. Rutland, VT and Japan: C.E. Tuttle, 1965 (rev. ed.)





AUSTIN, TSUTSUMI & ASSOCIATES, INC. CIVIL ENGINEERS • SURVEYORS  
 CONTINUING THE ENGINEERING PRACTICE FOUNDED BY H. A. R. AUSTIN IN 1934

KENNETH K. KUROKAWA, P.E.  
 LAMBERT J. YAMASHITA, P.E.  
 DONOHUE M. FUJI, P.E.  
 STANLEY T. WATANABE  
 TERRANCE S. ARASHIRO, P.E.  
 MERNA S. KIBE

#O-01-001.1  
 May 22, 2002

Mr. Rick Stack, Project Manager  
 A&B Properties, Inc.  
 822 Bishop Street  
 P.O. Box 3440  
 Honolulu, Hawaii 96801-3440

NB DIH LNA JAC PWH YR  
 LR CWL  
~~SMK~~ AM  
 DMS MAY 22 2002 SKM  
 DVL JP  
 MGW BAK  
 RKS SWR CC SG JY DYY RBS

Dear Mr. Stack:

**Subject: Interim Paia By-Pass Road**

This is a letter report of our findings, conclusions and recommendations resulting from our study of the existing traffic conditions on Hana Highway during the afternoon traffic period in the vicinity of Paia Town, and more specifically the eastbound approach to the Hana Highway/Baldwin Avenue intersection.

Travel time runs and traffic counts were conducted on Tuesday, April 16, 2002 to quantify operating conditions on eastbound Hana Highway during the afternoon period of traffic. The results of the field data are attached to this letter.

**Background**

A&B Properties, Inc. has suggested the construction of a PM peak period of traffic by-pass road, which would skirt around the southwest side of Paia Town to facilitate the late afternoon movement of eastbound traffic through Paia Town. This by-pass road would follow an existing cane field road alignment, beginning just west of the County of Maui parking lot on Hana Highway, and connect to Baldwin Avenue south of the new Paia Post Office. Figure 1 shows the general alignment of the proposed by-pass road. The length of the by-pass road is approximately 0.4 miles. The by-pass road would be used only during the PM peak period of traffic, probably between 3 PM and 6 PM, and would be one-way in the eastbound direction only.

The concept for the by-pass road is to relieve the traffic demand on the eastbound approach to the Hana Highway/Baldwin Avenue intersection by diverting onto the by-pass road motorists destined for areas mauka of Paia town, or to the post office.

Eastbound PM traffic generally begins to congest at about 3 PM and deteriorates as the traffic demand increases towards the evening peak hour of traffic. Traffic operation on Hana Highway breaks down through Paia because of the street side friction caused by the diagonal parking, left-turning vehicles and pedestrians. This causes vehicular traffic at times to come to a complete stop while a motorist is backing out of a diagonal parking

REPLY TO:  
 501 SUMNER STREET, SUITE 521 • HONOLULU HAWAII 96817-5031  
 PHONE (808) 533-3949 • FAX (808) 528-1287 • EMAIL: atc@atahawaii.com

ET  
 4D-1

OFFICES IN:  
 HONOLULU, HAWAII  
 WAILUKU, MAUI, HAWAII

Mr. Rick Stack, Project Manager  
A&B Properties, Inc.

May 22, 2002

stall, and sends traffic shock waves on Hana Highway back towards Spreckelsville and queues back to near Nonohe Place.

#### Travel Time Data and Intersection Turning Movement Counts

Travel time between Haleakala Highway and Baldwin Avenue, a distance of 3.8 miles, normally takes approximately 6 minutes under free flow conditions, but during times of congestion through Paia Town, the travel time can take 15 to 20 minutes to traverse the 3.8 miles distance. Generally, traffic begins to slow down approaching Nonohe Place, and comes to "stop and go" operations near Ulupala Place, continuing on to Baldwin Avenue. The severity of the delay is dependent upon the street side friction on Hana Highway between the County parking lot and Baldwin Avenue.

275/12 → Traffic count data obtained on April 16, 2002 shows that 2,255 eastbound vehicles approached the Hana Highway/Baldwin Avenue intersection between 3 PM and 6 PM. Approximately 30% or 664 vehicles turned right to proceed mauka on Baldwin Avenue. The proposed by-pass road is targeting to remove or divert a good percentage of the right-turning vehicles to reduce the approach demand at the intersection and reduce the travel time for motorists destined for the post office or areas mauka of Paia Town, thus reducing the traffic demand approaching the Hana Highway/Baldwin Avenue intersection.

#### Discussion

The proposed by-pass road, beginning on Hana Highway at the west side of the County parking lot, continues southward, following the alignment of a cane field road for approximately 1,000 feet, then veers off southward to skirt around the Paia Post Office before connecting to Baldwin Avenue as a stop-controlled approach. It is anticipated that the by-pass road would be a single lane, paved roadway with a posted speed limit of 15 miles per hour. To traverse the 0.4-mile by-pass road, it would take approximately 1.6 minutes to arrive at the stop sign at its intersection with Baldwin Avenue.

On the other hand, depending upon the street side friction on Hana Highway, travel time runs show that it can take less than 30 seconds to cover the 0.2 miles from the by-pass road intersection with Hana Highway to the Hana Highway/Baldwin Avenue intersection, to an extreme of 3 minutes. Most likely the travel time during the PM peak will be somewhere 2± minutes. Then to head south (mauka) on Baldwin Avenue to the intersection with the proposed by-pass road, a distance of approximately 0.26 miles, could take another minute, for a total travel time of approximately 3± minutes. Therefore, there would be some savings in travel time for those electing to use the by-pass road.

However, it is our opinion that the by-pass road as proposed will not significantly reduce the length of the moving queue or eliminate the traffic shock wave because its access from Hana Highway is located too close to area of Paia Town that is causing the traffic to congest, namely the built-up area and the area along the diagonal parking. The majority

D-2



AUSTIN, TEUTSUMI & ASHLEY, INC.  
CIVIL ENGINEERS • SURVEYORS

Mr. Rick Stack, Project Manager  
A&B Properties, Inc.

May 22, 2002

of the delay is encountered between Ulupala Place and the intersection to the proposed by-pass road. It can take 4 or more minutes to cover this 1.2 miles distance.

### Findings and Conclusions

It is common knowledge to the residents and commuters that traffic literally comes to a slow crawl on eastbound Hana Highway every weekday afternoon between 3 and 6 PM. Field observations confirm that the street side friction is the primary cause of this traffic delay. A vehicle on the highway waiting for a parked vehicle to back out of the diagonal parking stall will cause traffic to come to a complete stop for 20 to 40 seconds, about the length of time for a wait at the traffic signal at the Baldwin Avenue intersection. In addition, left-turning vehicles and jay-walking pedestrians also cause traffic to come to a momentary stop. This also leads to the inefficient operation of the Baldwin Avenue traffic signal system, as there are times when the Hana Highway green indication is on and there are no eastbound traffic approaching the intersection (they are stopped waiting to access a diagonal parking stall).

Contributing to the slow moving queue that can extend for approximately 1.5 miles on Hana Highway, is the fact that the commuters know that its "stop and go" operations ahead, that they deliberately maintain a low speed and let gaps between vehicles grow, because they will most likely have to stop as they get into Paia Town.

The location of the proposed by-pass road intersection with Hana Highway, near the County parking lot, is too close to the Baldwin Avenue intersection to really provide meaningful relief to the queued traffic. To be beneficial, it is our opinion that the access to the by-pass road should be located approximately one mile west of its proposed connection to Hana Highway. A parallel road to Hana Highway could be constructed on the mauka side of Hana Highway between a point near Ulupala Place and the current location of the by-pass road. Being able to access the by-pass road a mile or more out of town will then provide a significant savings in travel time for motorists destined for the Paia Post Office or areas mauka of Paia via Baldwin Avenue. Further there should be no opportunity for motorists using this parallel road to re-enter Hana Highway closer to Paia Town to prevent "cheaters".

### Recommendation

Based upon our findings and conclusions, we propose the following to provide some reduction to the delay encountered by eastbound weekday commuters on Hana Highway at Paia:

1. Construct the single lane by-pass road between Hana Highway and Baldwin Avenue, as suggested by A&B Properties.
2. However, extend the proposed by-pass road to run parallel to Hana Highway to a point near Ululapa Place before connecting to Hana Highway.

"D-3"

Mr. Rick Stack, Project Manager  
A&B Properties, Inc.

May 22, 2002

3. This road will be one-way in the eastbound to southbound direction, and open to public traffic during a specified period of time, say, 3 PM to 6PM on weekdays, and closed during weekends and holidays.
4. There should be no opportunity to re-enter Hana Highway from the by-pass road. (Emergency access should be provided to and from Hana Highway along the parallel section of the by-pass road, but a removable barrier should physically control it.)

The specifics of the roadway section and costs will be developed further if this concept is acceptable to A&B Properties, Inc.

The above expresses our opinions of the merits of the proposed by-pass road without conducting a simulation model of the before and after effects of implementing the proposed by-pass road.

Should you have any questions on any part of our report, please do not hesitate to call me. It has been our distinct pleasure to provide this professional traffic engineering assessment to you.

Sincerely,

AUSTIN, TSUTSUMI & ASSOCIATES, INC.

By *Ted S. Kawahigashi*  
TED S. KAWAHIGASHI, P.E., FACEC  
Principal Transportation Engineer

TSK:svd

Attachments: Figure 1 Proposed Road Alignment  
Traffic Count Data  
Travel Time Run Data

Z:\2001\01-001.TA&S-Interim Pals By-Pass Rd 052002.doc

D-4<sup>u</sup>

**TRAVEL TIME - HANA HIGHWAY  
HALEAKALA HIGHWAY TO BALDWIN AVENUE**

Date of run: April 16, 2002 (Tue)

Weather: clear

Driver: TSK

Start time of the day	2:16 PM	2:48 PM	3:19 PM	3:41 PM	4:04 PM	4:21 PM	4:35 PM	Dist. (mi)
Haleakala Hwy to Sprecklesville Bch Rd	0:01:59	0:02:00	0:01:51	0:01:55	0:01:50			1.4
to Alapaka Pl	0:00:28	0:00:28	0:00:27	0:00:26	0:00:28			0.3
to Nonohe Pl	0:00:31	0:00:33	0:00:32	0:00:32	0:00:32	0:00:00	0:00:00	0.4
to Ulupala Pl	0:00:26	0:00:21	0:00:24	0:00:25	0:00:24		0:00:25	0.3
to H.A. Baldwin Park Rd	0:00:57	0:00:57	0:01:00	0:01:03	0:01:11	0:01:57	0:03:16	0.7
to Pala Sign	0:00:49	0:01:05	0:00:52	0:00:57	0:01:10	0:02:55	0:04:09	0.5
to Baldwin Avenue	0:00:26	0:03:07	0:00:28	0:01:17	0:01:24	0:01:51	0:00:55	0.2
<b>Total Time</b>	<b>0:05:36</b>	<b>0:08:31</b>	<b>0:05:34</b>	<b>0:06:35</b>	<b>0:06:57</b>	<b>0:08:43</b>	<b>0:09:16</b>	<b>3.8</b>
						0:08:59*	0:11:32*	

\*TT from Haleakala Highway to Baldwin Avenue

"D-5"

0-01-001

MANUAL TRAFFIC VOLUME COUNTS

Location: Paia, Maui  
 Hana Highway with Baldwin Avenue  
 April 16, 2002

Study Period	Hana Highway												Baldwin Avenue					
	Eastbound (Haiku Bound)				Westbound (Wailuku Bound)				Northbound (Makai Bound)									
	LT	HV	TH	RT	LT	HV	TH	RT	LT	HV	TH	RT	LT	HV				
3:00 PM	3	0	127	2	44	0	31	1	106	7			33	0			24	1
3:15 PM	3	0	114	1	49	0	30	0	92	3			45	0			28	0
3:30 PM	1	0	96	1	52	0	24	0	102	1			52	0			30	0
3:45 PM	8	0	121	1	75	0	28	0	127	3			42	0			35	0
4:00 PM	4	0	154	2	65	1	28	0	132	0			31	0			44	0
4:15 PM	0	0	139	1	46	0	29	1	101	1			50	0			42	0
4:30 PM	8	0	133	0	55	0	33	0	129	3			51	2			36	0
4:45 PM	3	0	121	1	47	0	27	0	75	2			45	0			26	0
5:00 PM	5	0	156	0	52	0	21	0	98	4			45	0			36	0
5:15 PM	7	0	124	0	71	0	18	0	71	2			35	0			35	0
5:30 PM	10	0	161	0	50	0	32	0	78	1			35	0			46	0
5:45 PM	6	0	145	3	58	0	21	0	79	2			34	0			39	0
Totals	58	0	1591	12	664	1	322	2	1190	29			498	2			421	1

LT = Left-turn movements  
 TH = Through movements  
 RT = Right-turn movements  
 \* Eastbound left-turns include those turning left from Hana Highway into driveways immediately before and after the intersection.

D-64

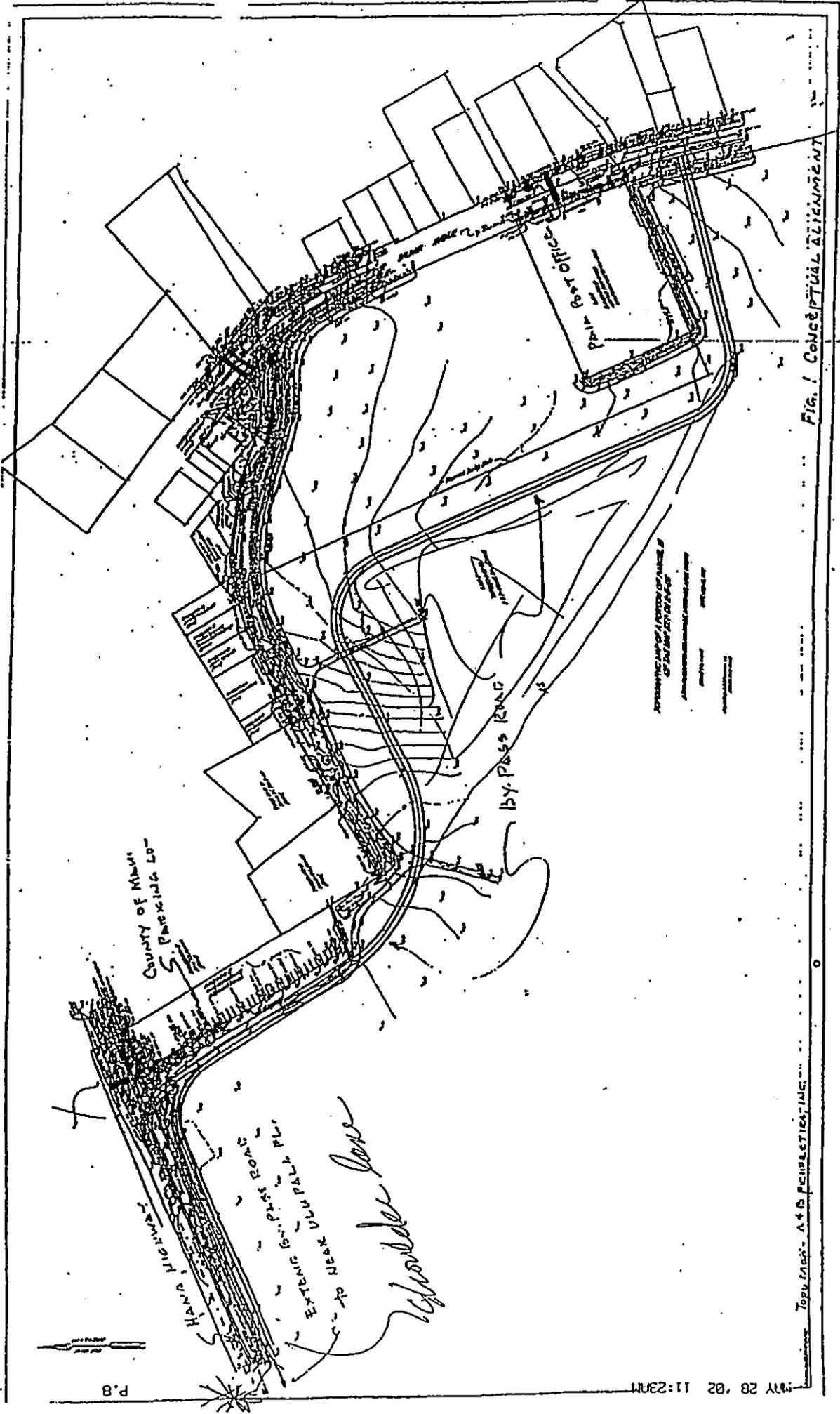


Fig. 1 Conceptual Alignment

D-7

## **TRAFFIC ASSESSMENT**

### **PAIA MINI-BYPASS**

October 2004

#### **BACKGROUND**

The purpose of this assessment is to evaluate anticipated traffic operations through Paia Town as a result of the proposed Paia Mini-Bypass. The one-way (eastbound) bypass road will only be open during the PM peak period and will connect Hana Highway (Route 36) southwest of the intersection with Baldwin Avenue to Baldwin Avenue near the Paia Post Office. This assessment evaluates traffic conditions during the afternoon PM peak hour period.

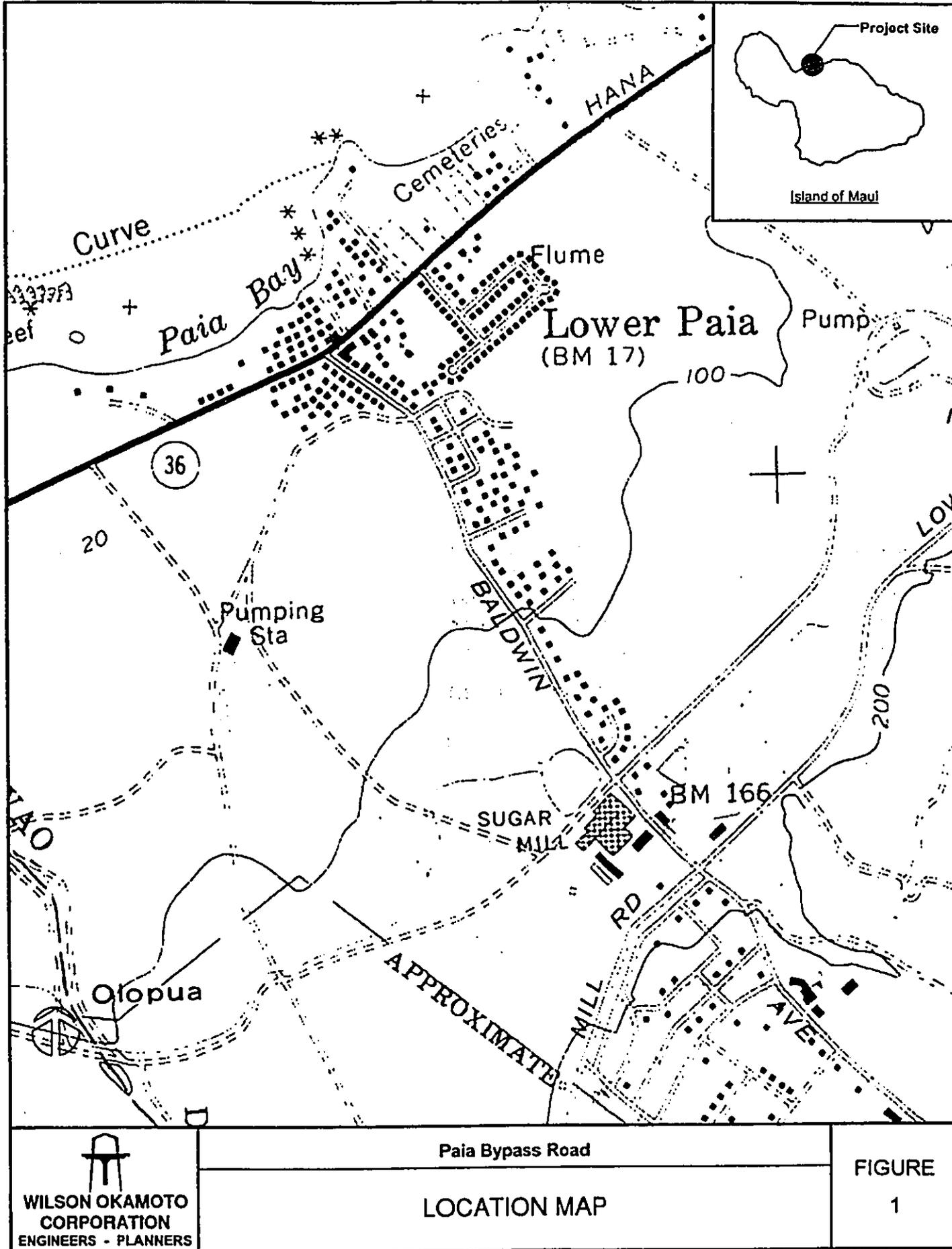
The proposed bypass roadway is expected to absorb some of the traffic demand in the vicinity by providing an alternate route for motorists headed eastbound along Hana Highway. Much of the traffic flow and congestion through Paia Town is controlled by the traffic signal system at the intersection of Hana Highway and Baldwin Avenue. The proposed bypass will allow motorists the opportunity to avoid this congested area.

#### **AREA ROADWAY SYSTEM**

There are two major roadways in the vicinity of Paia Town (see Figure 1). Hana Highway (Route 36) is a State arterial that connects with Kaahumanu Avenue at its western terminus near Maui Mall Shopping Center, and continues along the northern coast of the island towards Hana. In the vicinity of Paia Town, Hana Highway is a two-lane, two-way undivided roadway with turning bays provided at the intersection with Baldwin Avenue.

Baldwin Avenue is a County of Maui roadway that connects with Hana Highway at its northern terminus and continues southward towards Makawao. Baldwin Avenue is a two-lane, two-way roadway with access to local streets along its alignment. The Baldwin Avenue approach at the intersection with Hana Highway provides separate left-turn and right-turn lanes. On-street parking is provided along Baldwin Avenue within the Paia Town proper.

At the intersection of Hana Highway and Baldwin Avenue, vehicular traffic is controlled by a three-phase traffic signal operation with permissive westbound left-turn movements from Hana Highway to Baldwin Avenue. Right-turn yield movements on red indications are allowed from westbound Hana Highway to Baldwin Avenue, and from northbound Baldwin Avenue to Hana Highway. The three-phase, semi-actuated traffic signal system at the intersection generally operates within a 250-second cycle during the PM peak period of traffic.



## **EXISTING PM PEAK HOUR TRAFFIC OPERATIONS**

A field investigation was conducted on May 24, 2004 and consisted of manual turning movement count surveys during the afternoon peak hours between 3:00 PM and 6:00 PM at the intersection of Hana Highway and Baldwin Avenue. In addition, along with field observations, 24-hour traffic counts on Baldwin Avenue near the proposed bypass road connection were taken to aid in determining the approximate capture of trips to and from the Paia Town's shops, eateries, and other commercial establishments in the vicinity.

The highway capacity analysis performed for this assessment is based upon procedures presented in the "Highway Capacity Manual", Transportation Research Board, 2000, and the "Highway Capacity Software", developed by the Federal Highways Administration. The analysis is based on the concept a Level of Service (LOS) to identify traffic operational deficiencies of the study intersection. LOS is a quantitative and qualitative assessment of traffic operations. Levels of Service are defined by LOS "A" through "F", LOS "A" representing ideal or free-flow traffic operating conditions and LOS "F" representing unacceptable or potentially congested traffic operating conditions.

Based on the field data, the PM peak hour generally occurs between 3:15 PM and 4:14 PM at the intersection of Hana Highway and Baldwin Avenue. The existing left-turn and right-turn movements on the Baldwin Avenue approach to the intersection both operate at LOS "F" with turning vehicle volumes of 143 vehicles 177 vehicles, respectively. Maximum queue lengths on Baldwin Avenue at this intersection approach were observed to be generally at 17 vehicles, extending occasionally to 21 vehicles at the time of the survey. It was also observed that the relatively long vehicle queues would not clear after each traffic signal cycle change, thus resulting in significant motorist delays.

The eastbound and westbound approaches of Hana Highway at the intersection with Baldwin Avenue operate fairly well during the existing PM peak hour of traffic. The southbound through and left-turn movements operate at LOS "A" while the northbound through and right-turn movements operate at LOS "B" and LOS "A," respectively. However, the high eastbound traffic demand on Hana Highway in the afternoon resulted in significant vehicle queues along the highway. Figure 2 shows the existing PM peak hour traffic volumes and operating conditions.

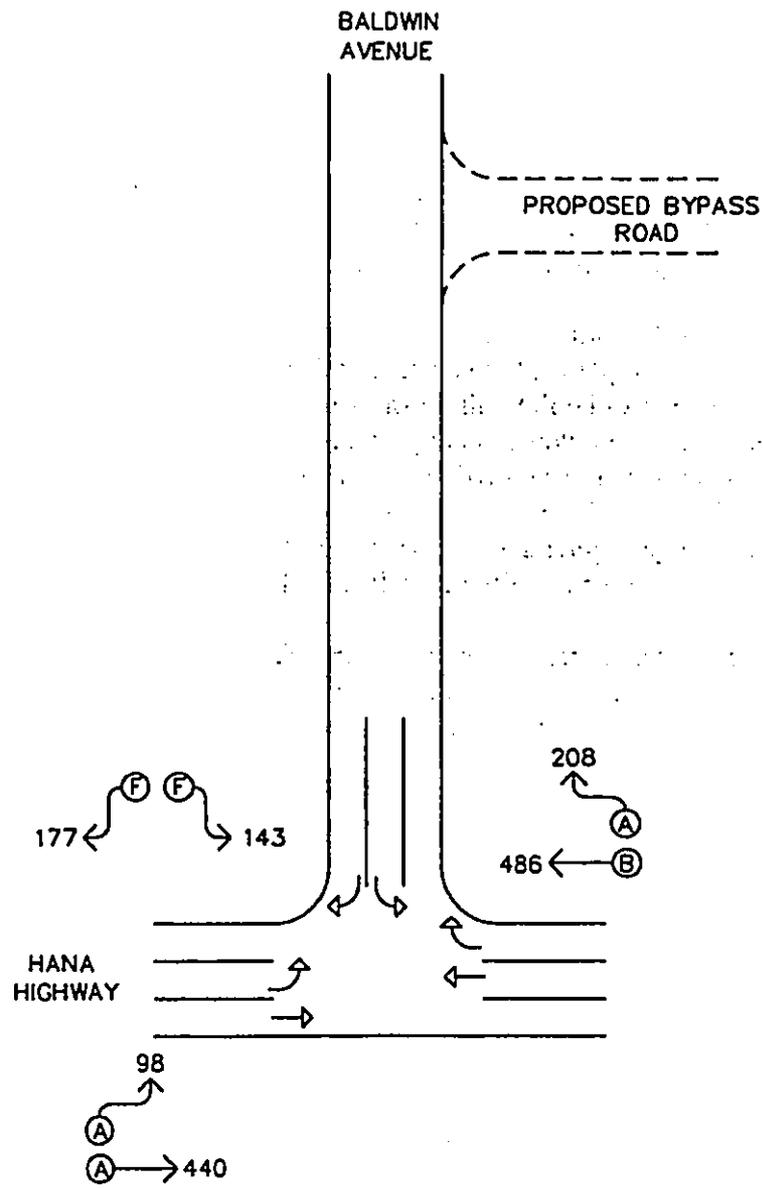
## **PROJECTED PM PEAK HOUR TRAFIC CONDITIONS**

Based on the field data, some of the traffic destined to areas in the vicinity of Paia Town were captured within the town itself and may be considered as internal trips. There are three primary routes that can be used to enter Paia Town and thus contribute to this capture. These include northbound Baldwin Avenue, eastbound Hana Highway, and westbound Hana Highway. To model diverted trips as a result of the bypass road, incoming traffic demands were assigned to the available routes recognizing that some of the trips will continue to use the existing available routes. The bypass road would primarily impact trips associated with the eastbound Hana Highway right-turn movement to southbound Baldwin Avenue route. The diversion would represent approximately 60% of the incoming trips, with the balance or

- LEGEND
- 90  TRAFFIC MOVEMENT VOLUME (VPH)
  -  LANE USAGE
  - Ⓐ LANE GROUP LEVEL OF SERVICE



DATE OF COUNT: MAY 24, 2004



PAIA BYPASS ROAD

EXISTING PM PEAK HOUR OF TRAFFIC

FIGURE  
2

40% of the incoming trips utilizing the existing route. In addition, passerby trips, which are defined as trips traversing the vicinity but not captured within Paia Town were redistributed to the bypass road. Examples of this trip type include those destined to Makawao, Haiku, and other areas outside of Paia Town. It was assumed that since motorists tend to travel along the most efficient route, 50% of the passerby trips were assumed to use the bypass route while the remaining 50% will continue to use the existing route. The equalization of traffic demands is a typical characteristic of roadway networks with bypass roads.

Figure 3 shows the projected PM peak hour traffic volumes and operating conditions. The reduction in the traffic demand at the intersection of Hana Highway and Baldwin Avenue would not significantly improve intersection operations. The left-turn and right-turn movements on Baldwin Avenue are anticipated to continue operating at LOS "F" conditions while the southbound and northbound movements on Hana Highway are anticipated to continue operating at LOS "B" or better. However, the existing queuing along the eastbound approach of Hana Highway would be significantly reduced due to the redistribution of vehicles between eastbound Hana Highway and the proposed bypass road. With these queue reductions, the traffic signal system may be able to clear most queues on Hana Highway after each traffic signal cycle change.

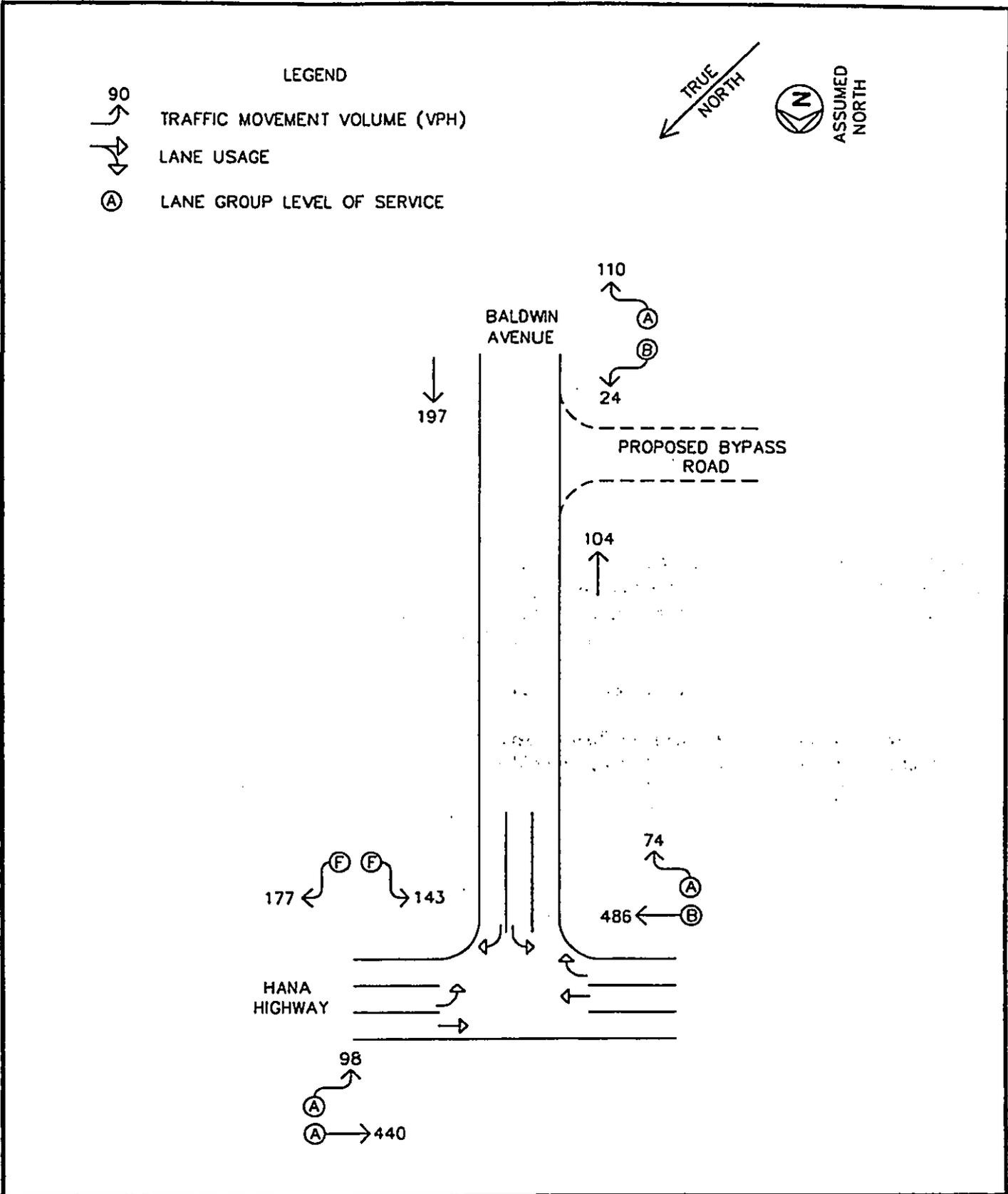
The intersection of Baldwin Avenue and the proposed bypass road would operate fairly well under unsignalized conditions. The left-turn and right-turn movements on the bypass road would operate at LOS "B" and LOS "A" conditions, respectively, during the projected PM peak hour of traffic.

#### **TRAFFIC ASSESSMENT**

The intersection of Hana Highway and Baldwin Avenue may continue to operate at undesirable traffic conditions even with the construction of the proposed bypass if the existing traffic signal phase timing is not modified. With the reduction in traffic demand at the intersection, the traffic signal timing may need to be adjusted to efficiently service the anticipated traffic volumes entering the intersection.

Figure 4 shows the projected traffic volumes and operating conditions with modifications to the traffic signal timing at the intersection of Hana Highway and Baldwin Avenue. The reduction of traffic demands allows the reduction of the traffic signal cycle length, thus resulting in an efficient intersection operation. All of the traffic movements at the intersection of Hana Highway and Baldwin Avenue are expected to operate at LOS "C" or better with the proposed bypass and traffic signal timing modifications.

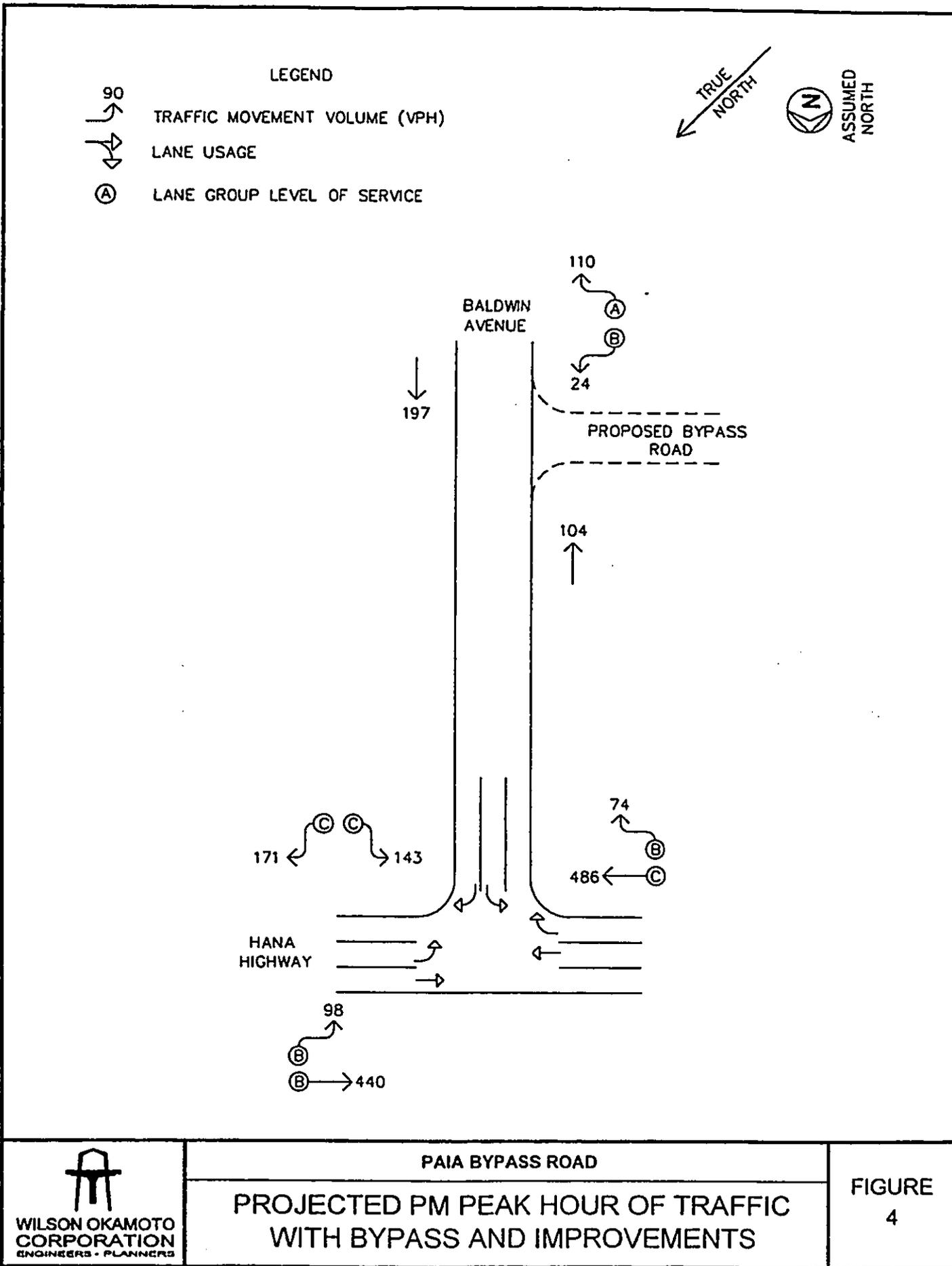
Queue lengths on the northbound Baldwin Avenue approach of the intersection would be significantly reduced with the traffic signal timing modifications at the intersection with Hana Highway. The queue length reduction may provide easier ingress and egress to and from parking areas off of Baldwin Avenue as well as improve existing on-street parking maneuvers.



**PAIA BYPASS ROAD**

**PROJECTED PM PEAK HOUR OF TRAFFIC WITH BYPASS**

**FIGURE 3**



The proposed bypass connection on Baldwin Avenue is expected to be located near the existing Paia Post Office driveway. However, the critical demand at the intersection is the eastbound right-turn movement from the bypass road to southbound Baldwin Avenue and, as such, would not significantly impact the post office driveway operations. Furthermore, since the proposed bypass road would be located on the opposite side of the post office from the driveway, there should be adequate separation between turning maneuvers. Therefore, from a traffic capacity standpoint, the location of the bypass road connection at Baldwin Avenue should not have a significant impact to the post office driveway operations provided that sufficient sight distances are provided at the bypass road and Baldwin Avenue intersection. The design of the proposed bypass road connection on Baldwin Avenue may also consider strictly prohibiting on-street parking on Baldwin Avenue fronting the post office to further increase driver visibility.

#### **CONCLUSION**

With the aforementioned modifications to the existing traffic signal system timing at the intersection of Hana Highway and Baldwin Avenue, the proposed bypass road is expected to significantly improve traffic operations along Hana Highway, through Paia Town, and at the intersection of Hana Highway and Baldwin Avenue. The bypass would provide an alternate travel route for motorists currently navigating congested intersections and roads leading to and in Paia Town. As a result, the existing vehicle queue lengths along eastbound Hana Highway and northbound Baldwin Avenue are expected to reduce considerably thus providing more convenient access to areas within Paia Town, shops, eateries, and other establishments. In addition, the reduction in traffic demand in these areas should also improve pedestrian safety and improve on-street parking maneuvers along Baldwin Avenue.

---

*Turning Movement Count Data*  
*24-hour Count Data*  
*Existing Capacity Analysis Calculations*  
*Projected Capacity Analysis Calculations With Bypass*  
*Projected Capacity Analysis Calculations With Bypass and Signal Improvements*

---





**Wilson Okamoto**  
 1907 S. Beretania St., #400  
 Honolulu, HI 96826

Site: I  
 Date: 05/24/04

Title1 : Paia Bypass  
 Title2 : SUNNY  
 Title3 : 0234

Interval	WB		EB		Combined		Day	Monday
	AM	PM	AM	PM	AM	PM		
12:00	.	.	.	.	.	.		
12:15	.	.	.	.	.	.		
12:30	.	.	.	.	.	.		
12:45	.	0	.	0	.	0		
01:00	.	48 200	.	47 189	.	95 389		
01:15	.	46	.	54	.	100		
01:30	.	48	.	44	.	92		
01:45	.	58	.	44	.	102		
02:00	.	50 233	.	56 201	.	106 434		
02:15	.	77	.	52	.	129		
02:30	.	60	.	52	.	112		
02:45	.	46	.	41	.	87		
03:00	.	45 206	.	42 204	.	87 410		
03:15	.	52	.	48	.	100		
03:30	.	70	.	56	.	126		
03:45	.	39	.	58	.	97		
04:00	.	60 191	.	52 210	.	112 401		
04:15	.	39	.	44	.	83		
04:30	.	49	.	56	.	105		
04:45	.	43	.	58	.	101		
05:00	.	54 207	.	58 220	.	112 427		
05:15	.	52	.	62	.	114		
05:30	.	51	.	54	.	105		
05:45	.	50	.	46	.	96		
06:00	.	35 121	.	60 212	.	95 333		
06:15	.	24	.	54	.	78		
06:30	.	37	.	56	.	93		
06:45	.	25	.	42	.	67		
07:00	.	36 129	.	46 171	.	82 300		
07:15	.	28	.	42	.	70		
07:30	.	41	.	39	.	80		
07:45	.	24	.	44	.	68		
08:00	.	24 78	.	33 133	.	57 211		
08:15	.	24	.	48	.	72		
08:30	.	16	.	24	.	40		
08:45	.	14	.	28	.	42		
09:00	.	16 52	.	24 108	.	40 160		
09:15	.	13	.	31	.	44		
09:30	.	13	.	26	.	39		
09:45	.	10	.	27	.	37		
10:00	.	20 44	.	20 73	.	40 117		
10:15	.	11	.	20	.	31		
10:30	.	6	.	20	.	26		
10:45	.	7	.	13	.	20		
11:00	.	6 19	.	13 54	.	19 73		
11:15	.	2	.	16	.	18		
11:30	.	5	.	12	.	17		
11:45	.	6	.	13	.	19		
Totals	0	1,480	0	1,775	0	3,255		
Split%	.	45.5	.	54.5				
Day Totals		1,480		1,775		3,255		
Day Splits		45.5		54.5				
Peak Hour	.	01:45	.	04:30	.	01:45		
Volume	.	245	.	234	.	449		
Factor	.	0.80	.	0.94	.	0.87		

**Wilson Okamoto**  
 1907 S. Beretania St., #400  
 Honolulu, HI 96826

Site: 1  
 Date: 05/25/04

Title1 : Paia Bypass  
 Title2 : SUNNY  
 Title3 : 0234

Interval	WB		EB		Combined		Day:	Tuesday
	AM	PM	AM	PM	AM	PM		
Begin								
12:00	5	42	8	26	13	68		354
12:15	5	54	9	36	14	90		
12:30	2	47	6	50	8	97		
12:45	1	48	2	51	3	99		
01:00	2	0	8	0	10	0	43	
01:15	1	.	9	.	10	.		
01:30	8	.	7	.	15	.		
01:45	4	.	4	.	8	.		
02:00	4	11	8	16	12	27		
02:15	2	.	2	.	4	.		
02:30	4	.	2	.	6	.		
02:45	1	.	4	.	5	.		
03:00	1	7	1	9	2	16		
03:15	1	.	2	.	3	.		
03:30	2	.	3	.	5	.		
03:45	3	.	3	.	6	.		
04:00	3	24	2	7	5	31		
04:15	4	.	2	.	6	.		
04:30	9	.	1	.	10	.		
04:45	8	.	2	.	10	.		
05:00	17	75	3	24	20	99		
05:15	13	.	3	.	16	.		
05:30	24	.	9	.	33	.		
05:45	21	.	9	.	30	.		
06:00	18	135	7	101	25	236		
06:15	29	.	28	.	57	.		
06:30	42	.	24	.	66	.		
06:45	46	.	42	.	88	.		
07:00	58	305	51	200	109	505		
07:15	76	.	44	.	120	.		
07:30	81	.	55	.	136	.		
07:45	90	.	50	.	140	.		
08:00	81	251	28	122	109	373		
08:15	65	.	36	.	101	.		
08:30	53	.	30	.	83	.		
08:45	52	.	28	.	80	.		
09:00	52	220	30	153	82	373		
09:15	57	.	35	.	92	.		
09:30	57	.	44	.	101	.		
09:45	54	.	44	.	98	.		
10:00	39	191	37	164	76	355		
10:15	52	.	59	.	111	.		
10:30	50	.	32	.	82	.		
10:45	50	.	36	.	86	.		
11:00	40	166	44	173	84	339		
11:15	42	.	32	.	74	.		
11:30	38	.	53	.	91	.		
11:45	46	.	44	.	90	.		
Totals	1,413	191	1,022	163	2,435	354		
Split%	58.0	54.0	42.0	46.0				
Day Totals		1,604		1,185		2,789		
Day Splits		57.5		42.5				
Peak Hour	07:15	12:00	07:00	12:00	07:00	12:00		
Volume	328	191	200	163	505	354		
Factor	0.91	0.88	0.91	0.80	0.90	0.89		

HCS2000: Signalized Intersections Release 4.1b

Analyst: IQ  
 Agency: Wilson Okamoto Corporation  
 Date: 6/15/2004  
 Period: PM Peak  
 Project ID: Paia Bypass Road  
 E/W St: Hana Hwy

Inter.: Baldwin Ave/Hana Hwy  
 Area Type: All other areas  
 Jurisd:  
 Year : 2004  
 N/S St: Baldwin Ave

SIGNALIZED INTERSECTION SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	0	1	1	1	1	0	1	0	1	0	0	0
LGConfig		T	R	L	T		L		R			
Volume		486	208	98	440		143		177			
Lane Width		12.0	12.0	12.0	12.0		12.0		12.0			
RTOR Vol			40						18			

Duration 1.00 Area Type: All other areas  
 Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left					NB Left	A		
Thru					Thru			
Right		A			Right	A		
Peds					Peds			
WB Left		A	A		SB Left			
Thru		A	A		Thru			
Right					Right			
Peds					Peds			
NB Right					EB Right			
SB Right					WB Right			
Green		15.0	185.0			40.0		
Yellow		0.0	4.0			4.0		
All Red		0.0	1.0			1.0		

Cycle Length: 250.0 secs

Intersection Performance Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group		Approach	
			v/c	g/C	Delay	LOS	Delay	LOS
Eastbound								
T	1406	1900	0.36	0.74	11.6	B	11.1	B
R	1195	1615	0.14	0.74	9.5	A		
Westbound								
L	722	1805	0.16	0.80	6.2	A		
T	1520	1900	0.34	0.80	7.0	A	6.8	A
Northbound								
L	289	1805	0.56	0.16	99.5	F		
R	258	1615	0.70	0.16	108.0	F	104.0	F
Southbound								

Intersection Delay = 28.9 (sec/veh) Intersection LOS = C

HCS2000: Signalized Intersections Release 4.1b

Analyst: IQ  
 Agency: Wilson Okamoto Corporation  
 Date: 6/15/2004  
 Period: PM Peak  
 Project ID: Paia Bypass Road  
 E/W St: Hana Hwy

Inter.: Baldwin Ave/Hana Hwy  
 Area Type: All other areas  
 Jurisd:  
 Year : Projected  
 N/S St: Baldwin Ave

SIGNALIZED INTERSECTION SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	0	1	1	1	1	0	1	0	1	0	0	0
LGConfig		T	R	L	T		L		R			
Volume		486	74	98	440		143		177			
Lane Width		12.0	12.0	12.0	12.0		12.0		12.0			
RTOR Vol			44						100			

Duration 1.00 Area Type: All other areas  
 Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left					NB Left	A		
Thru			A		Thru			
Right			A		Right	A		
Peds					Peds			
WB Left		A	A		SB Left			
Thru		A	A		Thru			
Right					Right			
Peds					Peds			
NB Right					EB Right			
SB Right					WB Right			
Green		15.0	185.0			40.0		
Yellow		0.0	4.0			4.0		
All Red		0.0	1.0			1.0		

Cycle Length: 250.0 secs

Intersection Performance Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group Delay LOS		Approach Delay LOS	
			v/c	g/C				
<b>Eastbound</b>								
T	1480	2000	0.34	0.74	11.4	B	11.2	B
R	1258	1700	0.02	0.74	8.6	A		
<b>Westbound</b>								
L	728	1900	0.16	0.80	6.1	A		
T	1600	2000	0.32	0.80	6.8	A	6.7	A
<b>Northbound</b>								
L	304	1900	0.54	0.16	98.4	F		
R	272	1700	0.32	0.16	93.7	F	96.7	F
<b>Southbound</b>								

Intersection Delay = 24.5 (sec/veh) Intersection LOS = C



HCS2000: Signalized Intersections Release 4.1b

Analyst: IQ  
 Agency: Wilson Okamoto Corporation  
 Date: 6/15/2004  
 Period: PM Peak  
 Project ID: Paia Bypass Road  
 E/W St: Hana Hwy

Inter.: Baldwin Ave/Hana Hwy  
 Area Type: All other areas  
 Jurisd:  
 Year : Projected w/Improvements  
 N/S St: Baldwin Ave

SIGNALIZED INTERSECTION SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	0	1	1	1	1	0	1	0	1	0	0	0
LGConfig		T	R	L	T		L		R			
Volume		486	74	98	440		143		177			
Lane Width		12.0	12.0	12.0	12.0		12.0		12.0			
RTOR Vol			44						100			

Duration	1.00	Area Type: All other areas							
Signal Operations									
Phase Combination	1	2	3	4	5	6	7	8	
EB Left					NB Left	A			
Thru					Thru	A			
Right					Right	A			
Peds					Peds				
WB Left		A	A		SB Left				
Thru		A	A		Thru				
Right					Right				
Peds					Peds				
NB Right					EB Right				
SB Right					WB Right				
Green		5.0	56.0			49.0			
Yellow		0.0	4.0			4.0			
All Red		0.0	1.0			1.0			
Cycle Length: 120.0 secs									

Intersection Performance Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group		Approach	
			v/c	g/C	Delay	LOS	Delay	LOS
<b>Eastbound</b>								
T	933	2000	0.52	0.47	23.1	C	22.7	C
R	793	1700	0.04	0.47	17.4	B		
<b>Westbound</b>								
L	355	1900	0.28	0.51	17.8	B		
T	1017	2000	0.43	0.51	18.9	B	18.7	B
<b>Northbound</b>								
L	776	1900	0.18	0.41	22.8	C	22.6	C
R	694	1700	0.11	0.41	22.1	C		
<b>Southbound</b>								

Intersection Delay = 21.0 (sec/veh)      Intersection LOS = C

APPENDIX D  
DRAINAGE REPORTS



Sato & Associates, Inc.

Consulting Engineers

2046 S. King Street, Honolulu, Hawaii 96826  
OFFICES IN HONOLULU AND MAUI  
www.satoandassociates.com

Tel: (808) 955-4441  
Fax: (808) 942-2027

mailbox@satoandassociates.com

January 28, 2004

**RECEIVED**

JAN 30 2004

H KAWAHARA, P.E.

**RECEIVED**

JAN 29 2004

A&B PROPERTIES-MAUI

Mr. Hideo Kawahara  
A & B Properties, Inc.  
33 Lono Avenue, Suite 400  
Kahului, Maui, Hawaii 96732-1608

Dear Mr. Kawahara:

Subject: Paia Mini-Bypass Road  
Drainage Calculations

The following Hydrologic Calculations for both existing and developed storm runoff conditions are based on the rational method. Factors used in the calculations are taken from the "Rules for the Design of Storm Drainage Facilities in the County of Maui."

The following factors were used:

A. RECURRENCE INTERVAL

50-Year, 1-Hour Storm  $I = 2.5"$  (Plate 7)

B. TIME OF CONCENTRATION (Overload Flow Chart - Plate 1)

$T_{C_{EXIST}} = 25$  min.  
 $T_{C_{DEV}} = 25$  min.

C. RAINFALL INTENSITY (Intensity Duration 1 Hour Rainfall Curves - Plate 2)

$i_{EXIST} = 3.7$  in./hr.  
 $i_{DEV} = 3.7$  in./hr.

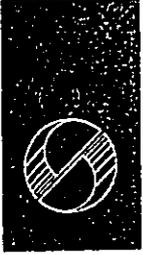
D. RUNOFF COEFFICIENT

C - Determined from Table 2, as follows:

Existing Conditions:

<u>Type of Drainage Area</u>	<u>Runoff Coefficient C</u>	<u>Area (Acres)</u>
Unimproved	0.30	3.16

$C_{EXIST} = 0.30$



Developed Conditions:

<u>Type of Drainage Area</u>	<u>Runoff Coefficient C</u>
Asphalt Pavement	0.95
Unimproved	0.30

Weighted C:

$$C_{AVG} = \frac{(0.47)(0.95) + (2.69)(0.30)}{3.16} = 0.40$$

E. STORM WATER RUNOFF

Existing Condition:

$$Q = (0.30)(3.7)(3.16) = 3.51 \text{ CFS}$$

Developed Condition:

$$Q = (0.40)(3.7)(3.16) = 4.68 \text{ CFS}$$

Based on the above calculations and the attached plans, it appears that the increase in storm water will have negligible impact to the existing holding basin adjacent to Hana Highway.

We trust that this meets with your approval. Should you have any questions, please do not hesitate to call.

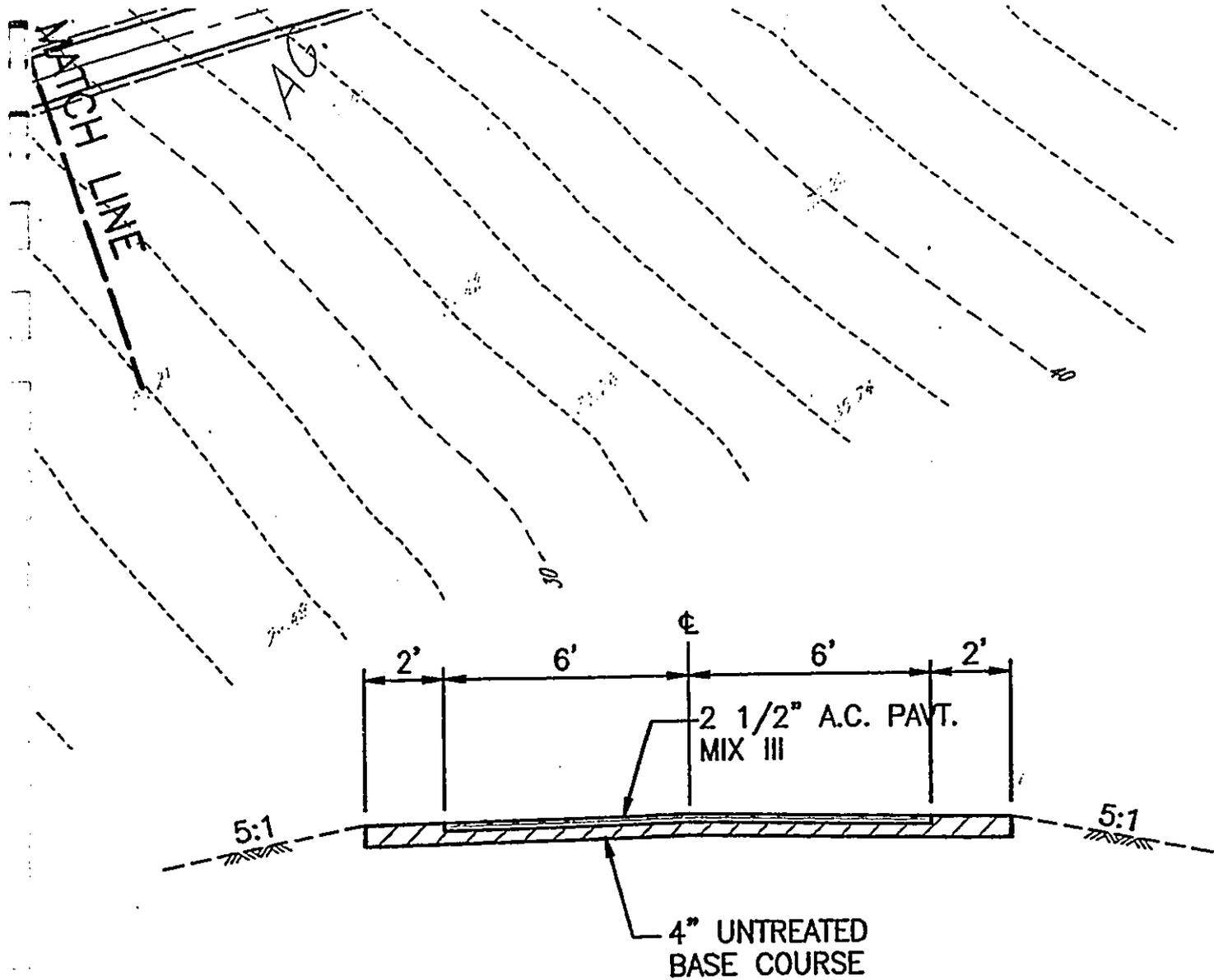
Very truly yours,

SATO & ASSOCIATES, INC.

Daniel S. Miyasato  
Vice President

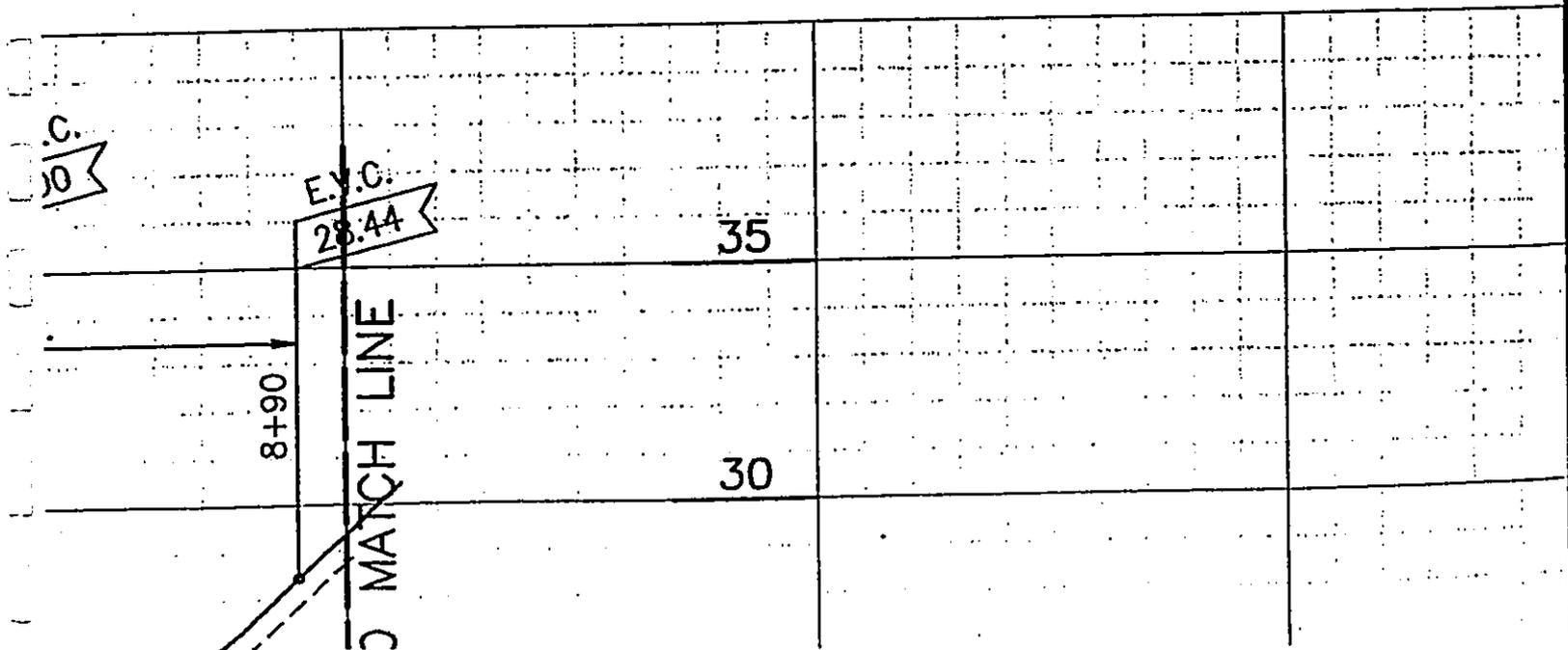
DSM/js

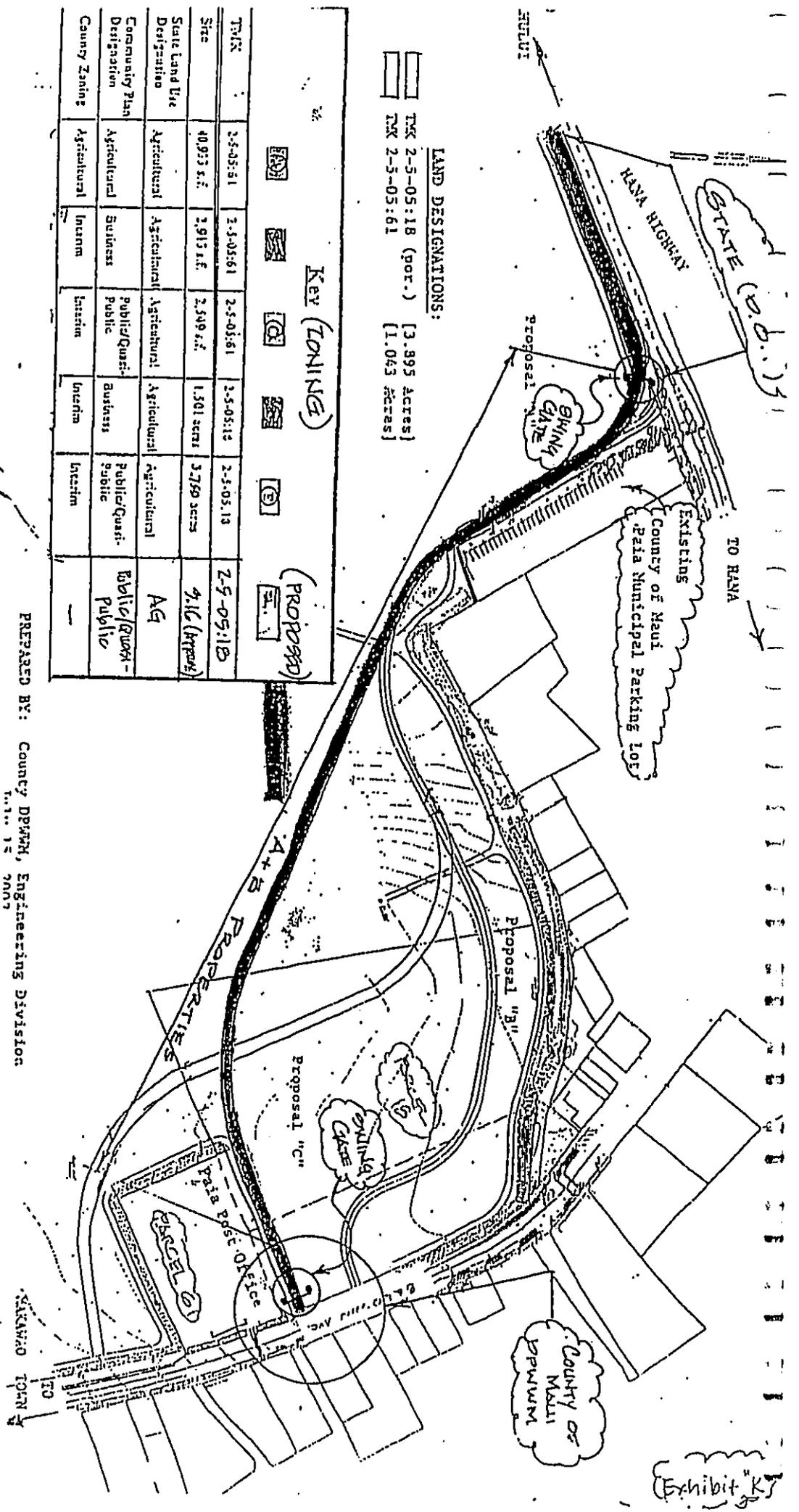
Attachments



**TYPICAL SECTION**

SCALE: 1/4"=1'-0"





LAND DESIGNATIONS:  
 TKR 2-5-05:18 (por.) [3,895 acres]  
 TKR 2-5-05:61 [1,043 acres]

TRAK	2-5-05:61	2-5-05:61	2-5-05:61	2-5-05:12	2-5-05:13	2-5-05:18
Site	40,953 s.f.	2,913 s.f.	2,349 s.f.	1,501 sqm	3,759 sqm	9.16 (approx)
State Land Use Designation	Agricultural	Agricultural	Agricultural	Agricultural	Agricultural	AG
Community Plan Designation	Agricultural	Business	Public/Quasi-Public	Business	Public/Quasi-Public	Public/Quasi-Public
County Zoning	Agricultural	Interim	Interim	Interim	Interim	

PREPARED BY: County DEPTM, Engineering Division  
 11/11/15 2007

(Exhibit "K")

**HANA HIGHWAY SHOULDER LANE  
ALAWAI ROAD TO PAIA  
PROJ. NO. 36B-02-02**

**DRAINAGE REPORT**

**INTRODUCTION**

This report has been prepared to evaluate both the existing drainage condition and the proposed drainage plan for the subject project.

**PROPOSED PROJECT**

**A. Location**

The subject project area is located in Paia, on the island of Maui along Hana Highway between Alawai Road (Baldwin Park entrance) and Paia Town.

**B. Project Description**

The proposed project will consist of a ten (10) foot wide asphalt paved shoulder that will be constructed in conjunction with the County and A & B properties Inc., to build a mini bypass around Paia town. This bypass will be a one lane road from Hana Highway (before County parking lot) to Baldwin Avenue (above the Post Office). Our project will involve widening the existing Hana Highway four (4) feet along the ocean side and six (6) feet along the mountainside for a total of ten (10) feet. Other work involved will be relocation of existing regulatory and warning signs, pavement marking and striping.

**EXISTING CONDITIONS**

According to our calculation, approximately 8.8 cfs of pre-development runoff is being generated by the project site during a 50 year – 1 hour storm. This surface runoff is conveyed in a westerly direction along the grassed shoulders to an existing box culvert in the proximity of the Baldwin Park entrance. This runoff eventually outlets to the ocean.

**DRAINAGE PLAN**

The design of the shoulder lane project will include roadway widening that will include minimal alteration to the existing drainage pattern. According to our calculations, the post development runoff generated from the project site is approximately 10.4 cfs, for a 50 year – 1hour recurrence interval. This translates to a net increase of approximately 1.6 cfs. This post development runoff will continue to flow to the existing box culvert at the Baldwin Park Entrance.

**Rational Formula Used:  $Q = CIA$**

Where Q = rate of flow  
A = area (acres)  
I = rainfall intensity for a duration of time of concentration  
C = runoff coefficient

**Existing Conditions**

(Existing roadway and shoulder)

Paved Area = 2.05  
Grassed Shoulders = 1.79  
Total Area = 3.84

Runoff Coefficient "C" - (AC Pavement) = 0.95

Runoff Coefficient "C" - (Grassy Areas)

Infiltration	Medium	0.07
Relief	Flat	0.00
Vegetal Cover	High	0.00
Development	Roadway	<u>0.55</u>
		0.62

Solving for Weighted Runoff coefficient ( $C_w$ )

$$\begin{aligned} C_w &= C_{\text{paved}}(\text{Area}_{\text{paved}}) + C_{\text{grass}}(\text{Area}_{\text{grass}}) / \text{Total Area} \\ &= 0.95(2.05) + 0.62(1.79) / 3.84 \\ &= 0.79 \end{aligned}$$

50 year 1 hour rainfall = 2.5 inches

L = 2,788 feet  
S = 0.2%  
 $T_c$  = 45 min  
"I" = 2.9 in./hr.  
Q = (0.79) (2.9) (3.84)  
Q = 8.8 cfs runoff before widening

**Developed Condition**

Paved Area = 2.67  
Shoulder = 1.17  
Total Area = 3.84

Runoff Coefficient "C" - (AC Pavement) = 0.95

Runoff Coefficient "C" - (Grassy Areas)

Infiltration	Medium	0.07
Relief	Flat	0.00
Vegetal Cover	High	0.00
Development	Roadway	<u>0.55</u>
		0.62

Solving for Weighted Runoff coefficient ( $C_w$ )

$$\begin{aligned}C_w &= C_{\text{paved}}(\text{Area}_{\text{paved}}) + C_{\text{grass}}(\text{Area}_{\text{grass}}) / \text{Total Area} \\ &= 0.95(2.67) + 0.62(1.17) / 3.84 \\ &= 0.85\end{aligned}$$

50 year 1 hour rainfall = 2.5 inches

L = 2,788 feet  
S = 0.2%  
T<sub>c</sub> = 37 min  
"I" = 3.2 in./hr.  
Q = (0.85) (3.2) (3.84)

**Q = 10.4 cfs runoff after widening (with 10 foot shoulder lane)**

Total increase in runoff = runoff after shoulder lane - runoff before shoulder lane

Q<sub>increase</sub> = 10.4 cfs - 8.8 cfs

Q<sub>increase</sub> = 1.6 cfs

## CONCLUSION

According to our calculations, the proposed project will generate an increase of approximately 1.6 cfs. Based upon the existing roadway section, the increase of roadway runoff should have a minimal effect to roadway and shoulder flooding. Additionally, the existing box culvert at the Baldwin Park entrance should have adequate capacity to convey the 1.6 cfs increase in surface runoff. Therefore, it is our professional opinion that the proposed development will not have any additional effect on the downstream properties or coastal ecosystem.

Table 1

GUIDE FOR THE DETERMINATION OF RUNOFF COEFFICIENTS FOR BUILT-UP AREAS\*

WATERSHED CHARACTERISTICS	EXTREME	HIGH	MODERATE	LOW
INFILTRATION	NEGLIGIBLE 0.20	SLOW 0.14	MEDIUM 0.07	HIGH 0.0
RELIEF	STEEP (> 25%) 0.08	HILLY (15-25%) 0.06	ROLLING (5-15%) 0.03	FLAT (0-5%) 0.0
VEGETAL COVER	NONE 0.07	POOR (< 10%) 0.05	GOOD (10-50%) 0.03	HIGH (50-90%) 0.0
DEVELOPMENT TYPE	INDUSTRIAL & BUSINESS 0.55	HOTEL-APARTMENT 0.45	RESIDENTIAL 0.40	AGRICULTURAL 0.15

\*NOTE: The design coefficient "c" must result from a total of the values for all four watershed characteristics of the site.

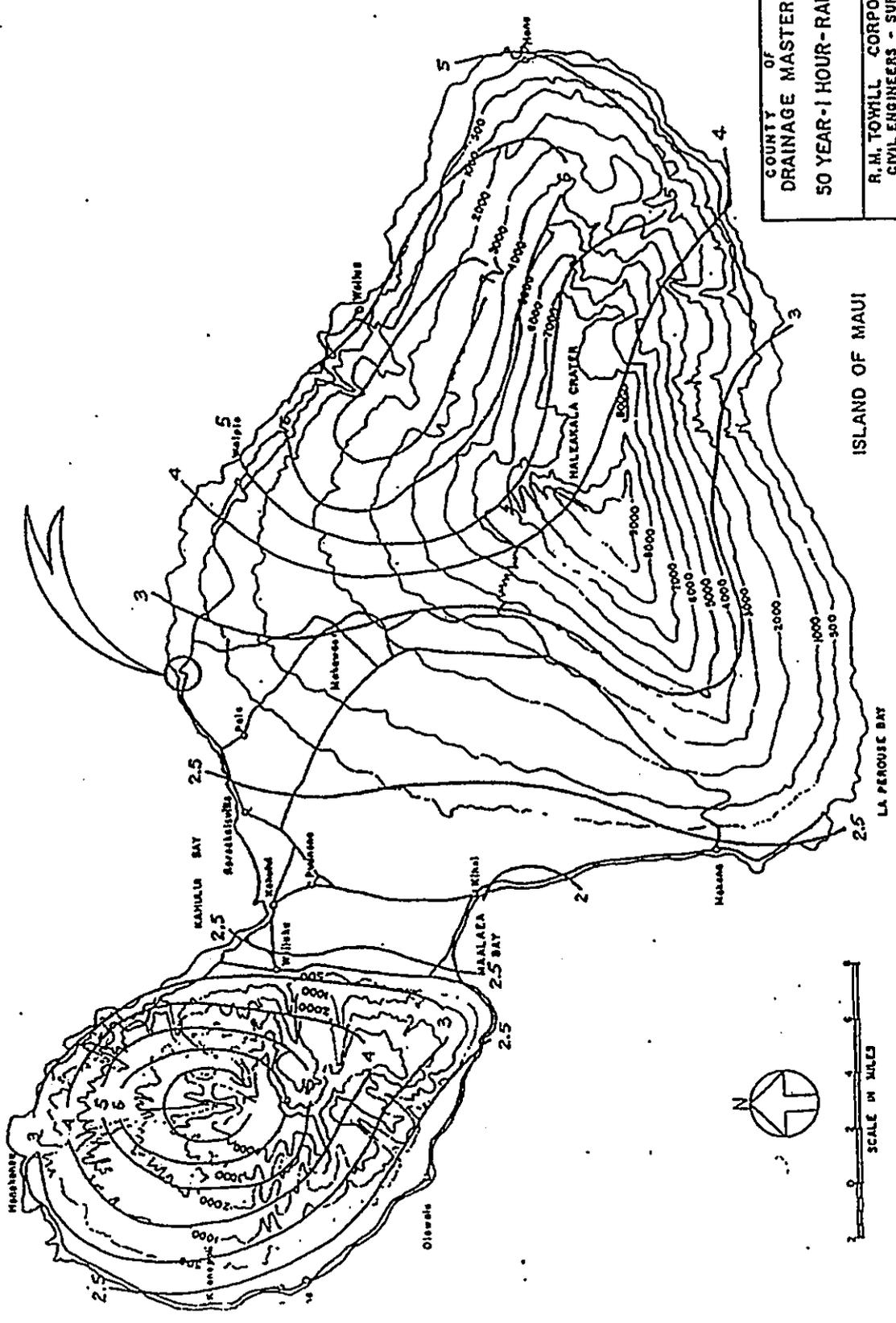
Table 2

RUNOFF COEFFICIENTS

Type of Drainage Area	Runoff Coefficient C
Parks, cemeteries	0.25
Playgrounds	0.35
Railroad yard areas	0.40
Unimproved areas	0.30
Streets:	<del>0.95</del>
Asphaltic	0.95
Concrete	0.85
Brick	0.85
Driveway and walks	0.95
Roofs	0.95
Lawns:	
Sandy soil, flat, 2%	0.10
Sandy soil, avg., 2-7%	0.15
Sandy soil, steep, 7%	0.20
Heavy soil, flat, 2%	0.17
Heavy soil, avg., 2-7%	0.22
Heavy soil, steep, 7%	0.35

Asphalt before widening - 2.05 AC  
total Area - 3.24

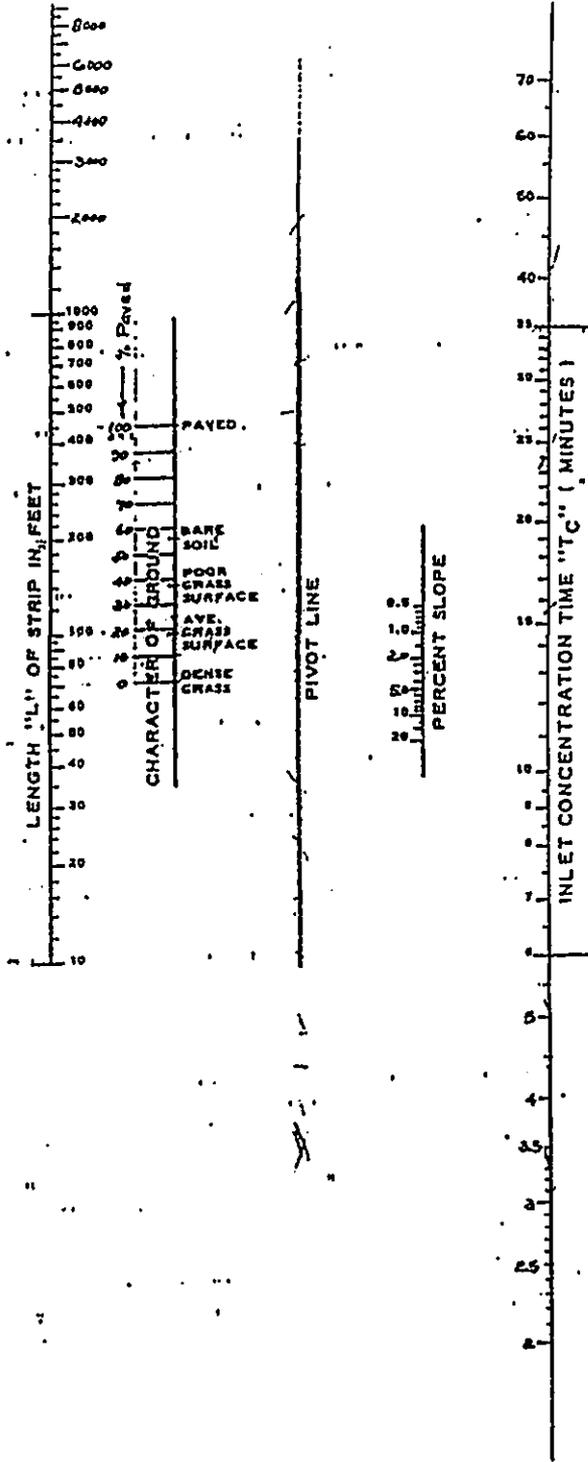
Asphalt after widening - 2.68 AC  
total Area - 3.24



COUNTY OF MAUI  
 DRAINAGE MASTER PLAN  
 50 YEAR-1 HOUR-RAINFALL  
 R.M. TOWILL CORPORATION  
 CIVIL ENGINEERS - SURVEYORS

PLATE 7

Plate 3  
Overland  
Flow  
Chart



10 20 30 40 50 60 70

APPENDIX E

MAP OF  
KALA/SUNNYSIDE ROAD

REDUCED IN  
FILE

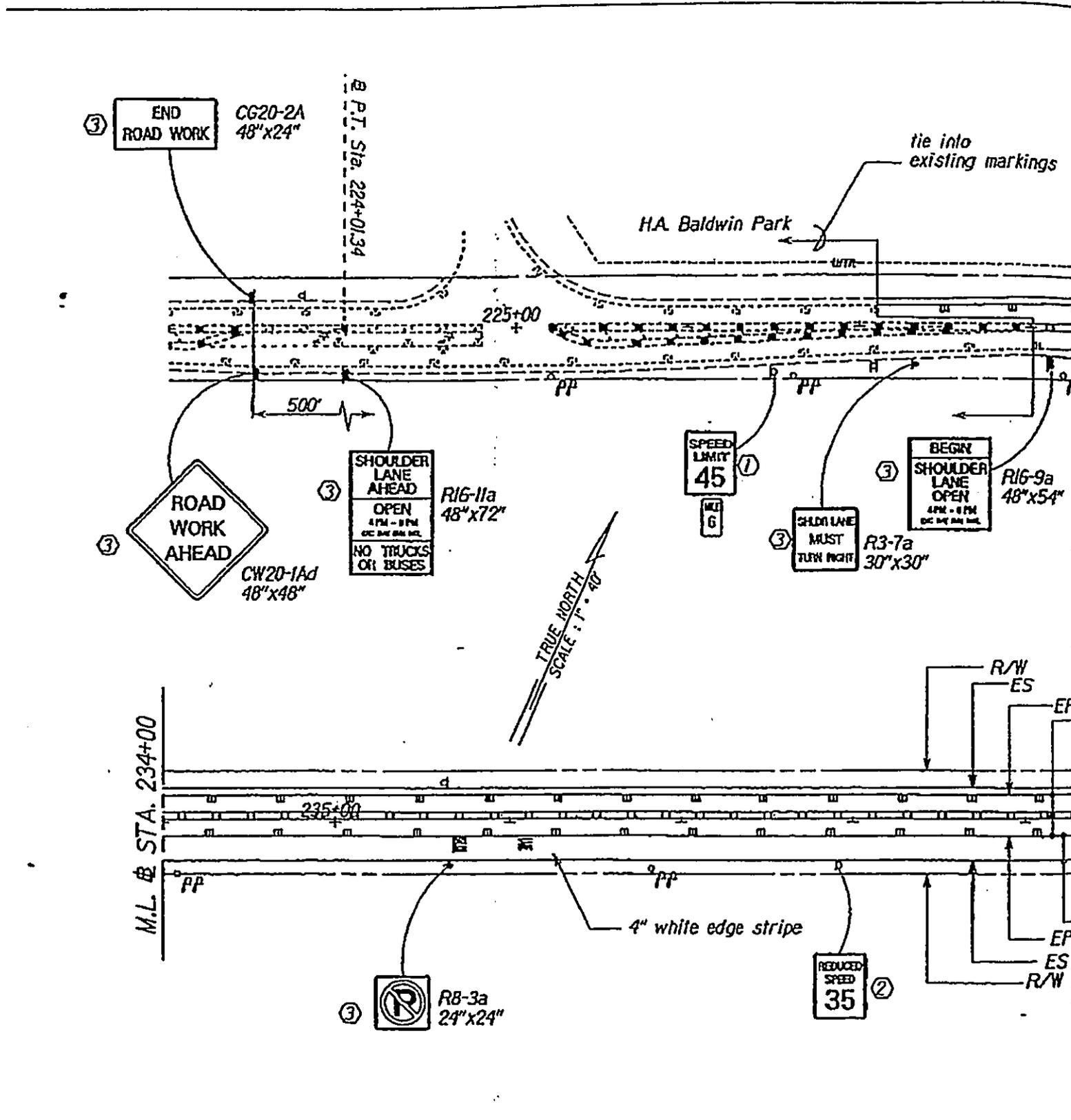
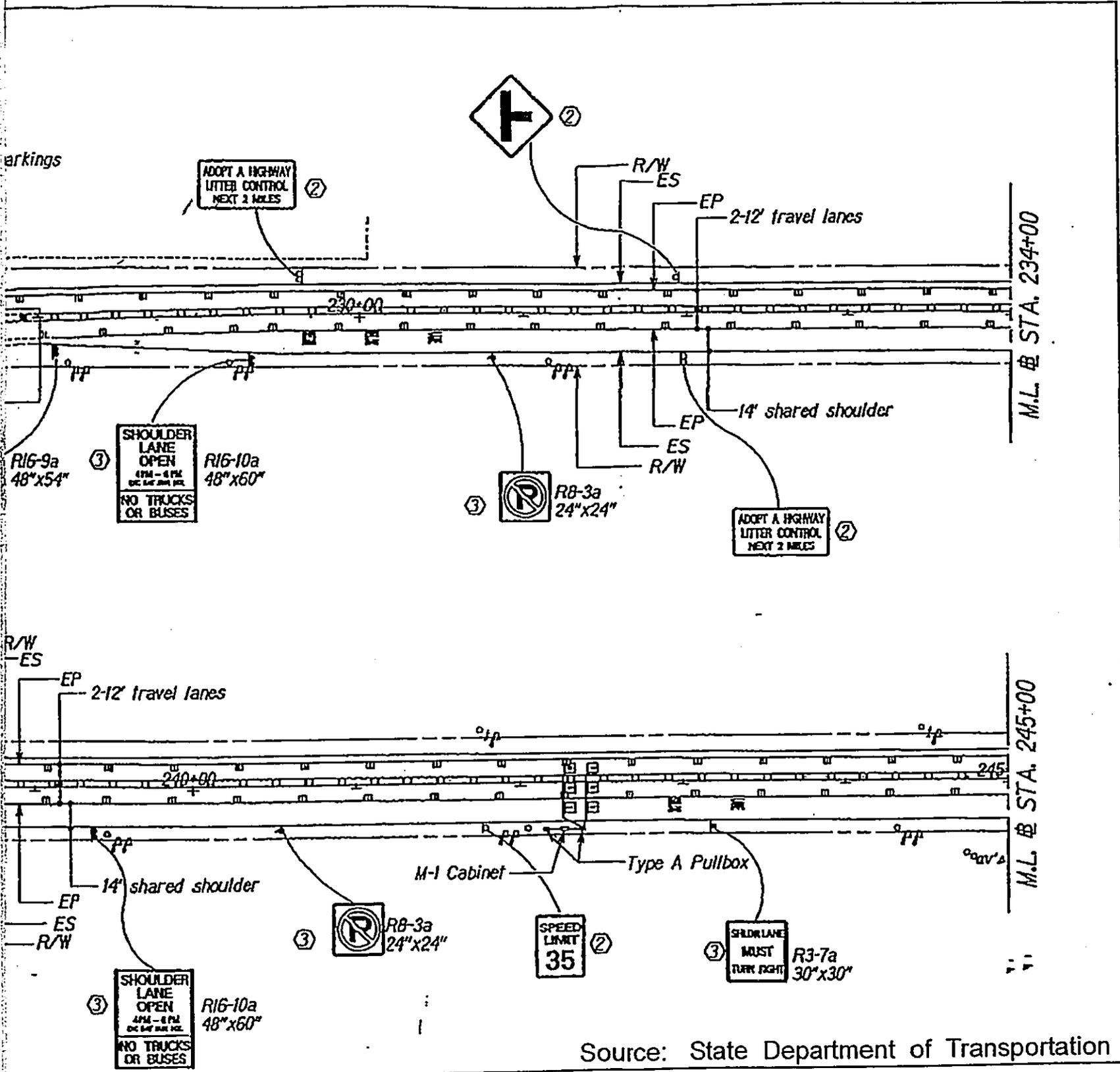


Figure 2

# PAIA MINI Signing and Pavem

RECEIVED AS FOLLOWS



Source: State Department of Transportation

# MINI-BYPASS Improvement Marking Plans

Not to scale



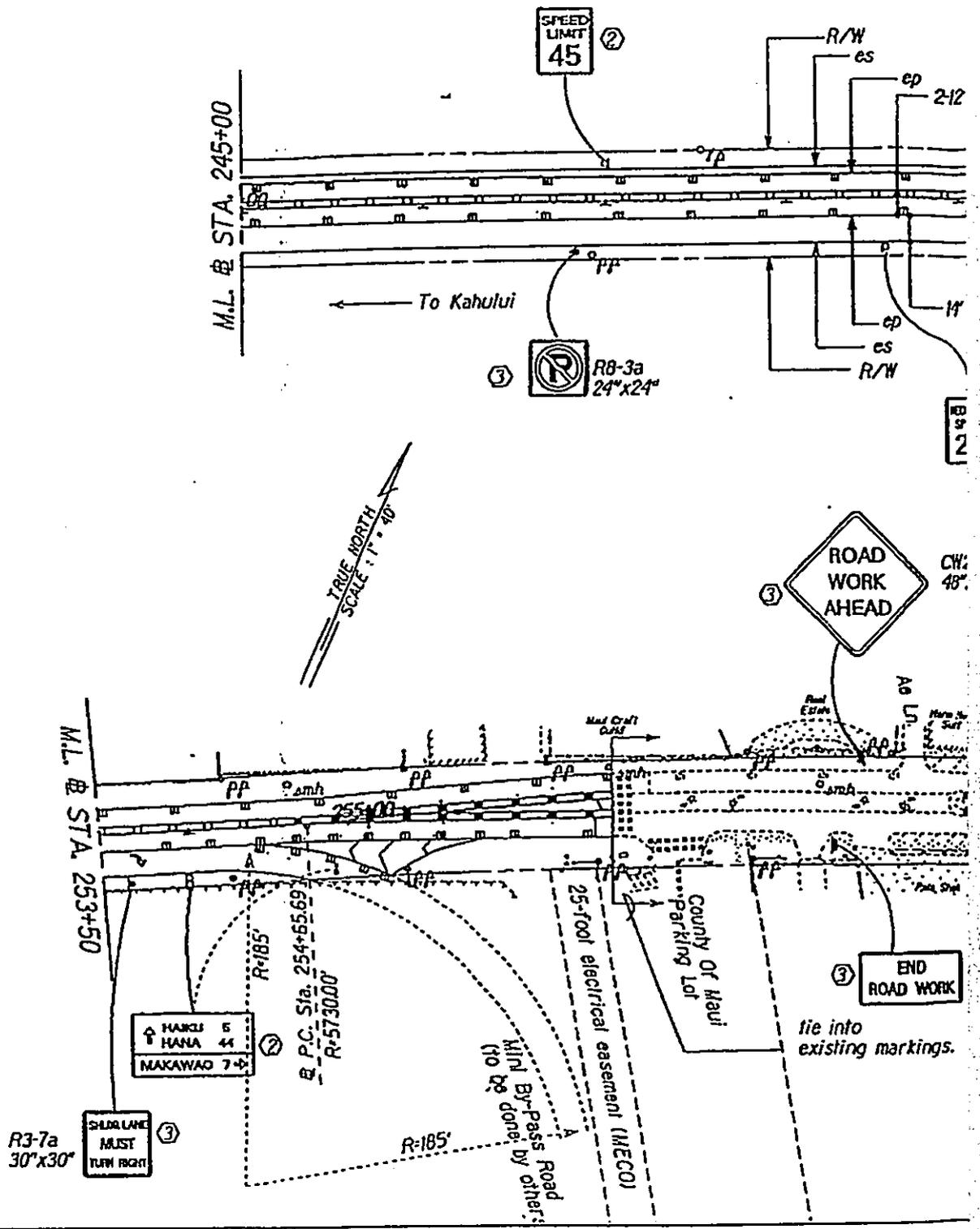
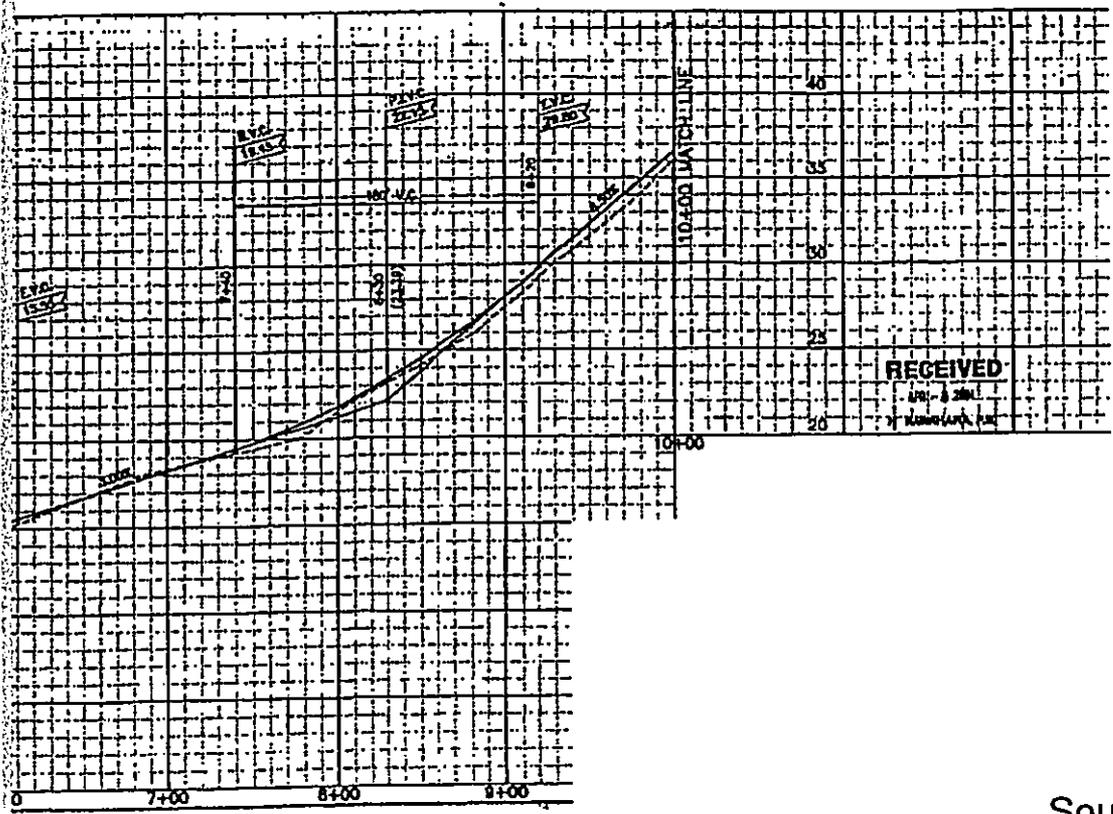
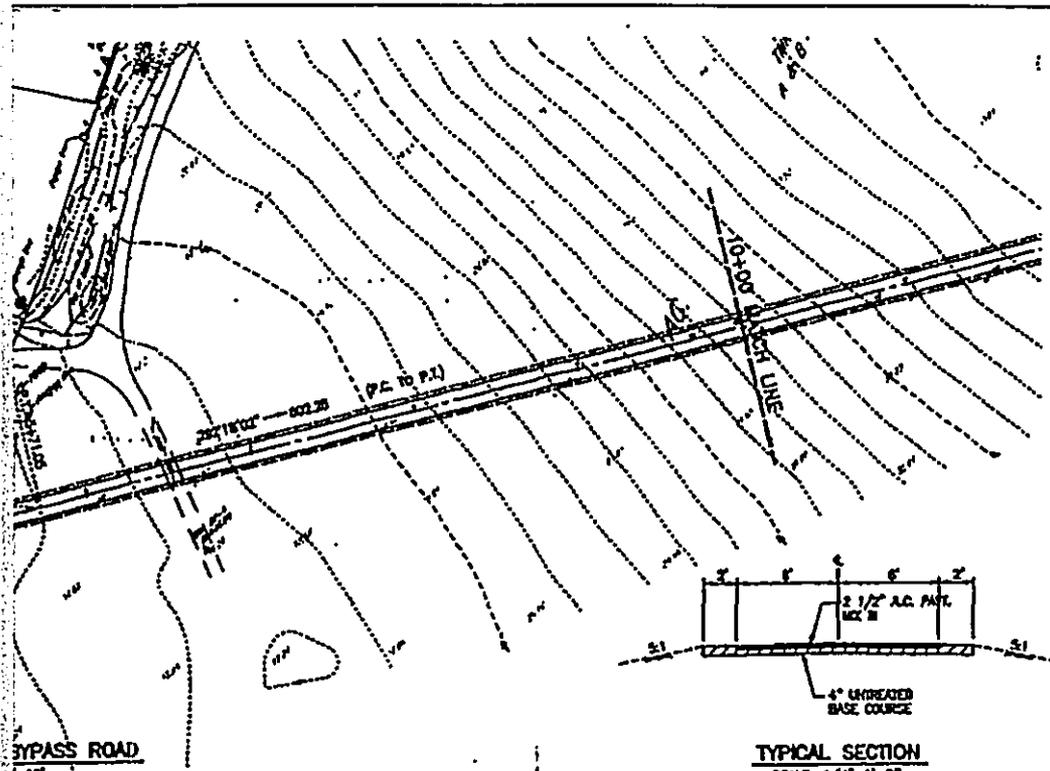


Figure 3 PAIA MINI-Signing and Pavement





RECEIVED AS FOLLOWS



Source: Sato and Associates, Inc.

# MINI-BYPASS and Profile

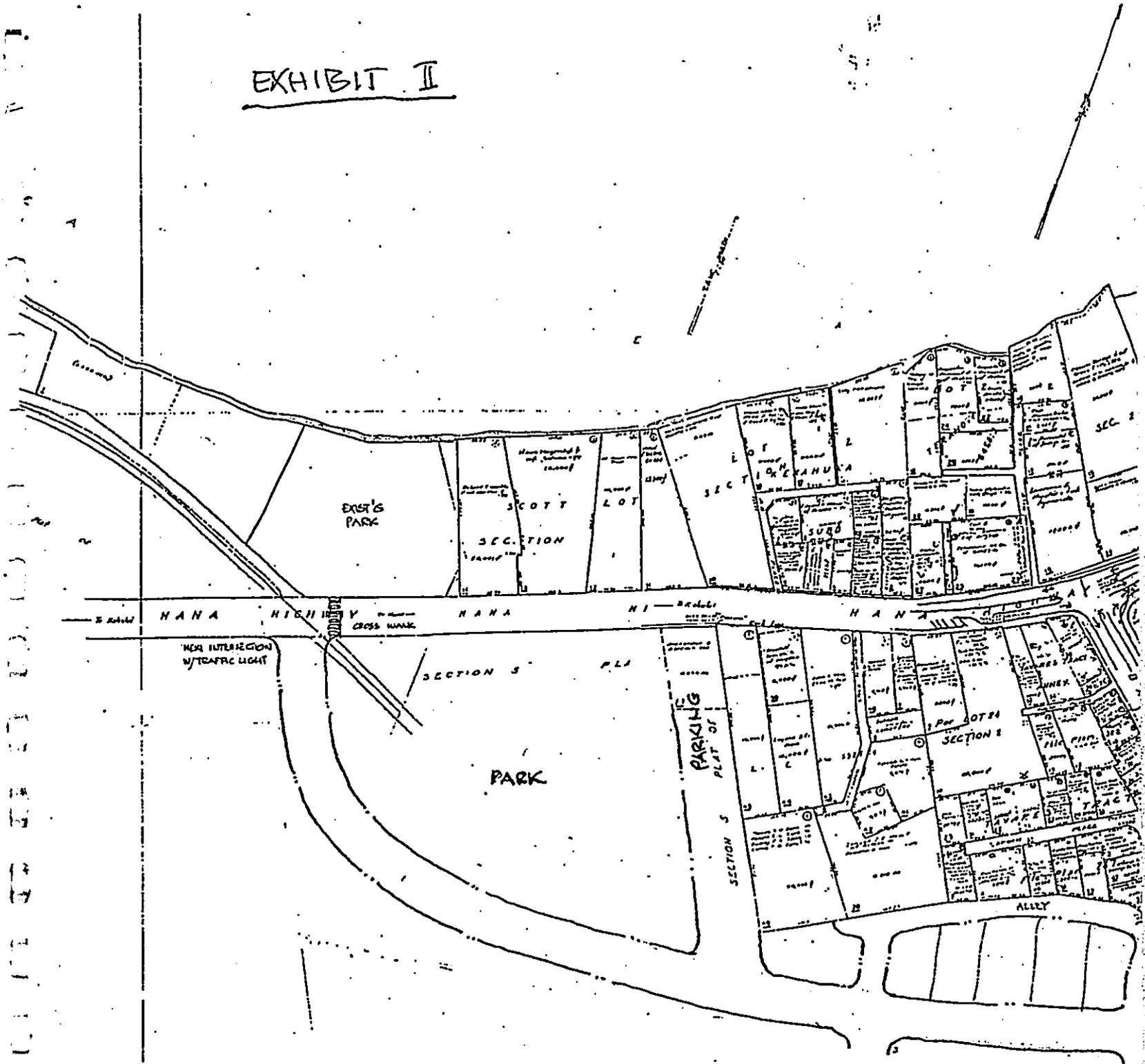
Not to scale





RECEIVED AS FOLLOWS

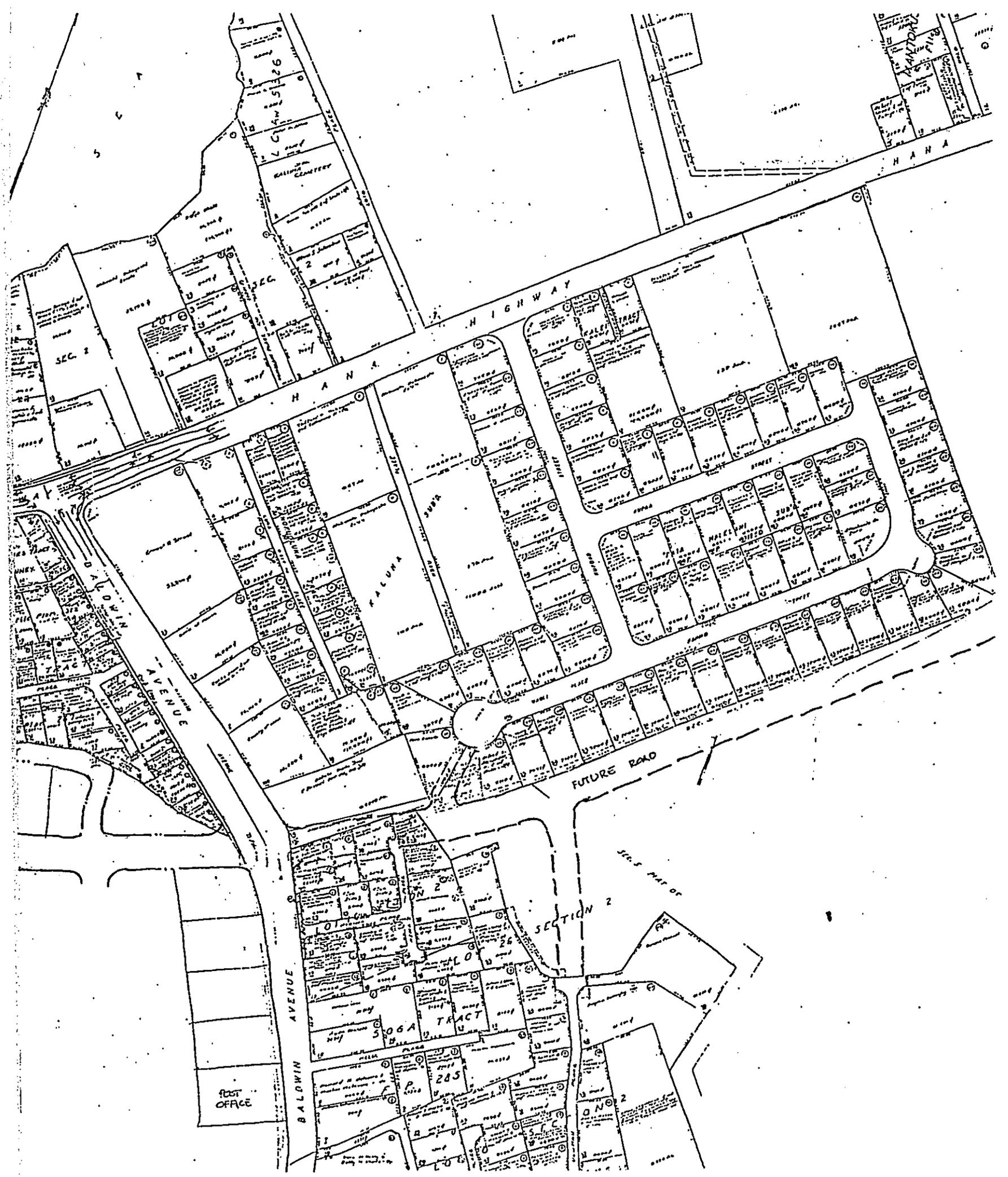
EXHIBIT II



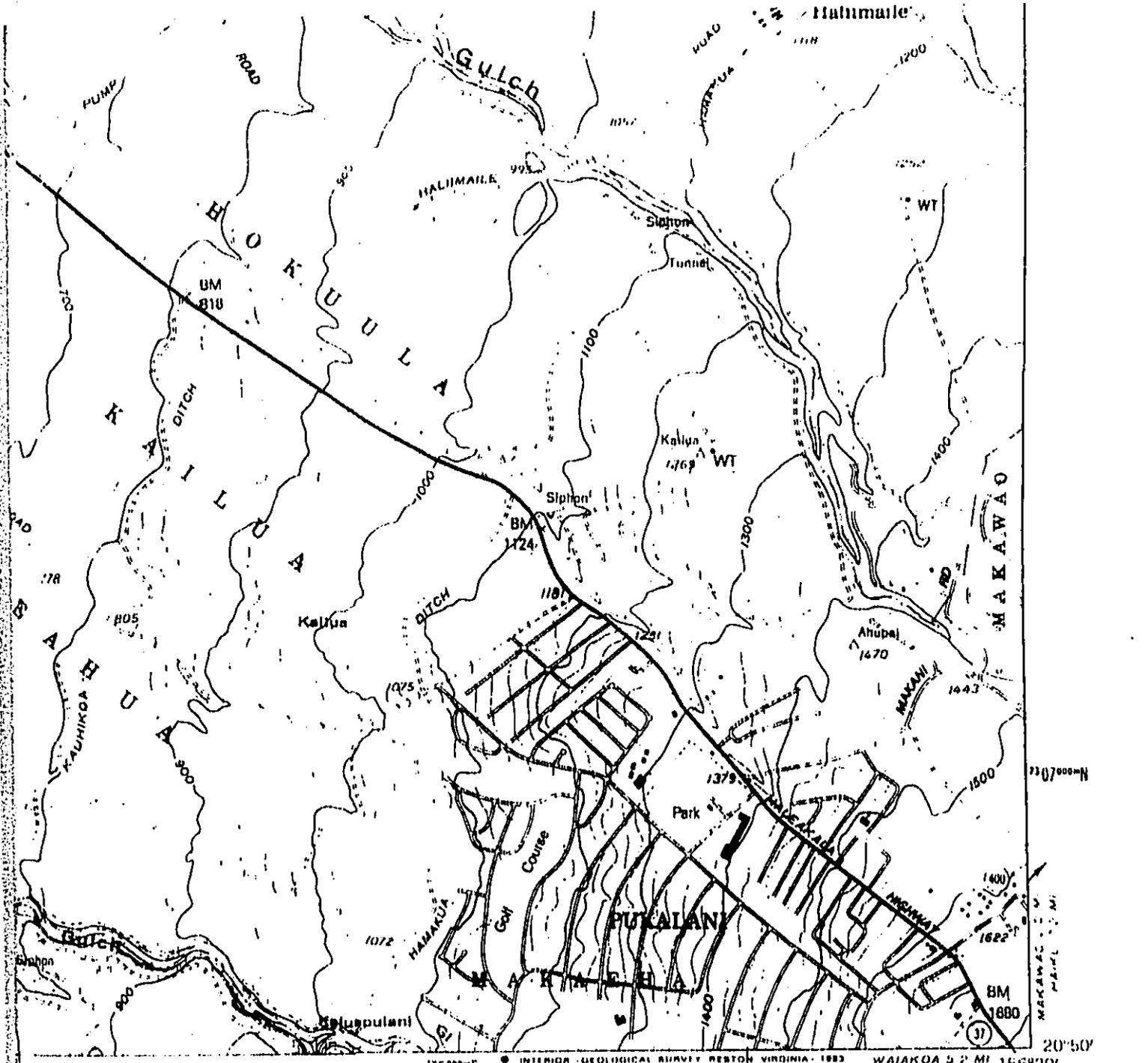
DEPT OF PLANNING  
 COUNTY OF MAUI  
 RECEIVED  
 04 MAR 24 AM 11:12

PAIA TOWN  
 SCALE 1" = 100'  
 11/17/77

RECEIVED AS FOLLOWS

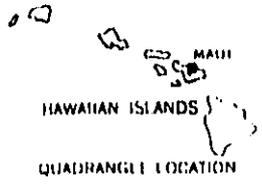


RECEIVED AS FOLLOWS



ROAD CLASSIFICATION

- Primary highway, all weather, hard surface
- Secondary highway, all weather, hard surface
- Light-duty road, all weather, improved surface
- Unimproved road, fair or dry weather
- State Route



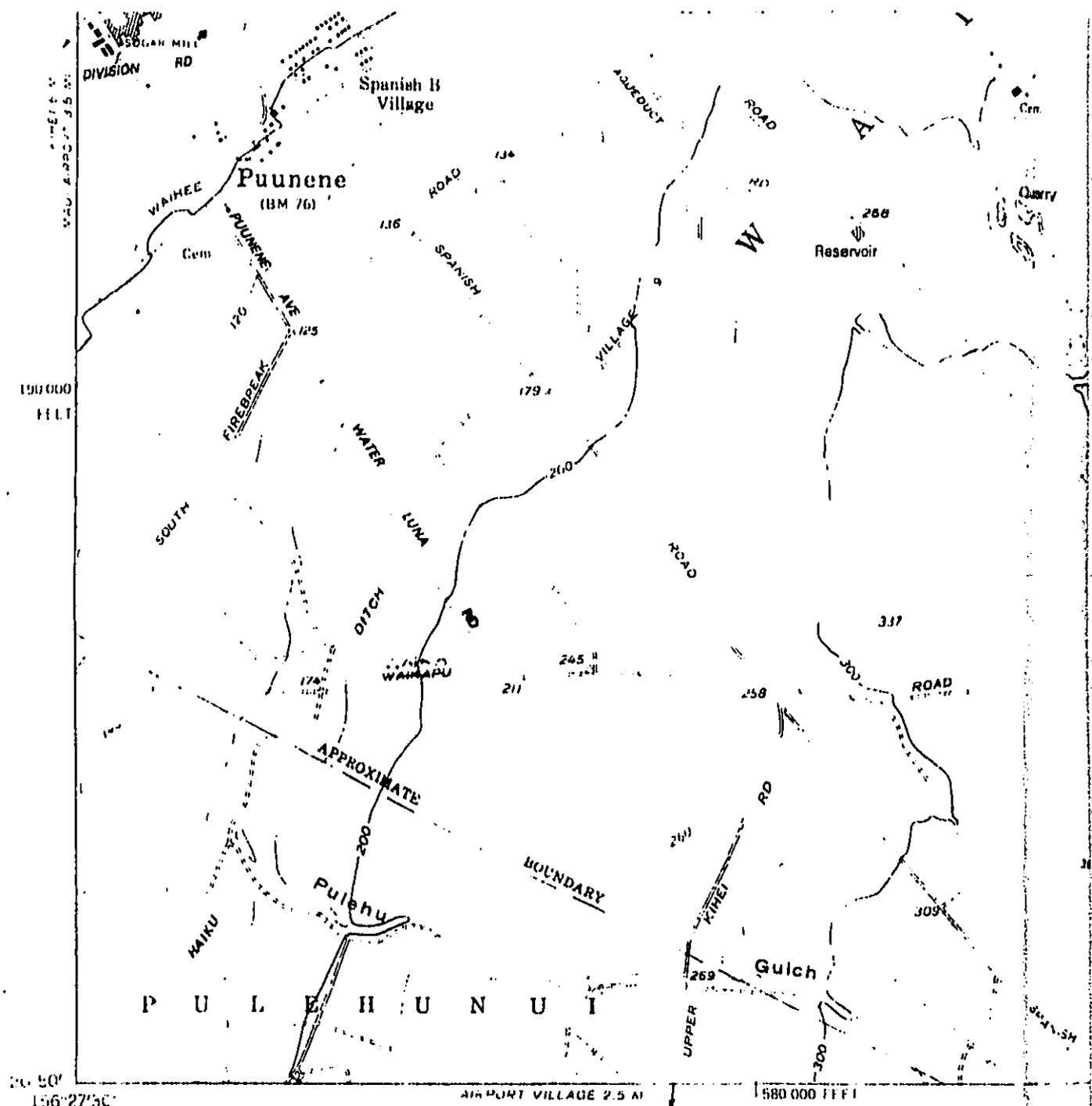
Map of the Island of Maui, scale 1:62 500, from an earlier survey is available

PAIA, HAWAII  
N2050 W15620/7.5

1983



RECEIVED AS FOLLOWS



(MAALAI A)

Mapped, edited, and published by the Geological Survey  
 Revised in cooperation with Hawaii Dept. of Transportation  
 Control by USGS and NOS/NOAA

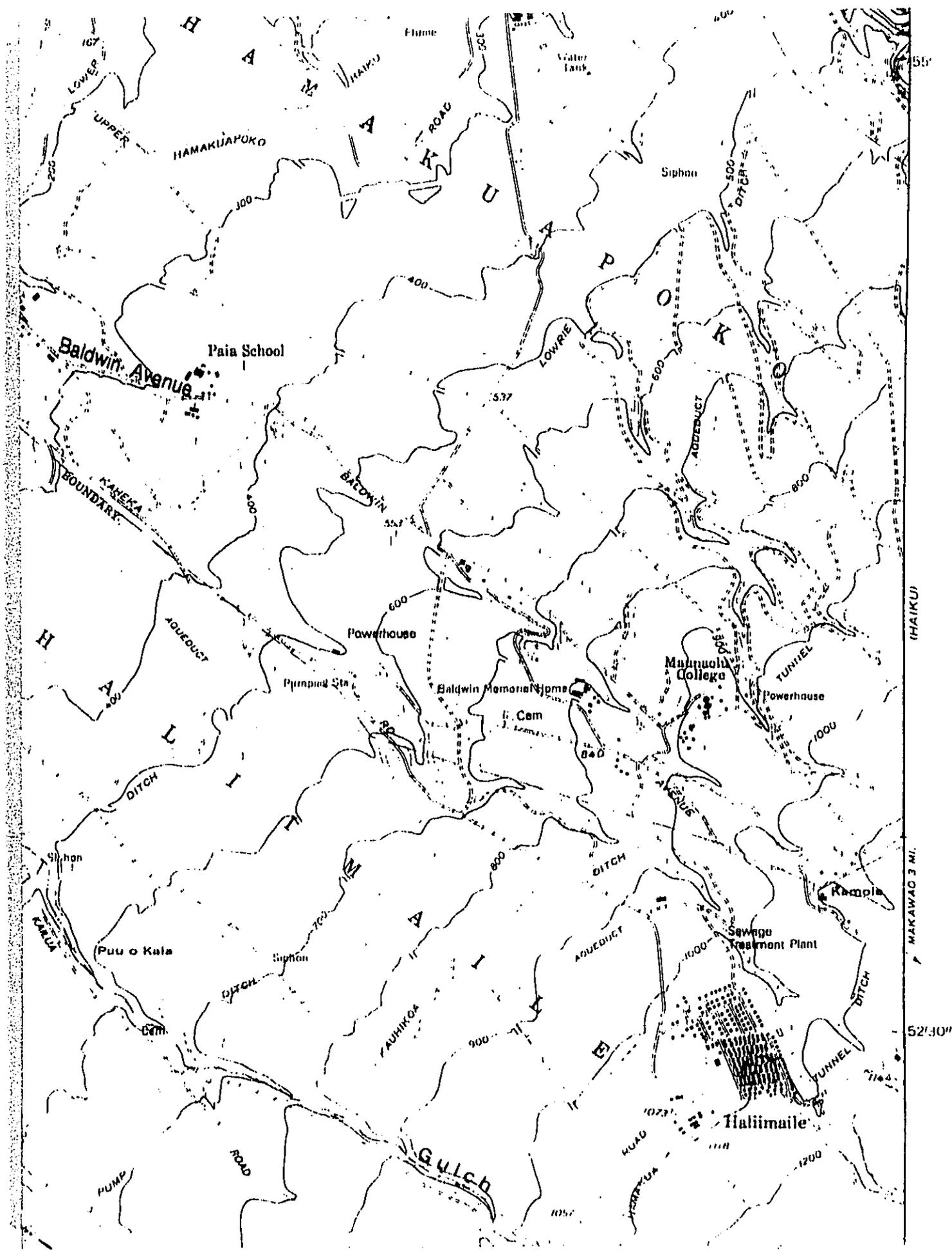
Topography by photogrammetric methods from aerial photographs  
 taken 1950 Field checked 1954. Revised from aerial photographs  
 taken 1977 Limited field check 1981. Map edited 1983

Selected hydrographic data compiled from NOS/NOAA charts 4116  
 4124, and 4130. This information is not intended for navigational  
 purposes

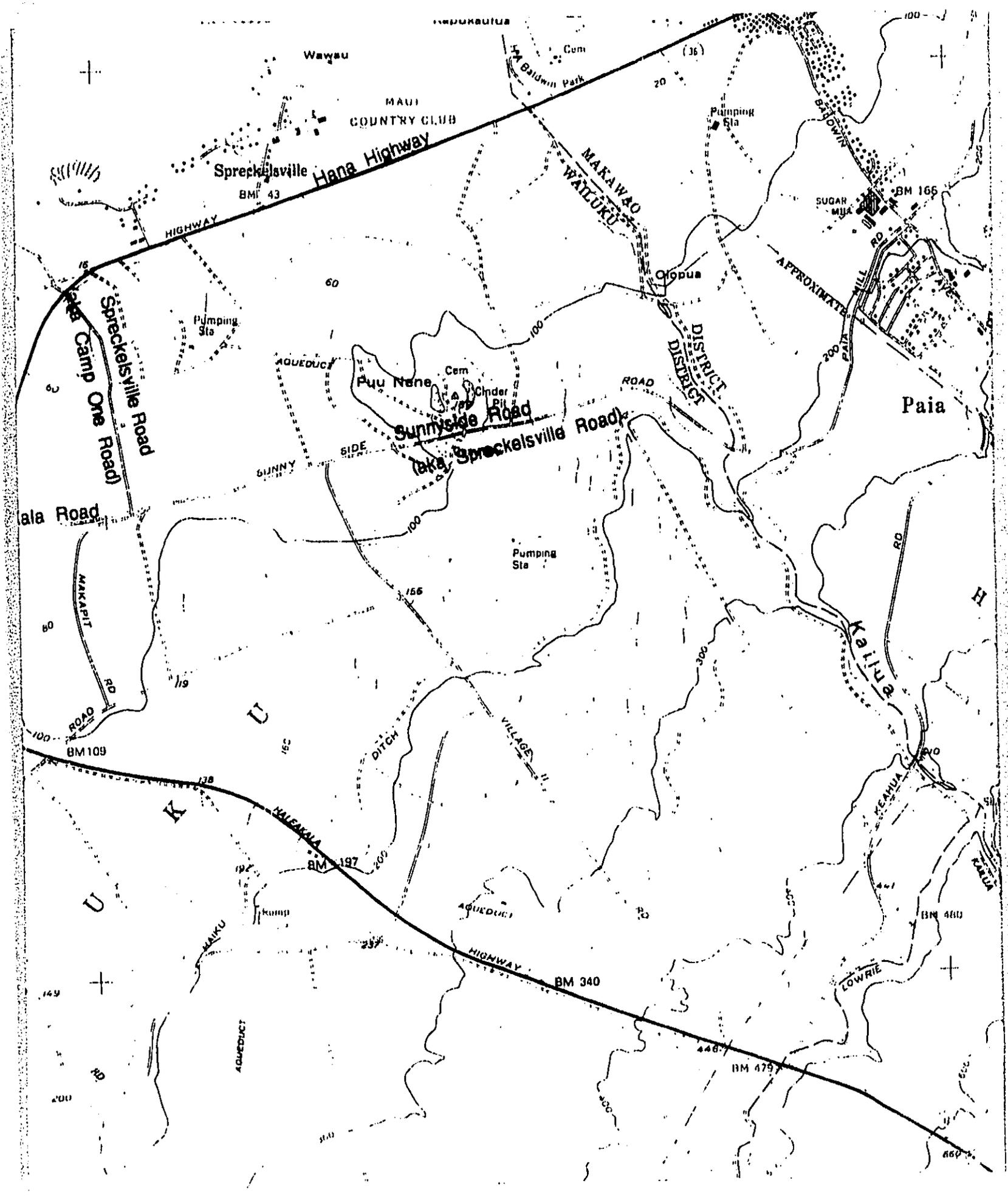
Projection and 10,000-foot grid ticks: Hawaii coordinate system,  
 zone 2 (transverse Mercator) Clarke spheroid 1866. Old Hawaiian Datum  
 1000-meter Universal Transverse Mercator grid ticks, zone 4, shown  
 in blue. International spheroid. To place on the predicted North  
 American Datum 1983 move the projection lines 357 meters north and  
 150 meters west as shown by dashed corner ticks



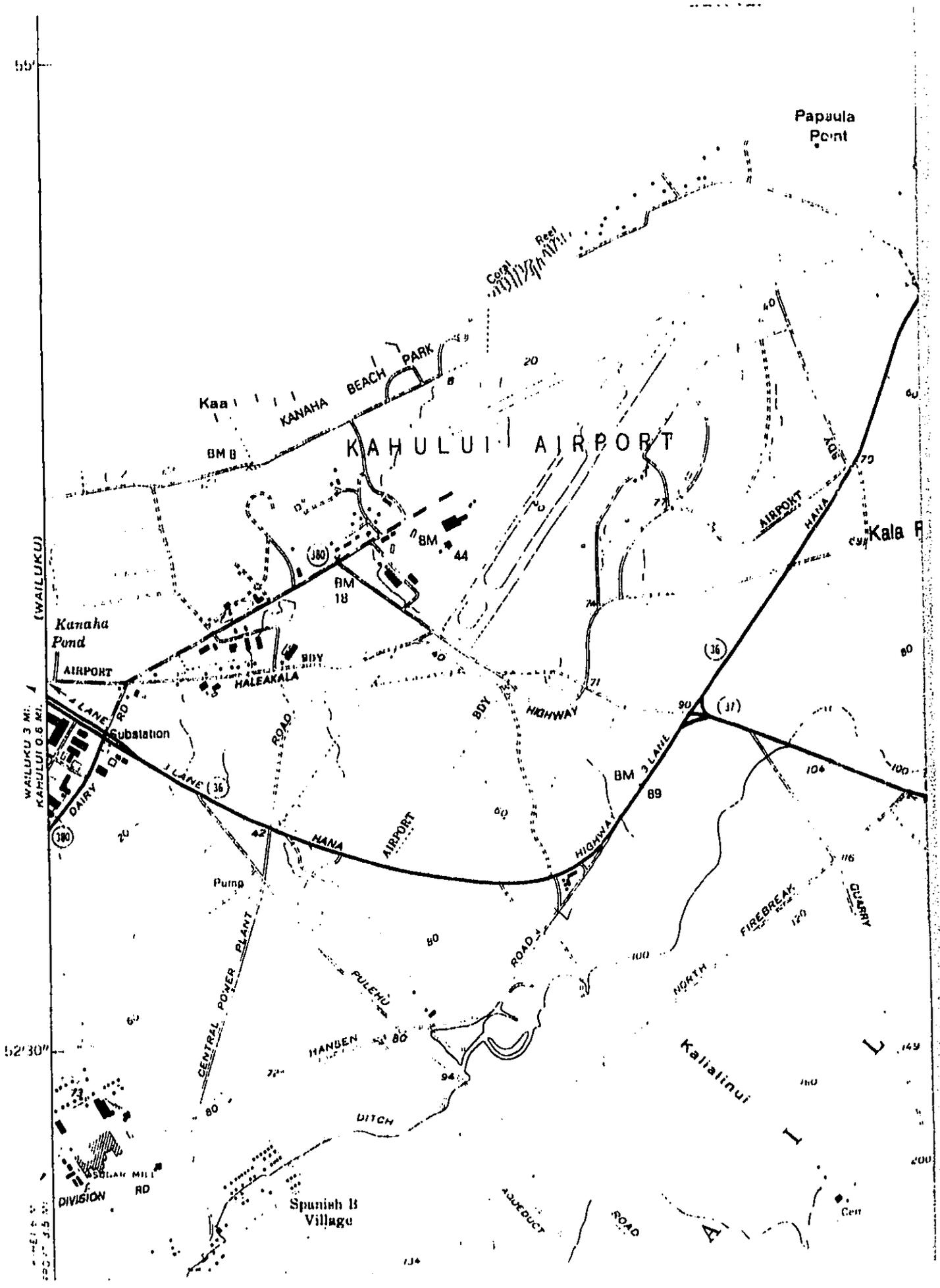
RECEIVED AS FOLLOWS



RECEIVED AS FOLLOWS



RECEIVED AS FOLLOWS



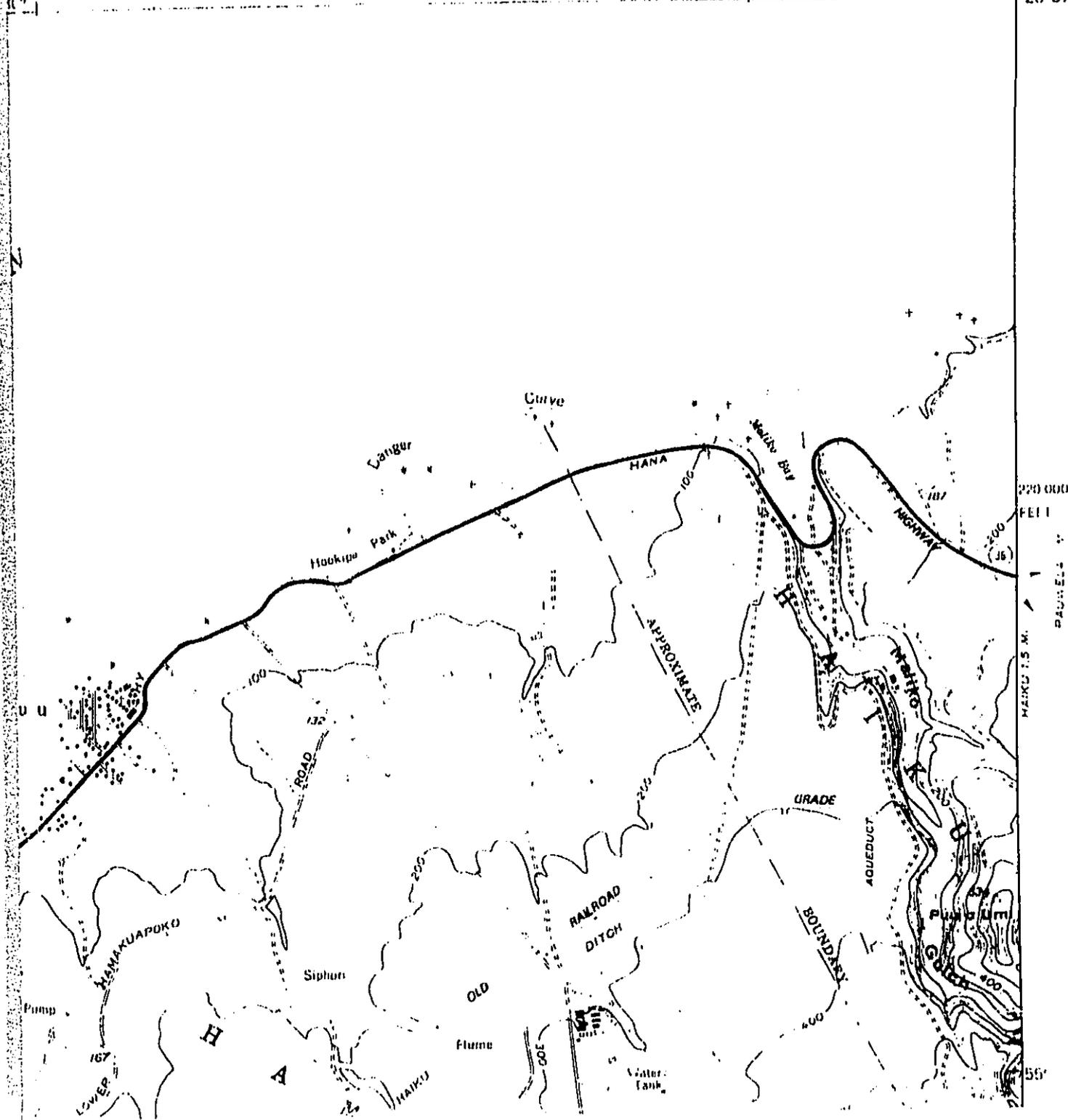
RECEIVED AS FOLLOWS

PAIA QUADRANGLE  
HAWAII—MAUI CO.  
ISLAND OF MAUI  
7.5 MINUTE SERIES (TOPOGRAPHIC)

610 000 FEET

156°20'

20°57'30"



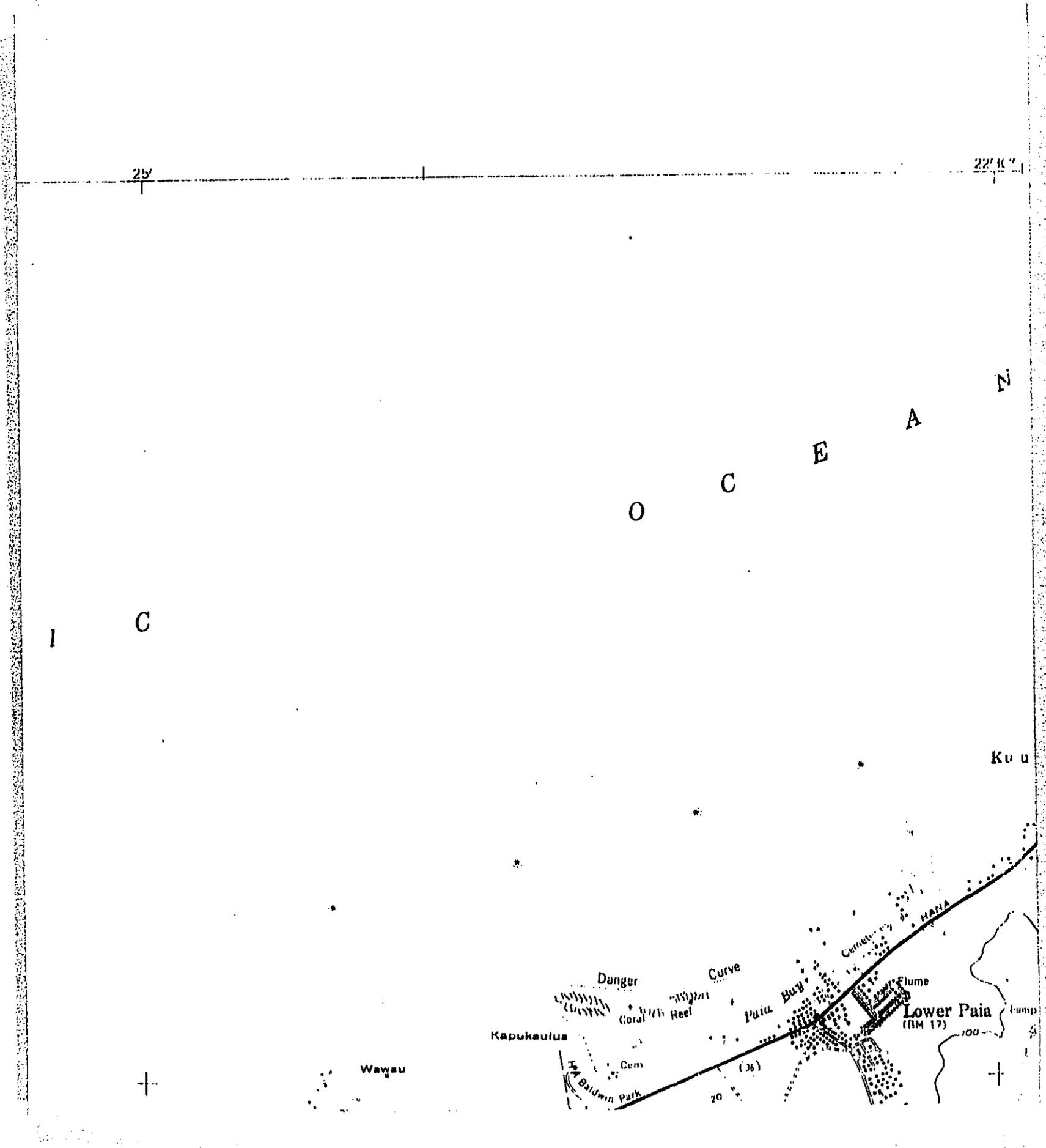
220 000  
FEET

PAUWELA

MARK 15 M.

55'

RECEIVED AS FOLLOWS



RECEIVED AS FOLLOWS

(KAHAKULOA)

UNITED STATES  
DEPARTMENT OF THE INTERIOR  
GEOLOGICAL SURVEY

156°27'30"  
20°57'30"

165°00'00"E

1000 ft

P A C I F I C

Spartan

Reef

55'